ORDINANCE NO. 2709

AN ORDINANCE OF THE LAKE OSWEGO CITY COUNCIL AMENDING LOC 50.05.007 (LAKE GROVE VILLAGE CENTER OVERLAY DISTRICT) TO REORGANIZE, REMOVE REDUNDANT PROVISIONS, AND CORRECT VARIOUS PROVISIONS; AND ADOPTING FINDINGS (LU 15-0064).

WHEREAS, through the application of the Community Development Code and related code provisions, the public and Planning Division staff have found that some sections of the Lake Oswego Code, Chapter 50 (Community Development Code) could be improved by removing ambiguous and conflicting language, correcting provisions, adding clarifying text which is consistent with past interpretations, and updating the Community Development Code;

The City of Lake Oswego ordains as follows:

<u>Section 1</u>. The City Council hereby adopts the Findings and Conclusions (LU 15-0064), attached as Attachment A.

<u>Section 2</u>. The Lake Oswego Code, Chapter 50 (Community Development Code) is hereby amended by deleting the text shown by strikethrough type and adding new text shown in <u>double underlined type</u> as set forth on Attachment B. (Sections or subsections within LOC Chapter 50 that are omitted below, and not marked for deletion or addition, are neither amended nor deleted by this Ordinance.)

<u>Section 3.</u> <u>Severability</u>. The provisions of this ordinance are severable. If any portion of this ordinance is for any reason held to be invalid, such decision shall not affect the validity of the remaining portions of this ordinance.

<u>Section 4.</u> <u>Effective Date</u>. As provided in Section 35.C. of the Lake Oswego Charter, this ordinance shall take effect on the thirtieth day following enactment.

Enacted at the meeting of the City Council of the City of Lake Oswego held on the 17th day of May, 2016.

AYES: Mayor Studebaker, Buck, Gudman, Gustafson, Collins, O'Neill, Manz.

NOES: None.

ABSENT: None.

ABSTAIN: None.

EXCUSED: None.

Kent Studebaker, Mayor

Dated: May 18,2016

ATTEST:

Anne-Marie Simpson

Anne-Marie Simpson, City Recorder

APPROVED AS TO FORM: 01 de 114 **David Powell City Attorney**

ATTACHMENT	A
(ORDINANCE 27	709)

1	BEFORE THE CITY COUNCIL				
2	OF THE CITY OF LAKE OSWEGO				
3	A REQUEST FOR AMENDMENTS TO THE COMMUNITY DEVELOPMENT CODE TO CITY OF LAKE OSWEGO				
4 5	REORGANIZE, REMOVE REDUNDANT PROVISIONS, AND CORRECT VARIOUS PROVISIONS OF THE LAKE GROVE VILLAGE				
6	CENTER OVERLAY DISTRICT				
7	NATURE OF PROCEEDINGS				
8	This matter came before the Lake Oswego City Council on the recommendation of the				
9	Planning Commission for legislative amendments to the Lake Oswego Community Development				
10	Code (CDC) to update various sections in order to reorganize text, remove redundant				
11	provisions, correct errors and clarify code text. The proposed amendments are to:				
12	LOC 50.05 Overlay and Design Districts LOC 50.10 Definitions and Rules of Measurement				
13					
14					
15					
16	HEARINGS				
17	The City Council considered this matter at a study session on April 19, 2016. On May 3				
18	2016, the City Council held a public hearing on the proposed amendments and made a				
19	tentative decision.				
20	CRITERIA AND STANDARDS				
21	A. City of Lake Oswego Comprehensive Plan:				
22	Community Culture – Civic Engagement Policies 1, 2, 4 and 5 Land Use Planning – Land Use Administration Policy D-1				
23	Inspiring Spaces and Places, Goal 1 Goal 1, Policy 7 Lake Grove Village Center Plan Goal 6, Policy 6.3				
24	Lake Grove Village Center Plan Goal 9, Policy 9.1				
25	; /////				
26					

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1 B. <u>City of Lake Oswego Community Development Code:</u>

2 LOC 50.07.003.16.a Legislative Decisions Defin	ned
LOC 50.07.003.16.c Required Notice to DLCD	
3 LOC 50.07.003.16.d Planning Commission Rec	ommendation Required
LOC 50.07.003.16.e City Council Review and D	ecision

4

5 FINDINGS AND REASONS

6 As findings supporting its decision, the City Council incorporates the February 11, 2016, 7 staff report, with all exhibits, the April 14, 2016, Council Report, with all exhibits, and the 8 Findings, Conclusions and Order of the Planning Commission, supplemented by the further 9 findings and conclusions below. If there is any inconsistency between the City Council's 10 11 supplemental findings below and the incorporated matters, the supplemental findings control. 12 Following are the supplemental findings and conclusions of this Council: 13 The City Council concurs with the Planning Commission's findings that the proposed 14 15 exceptions to certain streetscape standards for small remodels will encourage retention of 16 neighborhood-serving businesses and will provide needed flexibility for property owners. 17 Further, the proposed amendments providing flexibility for outdoor dining spaces will also 18 promote retention of these businesses, in addition to activating the streetscape and ensuring a 19 20 safe, attractive pedestrian environment. As a result, these Code amendments are consistent 21 with Policies 9.1 and 6.3 of Goal 9 of the Lake Grove Village Center Plan. 22 Relating to the proposed Code Amendments addressing the landscaping requirements 23 between low-density residential properties and commercial development greater than three 24 25 stories, the City Council agrees that the current buffering standard is not feasible in terms of 26 Page 2 - FINDINGS AND CONCLUSIONS (LU 15-0064)

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1	practicality and survivability of trees, and that other design elements, including canopies and			
2	trellises, can provide appropriate screening. The Council also finds that there are suitable			
3	coniferous trees that have the potential to provide sufficient screening within five years of			
4	planting, and that there are landscaping and design solutions to minimize the potential conflicts			
5 6	between root growth patterns of these trees and nearby infrastructure. Thus the related			
7	proposed amendments are appropriate, and are consistent with Goal 1, Policy 7 of the			
, 8	proposed amendments are appropriate, and are consistent with Goal 1, Policy 7 of the			
9	Comprehensive Plan.			
9 10	The City Council also concurs with the Planning Commission's findings that the current			
11	façade plane standard is superfluous on the side and rear facades of buildings, and that the			
12	proposed reduction of fenestration requirements will enhance consistency with approved			
13	architectural styles, and therefore finds that the related Code revisions are consistent with Goa			
14 15	1 of Policy 7 of the Comprehensive Plan and Goal 9 Policy 6.2 of the Lake Grove Village Conter			
16	Plan.			
17	CONCLUSION			
18	8 The City Council concludes that LU 15-0064, as recommended by the Planning			
19				
20	Commission, meets all applicable criteria and should be approved. The City Council also			
21	concludes that Ordinance 2709, which implements LU 15-0064, should be enacted.			
22				
23				
24				
25				
26	Page 3 – FINDINGS AND CONCLUSIONS (LU 15-0064)			
	David D. Power			

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50.05.007 LAKE GROVE VILLAGE CENTER OVERLAY DISTRICT

1. PURPOSE

The purpose of the Lake Grove Village Center Overlay District (LGVC Overlay) is to implement the vision, goals, and policies of the Lake Grove Village Center Plan, a special district plan within the City of Lake Oswego Comprehensive Plan. Implementation is intended to occur through land use requirements, development standards, and incentives that promote an attractive, pedestrian and bicycle friendly, mixed-use center. The LGVC Overlay features special standards to address specific conditions and ensure desired features and character as follows:

- **a.** Village Transition Area standards are intended to provide a transition between the core commercial area of the district and the surrounding residential areas.
- **b.** Village Commons and Village Gathering Places standards are intended to enhance community identity and provide opportunities for social and cultural engagement.
- **c.** Design Area standards are intended to ensure building design enhances the pedestrian environment and provides visual interest and appropriate scale suitable to either a street orientation or a campus orientation.
- **d.** Village Streetfront standards are intended to ensure spatial enclosure and either an urban, transitional or park character along public streets.

2. RELATIONSHIP TO OTHER DEVELOPMENT STANDARDS

Developments subject to this overlay district shall be in accordance with the standards of this section and, except in the event a section is superseded below (in part or whole), all other applicable Lake Oswego codes, standards and regulations.

- a. This overlay district supersedes the following sections in this Code:
 - i. Commercial Site Development Limitations, LOC 50.04.001.4.a;
 - Medium and High Density Residential Lot Coverage, Yard Setbacks and Buffers, and Height of Structures, Table <u>50.04.001-3</u>, Residential – Medium Density Zones Dimensions and Table <u>50.04.001-11</u>, Residential – High Density Zones Dimensions; and
 - iii. LOC 50.06.001.5, Commercial, Industrial, and Multi-Family Development Standards for Approval.
- **b.** In the event this section and other Lake Oswego codes, standards and regulations regulate the same matter, the LGVC Overlay standards shall supersede the other Lake Oswego codes, standards, and regulations, even if the LGVC Overlay standards are less restrictive than the other standard.

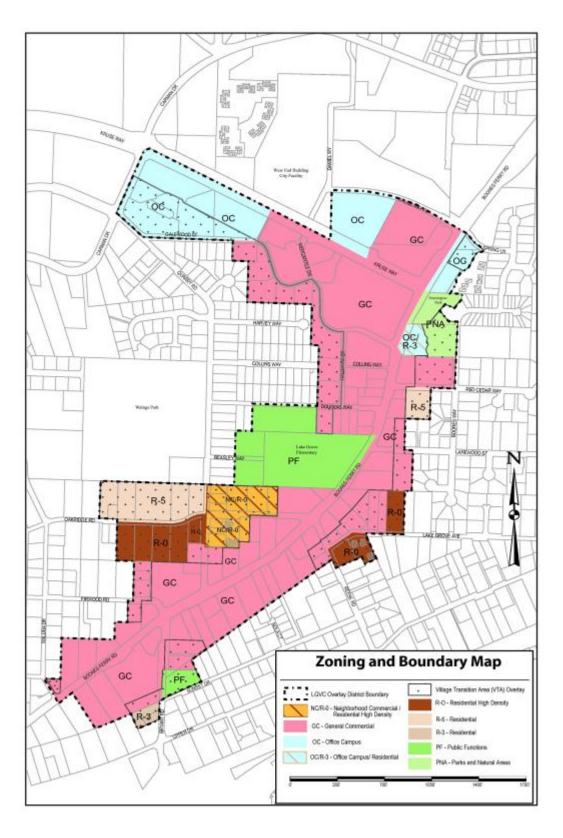


Figure 50.05.007-A: LGVC Overlay Boundaries

3. APPLICABILITY

a. Purpose

The purpose of the applicability section is to define the circumstances under which provisions of the LGVC Overlay apply to property, especially in regard to sites that are redeveloping or where buildings are being remodeled. To encourage incremental growth of existing, small local businesses, greater flexibility in application of the standards is allowed for projects that involve the remodeling of existing structures within the district.

b. General Applicability

ai. LGVC Overlay Applicability

The LGVC Overlay applies to all land within the boundaries of the Lake Grove Village Center Plan as identified on the Zoning and Boundary Map, Figure <u>50.05.007-A</u>: LGVC Overlay Boundaries. <u>The LGVC Overlay contains design standards that apply district-wide, and special</u> <u>design standards that apply only within the subareas shown in Figure 50.05.007-B: Village</u> <u>Transition Areas Map and Figure 50.05.007-C: Village Gathering Places and Commons Map.</u>

ii. Village Transition Area Applicability

Figure <u>50.05.007-B</u>: Village Transition Areas (VTA) Map, sets forth the areas in which special height and design standards and use restrictions apply. Three different types of Village Transition Area are depicted on the VTA Map. Standards set forth in the following sections apply to Village Transition Areas:

- (1) LOC <u>50.05.007.4.b</u>-e, Structure Height, Lot Coverage and FAR, Yard Setbacks and Streetfront Environment, and Site Dimensional Standards;
- (2) LOC 50.05.007.5.h, Special Design Standards within the Village Transition Area; and
- (3) LOC 50.05.007.7.b, Village Transition Area Uses.

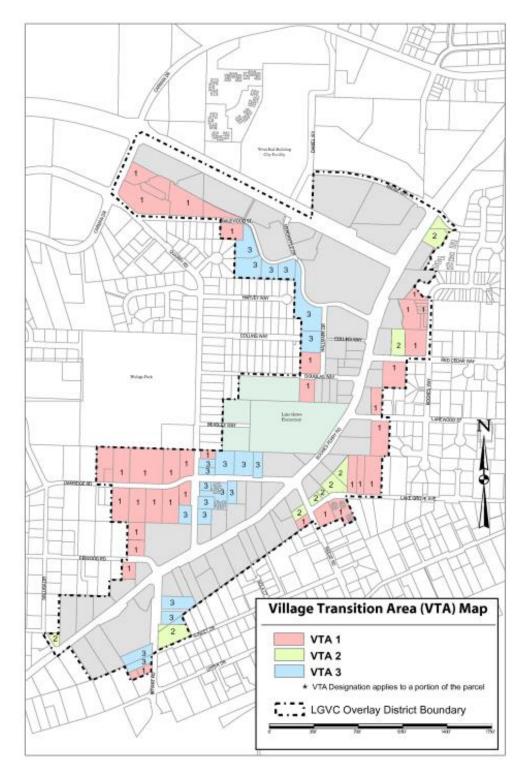


Figure 50.05.007-B: Village Transition Areas (VTA) Map

iii. Gathering Places and Village Commons Applicability

Figure <u>50.05.007-C</u>: Village Commons and Gathering Places Map, identifies a Village Commons and locations for seven Village Gathering Places. Standards set forth in LOC <u>50.05.007.7.c</u>, Village Commons and Gathering Places, apply to these designated open area features.

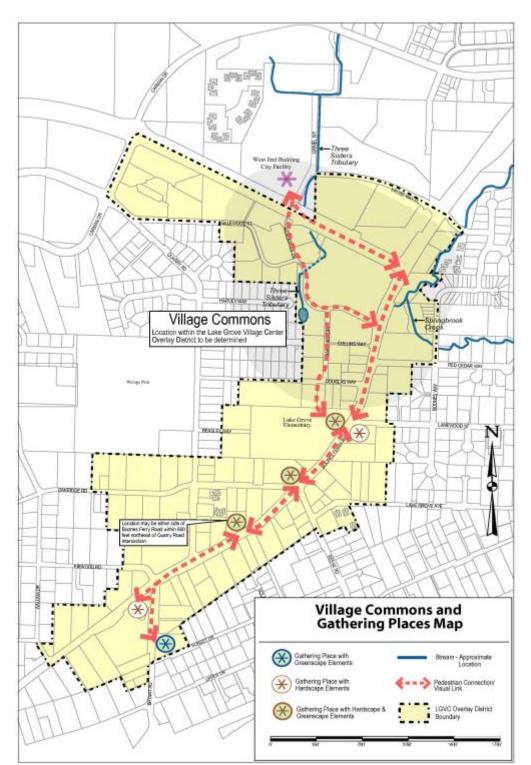


Figure 50.05.007-C: Village Commons and Gathering Places Map

biv. Design Area Applicability

Figure <u>50.05.007-D</u>: Village Character Map, sets forth the areas in which <u>Urban Village Design</u> and <u>Village Campus Design special building design</u> standards apply.

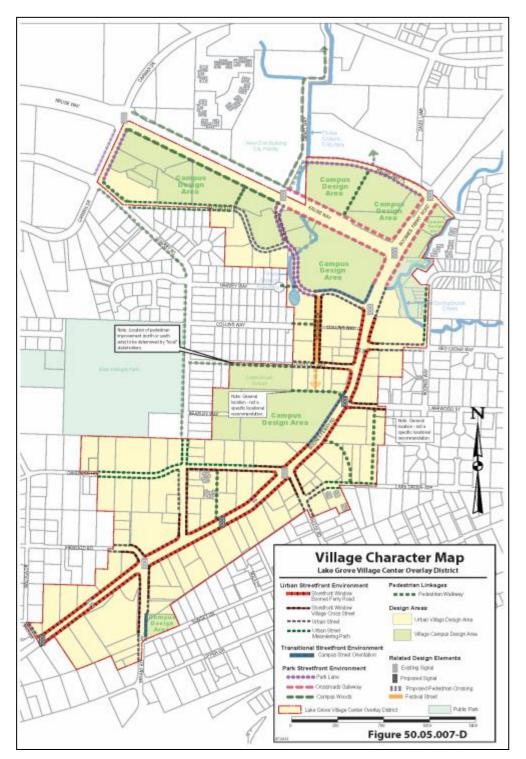


Figure 50.05.007-D: Village Character Map

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(1) Urban Village Applicability

Standards set forth in LOC <u>50.05.007.5.i</u>, Urban Village Design Areas, Building Design Standards, apply to locations designated as Urban Village.

(2) Village Campus Applicability

Standards set forth in LOC <u>50.05.007.5.j</u>, Village Campus Design Areas, Building Design Standards, apply to locations designated as Village Campus.

ev. Village Streetfront Applicability

Standards set forth in LOC <u>50.05.007.7.d</u>, Village Streetfront, apply to all public street frontage within the Lake Grove Village Center Overlay District. Distinct site dimensional, sidewalk or pathway, streetscape and landscape standards apply along public streets at locations identified on Figure <u>50.05.007-D</u>: Village Character Map.

(1) General Requirements Applicability

Standards set forth in LOC <u>50.05.007.7.d</u>.i, General Requirements, apply to all public street frontages within the Lake Grove Village Center Overlay District.

(2) Additional Village Streetfront Standards

Additional standards apply at specific locations as set forth below:

(a) Urban Streetfront Environment Applicability

Standards set forth in the following sections apply to locations designated Urban Streetfront Environment:

- (i) LOC <u>50.05.007.7.d</u>.ii, Urban Streetfront Environment, Special Requirements and Standards;
- (ii) LOC <u>50.05.007.4.b</u> and e, Structure Height and Streetfront Environment in the Site Dimensional Standards section. Village Transition Area dimensional standards may also apply;
- (iii) The following standards apply to street frontage designated Storefront Window Boones Ferry Road or Store Front Window Village Cross Streets:
 - (A) LOC 50.05.007.5.i.vii, Storefront Window;
 - (B) LOC 50.05.007.7.d.ii(4), Storefront Window Orientation;

- (C) Figure 50.05.007-CC: Storefront Window Streetscape; and
- (D) Figure 50.05.007-DD: Storefront Sidewalk Zone Requirements.
- (iv) The following standards apply to street frontage designated Urban Street or Urban Street Meandering Path:
 - (A) LOC 50.05.007.7.d.iii, Urban Street Orientation; and
 - (B) Figure 50.05.007-EE: Urban Street Orientation Streetscape.

(b) Transitional Streetfront Environment Applicability

Standards set forth in the following sections apply to Transitional Streetfront Environment, Campus Street Orientation designations:

(i) LOC <u>50.05.007.7.e</u>, Transitional Streetfront Environment; and

(ii) Figure 50.05.007-FF: Campus Street Orientation Streetscape.

(c) Park Streetfront Environment Applicability

Standards set forth in the following sections apply to Park Streetfront Environment designations:

- (i) LOC <u>50.05.007.4.d</u>, Yard Setbacks, Site Dimensional Standards. Village Transition Area yard setbacks in LOC <u>50.05.007.4.d</u>.ii may also apply;
- (ii) LOC 50.05.007.7.f, Park Streetfront Environment;
- (iii) Park Lane Applicability

Standards set forth in the following sections apply to Park Lane designations:

- (A) LOC 50.05.007.7.f.iii, Park Lane Special Requirements; and
- (B) Figure 50.05.007-GG: Park Lane Streetscape.
- (iv) Crossroads Gateway Applicability

Standards set forth in the following sections apply to Crossroads Gateway designations:

- (A) LOC 50.05.007.7.f.iv, Crossroads Gateway Special Requirements; and
- (B) Figure 50.05.007-HH: Crossroads Gateway Streetscape.

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(v) Campus Woods Applicability

Standards set forth in the following sections apply to Campus Woods designations:

(A) LOC 50.05.007.7.f.v, Campus Woods Special Requirements; and

(B) Figure 50.05.007-II: Campus Woods Streetscape.

c. Applicability by Type of Development

i. New Construction/Redevelopment

All standards of this <u>overlay</u> section apply to new building(s) and alterations to existing buildings that do not meet the definition of "remodel" in subsection 3.c.ii of this section, and site improvements on any vacant or redeveloped site and to new building(s) and related site improvements on any partially developed or developed site.

ii. Remodeled Buildings, Building Expansion, and Site Improvements

The standards of this <u>overlay</u> section apply to any remodeling, building expansion, or site improvement project on a partially developed or developed site, except as expressly provided below.

For the purpose of this subsection, "remodel" means less than 50% of the exterior wall is removed or is no longer a necessary and integral structural component of the overall building. Elements of the exterior wall include columns, studs, or similar vertical load-bearing elements and associated footings. However, existing exterior walls supporting a roof that is being modified to accommodate a new floor level or roofline shall continue to be considered necessary and integral structural components, provided the existing wall elements remain in place and provide necessary structural support to the building upon completion of the roofline modifications. For buildings not principally supported by exterior bearing walls, "remodel" means less than 50% of the principal support structure including columns, structural frames and other similar primary structural elements, is removed or no longer a necessary and integral structural component of the overall building. Alterations that are undertaken solely to bring an existing building into compliance with the Building Code are not counted towards the 50% measurement.

(1) Standards apply only to the structure or to that portion of a structure or site that is being constructed, modified, remodeled, or built upon.

Exception – Boones Ferry Road Constrained Sidewalk: On property along Boones Ferry Road depicted with constrained sidewalk sections on Figure 50.05.007-L (Streetfront Environment Map), public right-of-way shall be dedicated and sidewalks shall be widened as required by the site planning standards of LOC 50.05.007.6 when:

- (a) 750 sq. ft. or more of floor area is demolished and reconstruction occurs of at least 750 sq. ft. of floor area; or
- (b) The existing floor area is increased by 50% of more, or 750 sq. ft., whichever is less; or
- (c) The number of parking spaces is increased by 15% or more.

These can occur either by a single project or cumulative projects within five years from the date of application, but not including projects applied for before October 4, 2012.

Exception – Remodels Under 750 sq. ft.*: The standards for Build-to Line, Minimum Street Frontage and Minimum Height at Streetfront do not apply to remodels where:

- (a) Less than 750 sq. ft. of floor area is demolished and reconstructed, or
- (b) Existing floor area is increased by less than 750 sq. ft., or
- (c) <u>An outdoor dining / seating area of less than 750 sq. ft. is added between an existing</u> <u>structure and</u>
 - (i) <u>The outdoor dining / seating area abuts a restaurant or specialized food store;</u> and
 - (ii) <u>The outdoor dining / seating area is visible from the street; and</u>
 - (iii) <u>The building fronting the outdoor dining / seating area has windows, and doors</u> <u>opening onto the dining / seating area; and</u>
 - (iv) <u>The outdoor dining / seating area incorporates materials and landscaping</u> <u>consistent with the Lake Grove Village Center Design Handbook streetscape</u> <u>design standards; and</u>
 - (v) No dimension is less than 10 ft.; and
 - (vi) <u>A covenant is recorded requiring the outdoor dining / seating area to provide</u> <u>benches, landscaping in pots and an opportunity for public art if there is a change</u> <u>of use from a restaurant or specialized food store.</u>

<u>*The exceptions above apply cumulatively to all remodels and new construction after</u> [effective date of ordinance].

(2) Standards that enhance the pedestrian environment-<u>The following standards</u> apply only when the proposed development occurs within the build-to line, LOC <u>50.05.007.4.e.</u>i. For purposes

of this subsection, the following standards are the standards that enhance the pedestrian environment:

- (a) LOC 50.05.007.4.e, Streetfront Environment;
- (b) LOC <u>50.05.007.5.d</u>.v, Rain Protection;
- (c) LOC 50.05.007.5.i.iii, Public Plaza;
- (d) LOC <u>50.05.007.5.i</u>.iv, Urban Village Standards for Buildings Exceeding 35 ft. or Two and One-half Stories;
- (e) LOC 50.05.007.5.i.vii, Storefront Window;
- (f) LOC 50.05.007.7.d.ii, Urban Streetfront Environment; and
- (g) LOC 50.05.007.7.e, Transitional Streetfront Environment.
- (3) The site development standards in the following sections do not apply to building remodeling projects. The site development standards apply to site improvement projects only if the proposed site improvement includes the abutting street right-of-way:
 - (a) LOC 50.05.007.7.d, Village Streetfront;
 - (b) LOC 50.05.007.6.k.vii, Street Trees; and
 - (c) LOC <u>50.05.007.7.c</u>, Village Commons and Gathering Places.

d. Nonconforming Uses

This section supersedes all of LOC <u>50.01.006</u>, Nonconforming Uses, Structures, Lots and Site Features, except the following subsections.

- i. LOC 50.01.006.1, Nonconforming Defined; Rights Granted;
- ii. LOC <u>50.01.006.2.a</u>, Continuation of Nonconformity; and
- iii. LOC <u>50.01.006.6</u>, Discontinuation of Nonconforming Use.

4. SITE DIMENSIONAL STANDARDS

a. Purpose

These standards are intended to encourage buildings to be built at a pedestrian-scale and create a district that appears and operates like a traditional town or village with commercial and mixed-use structures centered on a primary commercial street, Boones Ferry Road. Village Edge and Village Transition Area requirements ensure heights compatible to adjacent residential development and provide a transition in scale, massing and height from the core commercial area to surrounding residential areas. Standards requiring a storefront environment do not apply to street frontage along Kruse Way and other locations where a different character is desired.

b. Structure Height

i. Maximum Height Limits

Maximum height limits are set forth in Table <u>50.05.007-1</u> below and in the provisions that follow. Height limits apply at locations identified on Figure <u>50.05.007-E</u>: Village Height Map<u>and are</u> <u>illustrated in Figure 50.05.007-F: Height Measurement, Figure 50.05.007-G: Step Back Above</u> <u>Second Story, Figure 50.05.007-H: Village Edge Height Limits and Figure 50.05.007-I: Village</u> <u>Transition Height Limits</u>. In case of a conflict, locations identified on <u>in Figure 50.05.007-E</u>, the Village Height Map_± apply.

TABLE 50.05.007-1: VILLAGE CENTER MAXIMUM HEIGHT LIMITS		
Applicability	Maximum Height	
Base Zone Height Limits – LOC <u>50.05.007.4.b</u>.i(1):		
GC, OC, R-O, R-3, R-5, OC/R-3	45 ft.	
PF, NC/R-O *(see NC/R-O below for residential use):	35 ft.	
*NC/R-O on a lot or lots developed as one project of 1/2 acre or greater in total area with residential dwellings at a minimum density of 20 units per acre.	45 ft.	
Step Back Above Second Story – LOC <u>50.05.007.4.b</u> .i(2):		
Structures exceeding 35 ft. in height or two and one-half stories at locations designated on the Village Character Map, LOC Appendix 50.11A.020-D as Urban Streetfront Environment, or as Transitional Streetfront Environment – Campus Street Orientation and located within 10 ft. of property line.	Stories above the second story shall be stepped back from the adjacent lower story facing a street by a	

TABLE 50.05.007-1: VILLAGE CENTER MAXIMUM HEIGHT LIMITS		
	Maximum Height	
		minimum of eight ft.
Village Edge Heig	ght Limits – LOC <u>50.05.007.4.b</u>.i(3):	
All Zones	Applies to a distance of 30 ft. to 48 ft. from parcels zoned R-7.5 or R-10 adjacent to the rear property line of the subject property. <u>May apply at the</u> <u>side property line of the subject property adjacent to parcels zoned R-</u> <u>7.5 or R-10 at limited locations identified on Figure 50.05.007-E: Village</u> <u>Height Map</u> . Applies at a distance less than 30 ft. from parcels zoned R-7.5 or R-10 adjacent to the rear property line of the subject property. Exceptions shall be allowed for legal fence and walls.	28 ft. 0 ft.
Village Transition	n Limits – LOC <u>50.05.007.4.b</u>.i(4):	
	Applies within VTA 1 at a distance less than 60 ft. from parcels zoned R- 7.5 or R-10 and front or side property line of the subject property.	
Village Transition Areas (VTA)	Applies within VTA 1 at a distance of 60 ft. to 120 ft. from parcels zoned R- 7.5 or R-10 and the front or side property line of the subject property.	35 ft.
,	Applies within VTA 2 and VTA 3 at a distance up to 120 ft. from parcels zoned R-7.5 or R-10 and the front or side property line of the subject property.	35 ft.

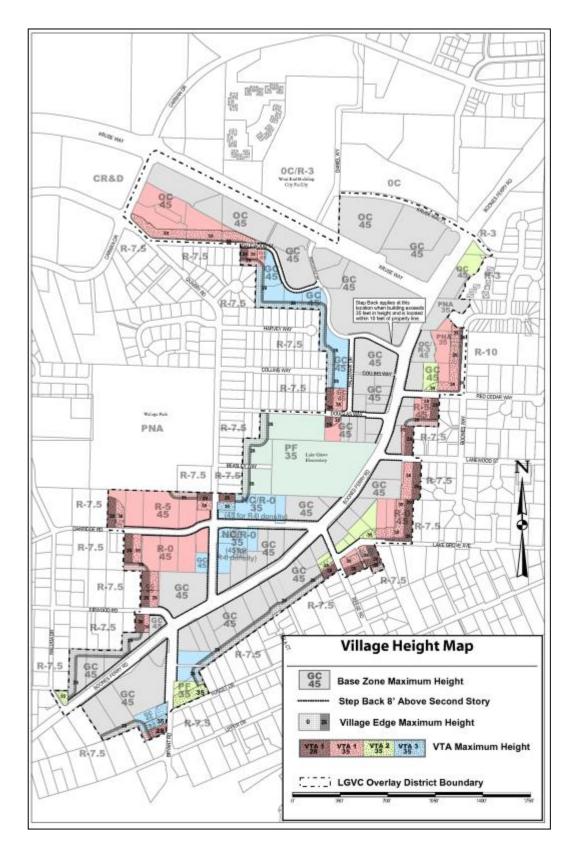
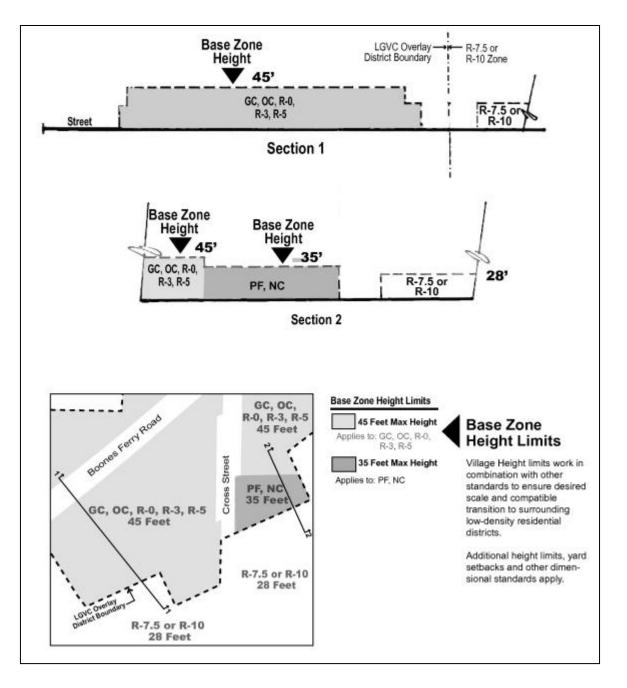


Figure 50.05.007-E: Village Height Map

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(1) Base Zone Height Limits

Maximum heights are established for each zone in the Lake Grove Village Center Overlay District as set forth below. Base zone height limits are illustrated in Figure <u>50.05.007-F</u>: Height Measurement.



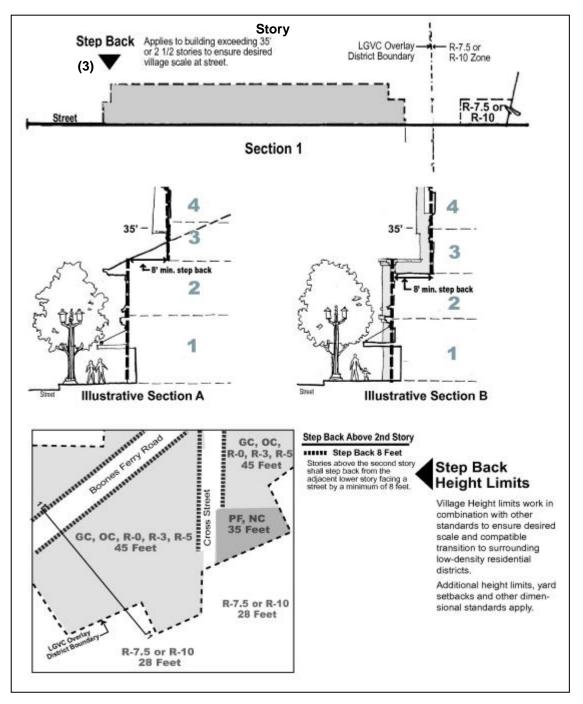


- (a) This standard applies at locations identified on Figure <u>50.05.007-E</u>: Village Height Map. Maximum base zone height limits are reduced in accordance with special conditions and at locations set forth in this subsection.
- (b) Maximum height in the PF zone is 35 ft.
- (c) Maximum height in the NC/R-0 zone is 35 ft. except 45 ft. maximum height is allowed on a lot or lots developed as one project of 1/2 acre or greater in total area with residential dwellings at a minimum density of 20 units per acre.
- (d) Maximum height in the GC, OC, R-0, OC/R-3 and R-3 zones is 45 ft.
- (e) Notwithstanding the provisions above, heights are permitted in accordance with LOC 50.04.003.4, General Exception to Structure Height Limitations.

(2) Step Back Above Second Story

This standard ensures a desired building scale along street frontages throughout the Village Center where Urban Streetfront Environment standards require buildings to be located close to a public street, or where Transitional Streetfront Environment standards allow a building to be located close to a public street. This standard also <u>and</u> ensures compatible transitions to surrounding low-density residential districts as illustrated in Figure <u>50.05.007-G</u>: Step Back Above 2nd Story.

- (a) This standard applies to all buildings exceeding 35 ft. in height or two and one-half stories at locations identified on Figure 50.05.007-E: Village Height Map.
- (b) Stories above the second story shall be stepped back from the adjacent lower story facing a street by a minimum of eight ft.





(3) Village Edge Height Limits

This standard ensures heights compatible to low density residential properties backing up to Village Center development at the edge of the Lake Grove Village Center Overlay District. Village Edge Height Limits standards are illustrated in Figure <u>50.05.007-H</u>: Village Edge Height Limits.

- (a) Village Edge Height Limits apply at locations along the edge of the overlay district as identified on Figure <u>50.05.007-F</u>: Height Measurement.
- (b) Maximum height is 28 ft. at a distance of 30 ft. to 48 ft. from parcels zoned R-7.5 or R-10 adjacent to the rear property line of the subject property. Village Edge Height Limits may apply at the side property line of the subject property adjacent to parcels zoned R-7.5 or R-10 at limited locations identified on Figure <u>50.05.007-E</u>: Village Height Map.
- (c) At a distance less than 30 ft., the height limit is 0 ft.

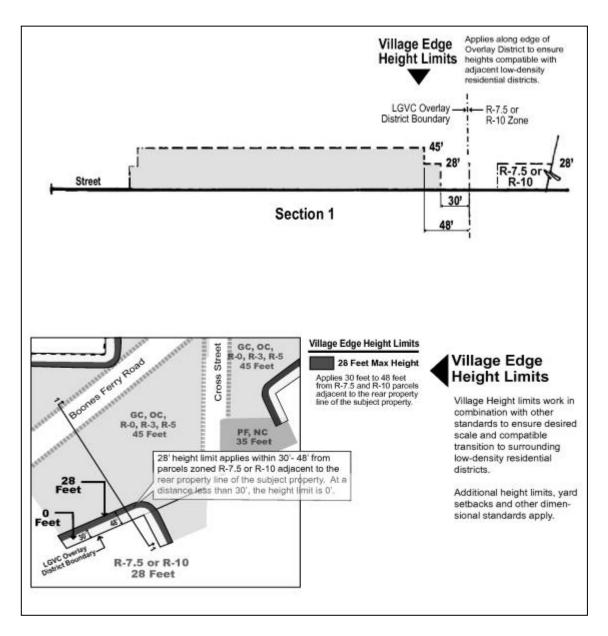
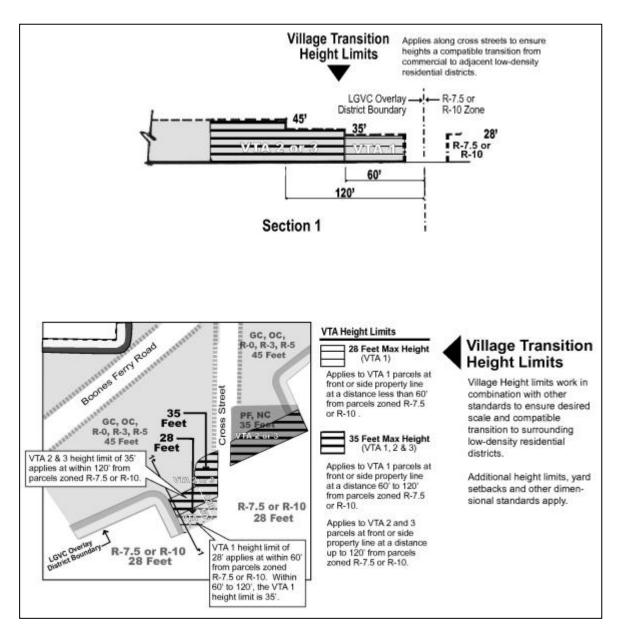


Figure 50.05.007-H: Village Edge Height Limits

(4) Village Transition Height Limits

This standard provides the height transition along streets connecting residential neighborhoods to the Village Center ensuring a desirable scale and compatible transition from commercial to adjacent low-density residential districts. Village Transition Height Limits are illustrated in Figure <u>50.05.007-1</u>: Village Transition Height Limits.





(a) Village Transition Height Limits apply to each transition area at locations identified in Figure <u>50.05.007-E</u>: Village Height Map.

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- **(b)** For areas identified as VTA 1, maximum heights are provided below. In case of a conflict, height limit applies to locations identified on the Village Height Map.
 - (i) Maximum height is 28 ft. at a distance less than 60 ft. from parcels zoned R-7.5 or R-10 and the front or side property line of the subject property.
 - (ii) Maximum height is 35 ft. at a distance of 60 to 120 ft. from parcels zoned R-7.5 or R-10 and the front or side property line of the subject property.
- (c) For areas identified as VTA 2 or VTA 3, maximum heights are 35 ft. at a distance up to 120 ft. from parcels zoned R-7.5 or R-10 and the front or side property line of the subject property. In case of a conflict, height limit applies to locations identified on the Village Height Map.
- **(d)** Village Transition Height Limits may not apply at the side property line of the subject property and parcels zoned R-7.5 or R-10 at limited locations identified on the Village Height Map.

ii. Standards for Buildings Exceeding 35 ft. in Height or 2.5 Stories

<u>Purpose: These standards ensure Ensure</u> taller buildings contribute to the pedestrian environment, and to support desired village scale and character, and provide for compatible transitions to surrounding low-density residential districts.

The following standards apply for any building exceeding 35 ft. in height or two and one-half stories:

- (1) Maximum Height at Boones Ferry Road. This standard applies for properties to buildings on lots along Boones Ferry Road. The maximum building height shall not exceed a plane that starts at the opposite street right-of-way and extends up toward the subject property at a 26.6 degree angle (two to one slope).
- (2) Public Sidewalk or Pathway. A minimum 12-ft. wide area for a public sidewalk or pathway consistent with Figure <u>50.05.007-V</u>: Pedestrian Facilities and Streetscape Map shall be provided. This may be accomplished within the public right-of-way or within a public pedestrian access easement on private property adjacent to the right-of-way.
- (3) Urban Village Design Standards. These requirements apply to locations designated Urban Village on the Village Character Map, Figure <u>50.05.007-D</u>.
- (3) Step Back Above Second Story: At locations designated on the Village Character Map, Figure 50.05.007-D, as Urban Streetfront Environment, or as Transitional Streetfront Environment – Campus Street Orientation and located within 10 feet of property line, stories above the second story shall be stepped back from the adjacent lower story facing a street by

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<u>a minimum of eight ft. This standard is illustrated on Figure 50.05.007-G: Step Back Above</u> <u>2nd Story.</u>

iii. Standards for Buildings Greater Than Three Stories

<u>Purpose: This standard eEnsures</u> that residential uses and additional landscape buffering adjacent to low-density residential zones are provided for buildings exceeding three stories.

Any building with more than three stories shall comply with subsection 4.b.ii of this section and the following:

(1) Residential Use Required Above Second Story

All stories above the second story shall be used exclusively for residential dwelling units.

(2) Requirements Adjacent to Low-Density Residential Zones

The following standards apply to properties lots adjacent to low-density residential zones:

(a) Screen Views from Upper Stories

Landscaping shall be provided to screen views of adjacent low-density residential property from upper stories. This shall be accomplished when the applicant provides graphic documentation such as site cross-sections or enhanced photos to demonstrate how proposed screening treatment effectively satisfies this requirement with consideration of distance and sight lines to and from adjacent low density residential property windows, decks and outdoor living spaces, and topography. <u>Acceptable</u> screening treatments include but are not limited to trees, shrubs, hedges, canopies, trellises, and a combination of these treatments. Trees, shrubs and hedges provided to satisfy this requirement must be of sufficient size to screen views-within five years; trees at the time of plantingmust be a coniferous species expected to grow a minimum of 2 ft. per year to maturity. When provided as screening, landscaping shall be bonded for a period of three years.

(b) Preserve Existing Trees and Features

Existing trees adjacent to low density residential property shall be incorporated in the landscape plan to the extent practicable pursuant to LOC 50.05.007.6.e, Landscape Requirements. Where preservation of existing trees is not practicable, mitigation trees shall be planted in the landscape buffer abutting the low density residential zone.

c. Lot Coverage and Floor Area Ratio (FAR)

i. Floor Area Ratio (FAR)

FAR restrictions do not apply within the LGVC Overlay.

ii. Lot Coverage

Lot coverage shall be permitted as set forth in Table <u>50.05.007-2</u> below and the provisions that follow:

TABLE 50.05.007-2: LOT COVERAGE				
Zone Districts and	Maximum Lot	Maximum Site	Additional Lot Coverage Allowed for Site Amenities	
Village Transition Areas	Coverage Base Case	Coverage with <u>All</u> Site Amenities Provided	Housing [1]	Sustainability Features
GC	50%	70%	Up to 15%	5%
OC, OC/R-3	30%	60%	Up to 25%	5%
NC/R-0, PF	40%	60%	Up to 15%	5%
R-0, R-3, R-5	40%	50%	NA	10%
VTA 1	40%	60%	Up to 15% [1]	5%
VTA 2	40%	60%	Up to 15% [1]	5%
VTA 3	40%	60%	Up to 15% [1]	5%
[1] Applies only when base zone is GC, OC, NC, NC/R-0, OC/R-3 or PF.				

iii. Site Amenities - Lot Coverage Bonus

Site amenities include at least one or both of the following:

(1) Housing

One sq. ft. of additional lot coverage up to the maximum amount listed in the housing column of Table <u>50.05.007-2</u> <u>is</u>shall be permitted for every two sq. ft. of housing provided on site. A minimum of three dwelling units per acre are <u>is</u>required to use this provision. A fractional dwelling unit shall be counted as the next highest whole dwelling unit.

(2) Sustainability Features

If any two of the following sustainability features are provided, an additional 5% of lot coverage is allowed within GC, NC, OC and PF zones, and an additional 10% of lot coverage is allowed within R-0, R-3 and R-5 zones:

- (a) Provide an ecoroof <u>over for 50% or more</u> of the building roof area. For the purposes of this section, an ecoroof shall be a lightweight, low-maintenance vegetated roof system that serves as a stormwater facility as reviewed and approved by the City Engineer. As part of the conditions for approval, applicant shall execute a covenant ensuring installation <u>in compliance with applicable building codes</u>, preservation, maintenance, and replacement, if necessary, of the ecoroof.
- (b) Reduce heat islands through one or more of the following:
 - (i) Provide shade (within five years) on at least 30% of non-roof impervious surfaces on the site, including parking lots, walkways, plazas, etc.; or
 - (ii) Place 50% of the parking underground or within a lower level<u>(s) of a parking</u> structure; or
 - (iii) Use Energy Star Roof compliant, high reflectivity and low emissivity roofing (initial reflectance of at least 0.65 and three-year aged reflectance of at least 0.5 when tested in accordance with ASTM E408) for a minimum of 75% of the roof surface.
- (c) Reduce potable water consumption by 50% over conventional means through use of captured rain and recycled site water for site irrigation.
- (d) Reduce wastewater and potable water demand by employing strategies that in aggregate use 20% less water than the water use baseline calculated for the building (not including irrigation). Strategies may include use of high efficiency fixtures, dry fixtures such as composting toilets and waterless urinals, and use of treated stormwater and graywater for nonpotable applications.

iv. Gathering Places - Lot Coverage Bonus

Lot coverage may be increased in accordance with LOC 50.05.007.7.c.ii.5.a.

On any lot where a Village Gathering Place is provided, lot coverage may be increased up to five percent in GC and OC zones without a Village Transition Area overlay, not to exceed maximums set forth in Table 50.05.007-2: Lot Coverage.

v. Public Plaza Easements - Lot Coverage Calculation

Lot coverage may be calculated in accordance with LOC 50.05.007.6.e.ii.

<u>Property within a public plaza easement area may be included as lot area for compliance with lot</u> <u>coverage standards when a public plaza meeting the requirements set forth in LOC</u> <u>50.05.007.5.i.iii is provided.</u>

vi. Sidewalk or Pathway Easements - Lot Coverage Calculation

<u>Property within a public easement for required sidewalk or pathway construction and</u> maintenance may be included as lot area for compliance with lot coverage standards.

d. Yard Setbacks

Yard setback requirements apply at locations identified on Figure <u>50.05.007-J</u>: Village Yard Setbacks Map. In case of a conflict with the standards set forth below, locations identified on the Village Yard Setbacks Map apply.

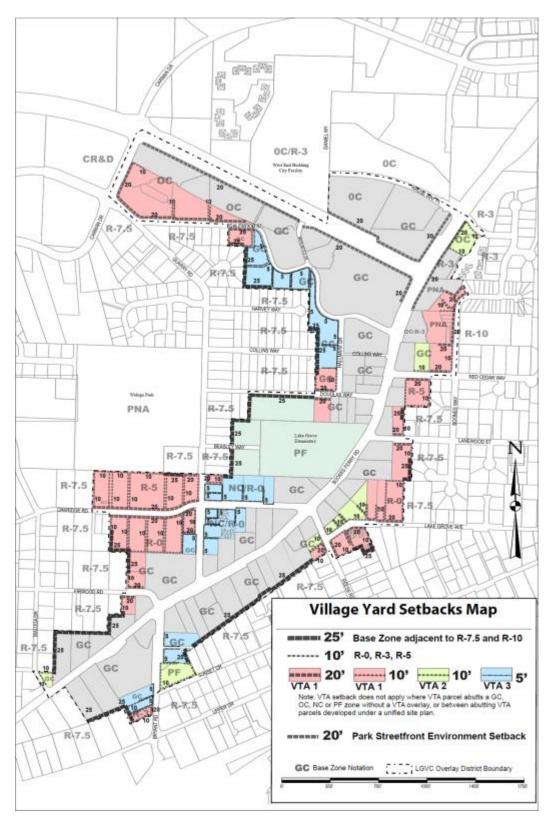


Figure 50.05.007-J: Village Yard Setbacks Map

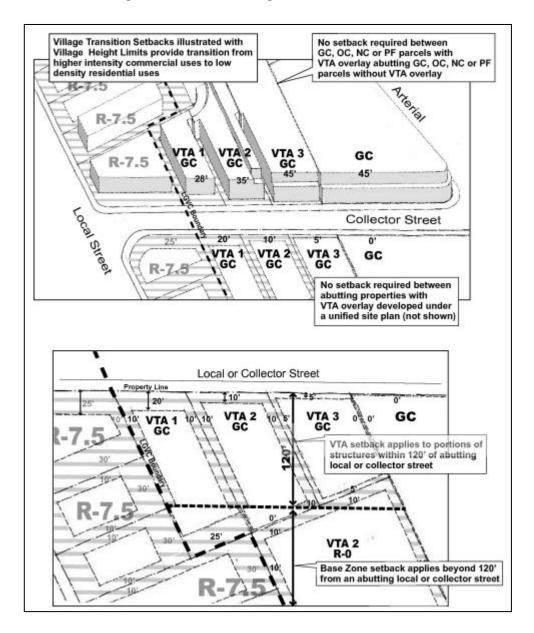
Required yard setbacks are set forth in Table <u>50.05.007-3</u> below and by the provisions that follow. Village Yard Setbacks are illustrated in Figure <u>50.05.007-J</u>: Village Yard Setbacks Map.

TABLE 50.05.007-3: VILLAGE YARD SETBACKS (LOCATIONS IDENTIFIED IN VILLAGE YARD SETBACKS MAP, FIGURE <u>50.05.007-J</u>)				
Applicability		Required Setback (minimum distance)		
	Base Zone Yard Setbacks LOC <u>50.05.007.4.d.</u> i:			
GC, NC/R-0, OC, PF, OC/R-3		 Yard adjacent to other than R-7.5 or R-10: None Yard adjacent to R-7.5 or R-10: 25 ft., except: 10 ft. – surface parking 5 ft. from southern line of the commercial area as described in LOC 50.05.007.I.vi(1) [ROW north of Sunset St.] (no surface parking in setback) 		
R-0, R-3, R-5 Front: 10 ft. Rear: 10 ft. Side: exterior wall: 10 ft. Side: interior wall: 0 ft.		Rear: 10 ft. Side: exterior wall: 10 ft.		
	Village	e Transition Yard Setbacks LOC <u>50.05.007.4.d</u>.ii <u>(1)</u>		
	VTA 1	Front: 20 ft. Rear: 20 ft. Side: 10 ft. GC, OC, NC, or PF abutting a GC, OC, NC, or PF zone without the VTA overlay: 0 ft.		
Village Transition Areas (VTAs) within 120 ft. of an abutting local or collector street	VTA 2	Front: 10 ft. Rear: 10 ft. Side: 10 ft. GC, OC, NC, or PF abutting a GC, OC, NC, or PF zone without the VTA overlay: 0 ft.		
	VTA 3	Front: 5 ft. Rear: 5 ft. Side: 5 ft. GC, OC, NC, or PF abutting a GC, OC, NC, or PF zone without the VTA overlay: 0 ft.		
Park Streetfront Environmental Setbacks LOC <u>50.05.007.4.d.iii</u>				
Road at locations where Park		Front (at street): 20 ft. Additional special pedestrian facilities, streetscape, and landscape requirements may apply within setback areas as set forth in LOC		

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TABLE 50.05.007-3: VILLAGE YARD SETBACKS (LOCATIONS IDENTIFIED IN VILLAGE YARD SETBACKS MAP, FIGURE <u>50.05.007-J</u>)		
Applicability Required Setback (minimum distance)		
Setbacks apply.	50.05.007.7.f, Park Streetfront Environment.	
<u>Notes:</u>	[1] No yard setback is required between abutting lots that are subject to the Village Transition Area overlay when both lots are developed under a unified site plan.	

Figure 50.05.007-K: Village Yard Setbacks



i. Base Zone Yard Setbacks

Yard setbacks are established for each zone in the Lake Grove Village Center Overlay District as set forth below.

- (1) This standard applies at locations identified on Figure <u>50.05.007-J</u>: Village Yard Setbacks Map. Base Zone Yard Setbacks are modified in accordance with special conditions and at locations as set forth in the Village Transition Yard Setbacks and Park Street Front Setbacks standards in this section.
- (2) Required yard setback in GC, NC/R-0, OC, and PF zones adjacent to an R-7.5 or R-10 zone shall be 25 ft.
- (3) Required yard setback in R-0, R-3 and R-5 zones shall ten ft. front, ten ft. rear, ten ft. side for exterior wall, and zero ft. side when attached.

ii. Village Transition Yard Setbacks

Village Transition Yard Setbacks apply as identified on Figure <u>50.05.007-J</u>: Village Yard Setbacks Map, in addition to other requirements set forth in this section. In case of a conflict, Village Transition Yard Setbacks apply. Village Yard Setbacks are illustrated in Figure <u>50.05.007-K</u>.

- (1) Village Transition Yard Setbacks apply to each transition area identified on the Village Yard Setbacks Map in Figure <u>50.05.007-J</u>.
- (2) Village Yard Setbacks apply to portions of structures within 120 ft. of an abutting local or collector street. For structures at a distance greater than 120 ft. from an abutting local or collector street, base zone yard setbacks apply.
- (3) No yard setback is required between abutting properties that are subject to the Village Transition Area overlay when both properties are developed under a unified site plan.

iii. Park Streetfront Environment Setbacks

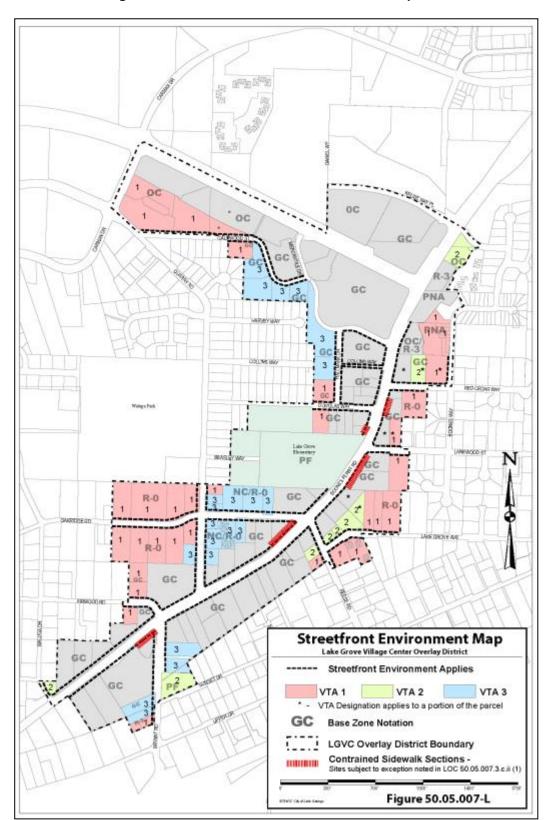
Park Streetfront Environment Setbacks identified on Figure <u>50.05.007-J</u>: Village Yard Setbacks Map apply along Boones Ferry Road and Kruse Way in addition to other requirements set forth in this section. In case of a conflict, Park Street Front Setbacks apply.

- (1) Required yard setback along Boones Ferry Road and Kruse Way shall be 20 ft.
- **(2)** Additional special pedestrian facilities, streetscape and landscape requirements may apply within setback area in LOC <u>50.05.007.7.f</u>, Park Streetfront Environment.

e. Streetfront Environment

<u>Purpose:</u>These standards ensure<u>Ensure</u> that buildings create a sense of enclosure along public streets.

The following standards apply at locations identified on the Figure <u>50.05.007-L</u>: Streetfront Environment Map.





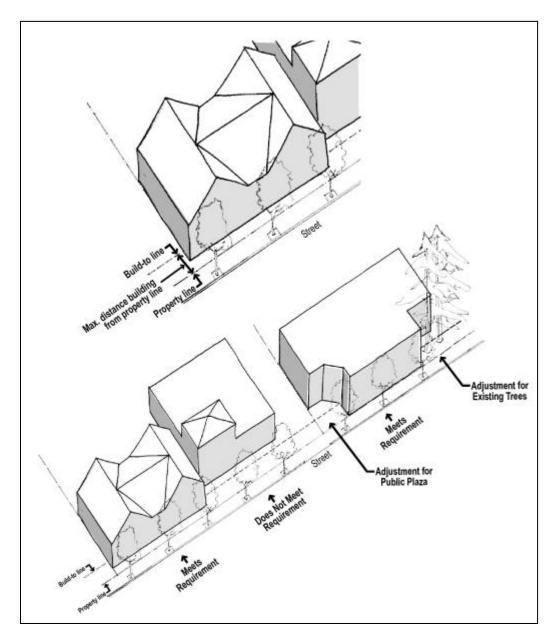
i. Build-to Line

The build-to line is the maximum distance a building may be set back from the front property line <u>Buildings shall be located to comply with the Build-to Line per Table 50.05.007-4 below,</u> as illustrated in Figure <u>50.05.007-M</u>: Build-to Line.

(1) Notwithstanding permitted adjustments, buildings shall meet build-to line requirements set forth in Table 50.05.007-4 below. The build-to line standard is illustrated in Figure 50.05.007-M: Build-to Line.

TABLE 50.05.007-4: BUILD-TO LINE STANDARDS			
Zone Districts and Village <u>Transition Areas</u>	<u>Frontage</u> <u>Conditions</u>	<u>Build-to Line</u>	
<u>GC, NC/R-0, R-0, R-3, OC/R-3</u>	All	<u>10 ft.</u>	
<u>VTA 1</u>	All	<u>25 ft.</u>	
<u>VTA 2</u>	All	<u>20 ft.</u>	
<u>VTA 3</u>	All	<u>10 ft.</u>	

Figure 50.05.007-M: Build-to Line



- (21) In case of a conflict with yard setback requirements in LOC <u>50.05.007.4.d</u>, Yard Setbacks, build-to line standards apply.
- (<u>3</u>2) If a parcel has more than two street frontages designated Streetfront Environment, the build-to line applies to two frontages only. On through lots where two street frontages are designated Streetfront Environment and only one building is proposed, the build-to line applies to the frontage to which the building is oriented. In all cases, the build-to line shall be met on the street with the highest street classification (i.e. an arterial street over a local street) subject to Streetfront Environment standards as set forth in this section.

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- (<u>4</u>3) The build-to line may be changed as set forth below:
 - (a) Up to ten ft. to protect any tree with a trunk diameter of at least five caliper in. or larger measured at 54 in. above mean ground level at the base of the trunk.
 - (b) As necessary to accommodate a public plaza meeting the requirements set forth in Building Design LOC <u>50.05.007.5.i.iii</u>, Public Plaza.
 - (c) As necessary to accommodate a Village Commons or Gathering Place in accordance with LOC <u>50.05.007.7.c</u>.
 - (d) As determined by the City Engineer to preserve existing trees and satisfy requirements set forth in LOC <u>50.05.007.6.b</u>-e, Site Planning Standards.
- (4) Notwithstanding permitted adjustments, buildings shall meet build-to line requirements set forth in Table <u>50.05.007-4</u> below. The build-to line standard is illustrated in Figure <u>50.05.007-4</u> <u>M</u>: Build-to Line.

TABLE 50.05.007-4: BUILD-TO LINE STANDARDS				
Zone Districts and Village Transition Areas	Frontage Conditions	Build-to Line		
GC, NC/R-0, R-0, R-3, OC/R-3	All	10 ft.		
VTA 1	All	25 ft.		
VTA 2	All	20 ft.		
VTA 3	АШ	10 ft.		

ii. Minimum Building Width at Street Frontage

The minimum width of building required at the street shall be determined as a percentage of street frontage designated on the Streetfront Environment Map as follows:

(1) Minimum <u>building width at</u> street frontage is the ratio of A to B expressed as a percentage where: A equals the width of the portion of the building(s) meeting the build-to line requirement, and B equals the length of the front lot line abutting the public right-of-way. Minimum <u>building width at</u> street frontage is illustrated in Figure <u>50.05.007-N</u>: Minimum <u>Building Width at</u> Street Frontage.

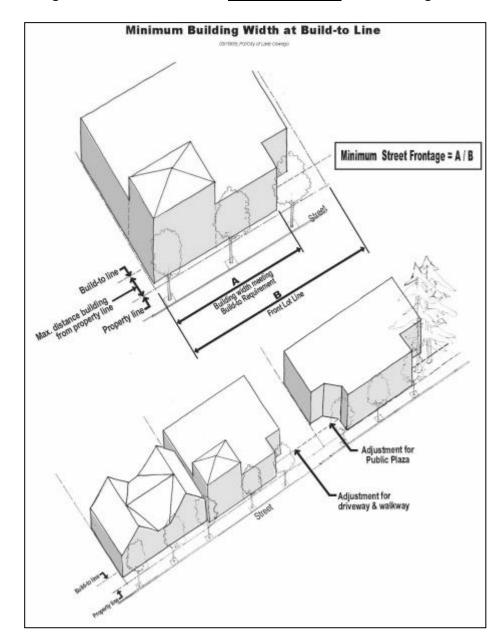


Figure 50.05.007-N: Minimum Building Width at Street Frontage

(2) If a parcel has more than two street frontages designated Streetfront Environment, the minimum <u>building width at</u> street frontage requirement applies to two frontages only. On through lots where two street frontages are designated Streetfront Environment and only one building is proposed, the minimum <u>building width at</u> street frontage requirement applies to the frontage to which the building is oriented. In all cases, the minimum <u>building width at</u> street frontage shall be met on the street with the highest street classification (i.e. an arterial street over a local street) subject to Storefront Environment standards set forth in this Section.

(3) Buildings shall meet minimum street <u>building width at street</u> frontage requirements set forth in Table <u>50.05.007-5</u> below.

TABLE 50.05.007-5: MINIMUM BUILDING WIDTH AT STREET FRONTAGE STANDARDS				
Zone Districts and Village Transition Areas (VTA)	Frontage Conditions	Minimum <u>Building</u> <u>Width at</u> Street Frontage	Minimum <u>Building Width at</u> Street Frontage with Shared Access	
GC, OC/R-3 – General Commercial	All	75%	60%	
NC/R-0 – Neighborhood Commercial	All	60%	50%	
VTA 1	All	60%	50%	
VTA 2	All	60%	50%	
VTA 3	All	75%	60%	

- (4) Exceptions to the minimum <u>building width at street</u> frontage requirement are allowed as follows:
 - (a) The minimum street frontage requirement can be wWaived when a public plaza or outdoor dining /seating area is provided in accordance with LOC <u>50.05.007.5.i</u>.iii, Public Plaza or LOC <u>50.05.007.3.c.ii</u>, Exception Remodels Under <u>750 sq. ft.</u>, or when the village commons or a gathering place is provided in accordance with LOC <u>50.05.007.7.c.</u>
 - (b) The minimum street frontage requirement may be r<u>R</u>educed proportionally where a pedestrian walkway of not less than 5 ft. in width is provided along a driveway, or to the degree necessary a driveway is for one or more of the following features, when required pursuant to City standards otherwise permitted on the site:
 - (i) 24-ft. wide driveway.
 - (ii) 5-ft. wide pedestrian walkway along driveway.

iii. Minimum Height at Streetfront

<u>Purpose:</u> This standard eEnsures buildings contribute to a sense of enclosure along the street <u>by</u> <u>complying with Table 50.05.007-6 Minimum Height at Streetfront Standards.</u> Minimum height standards are illustrated in Figure <u>50.05.007-0</u>: Minimum Height Standards.

TABLE 50.05.007-6: MINIMUM HEIGHT AT STREETFRONT <u>STANDARDS</u>			
Boones Ferry Road frontage	27 ft.		
All other street frontages	18 ft.		
Note: A minimum of 50% of the building width meeting the build-to requirement shall have a minimum depth of 20 ft. That portion of the building shall meet the minimum height requirements set forth in this table.			

A minimum of 50% of the building width meeting the build-to requirement shall have a minimum depth of 20 ft. That portion of the building shall meet the minimum height requirements set forth in Table 50.05.007-6, above.

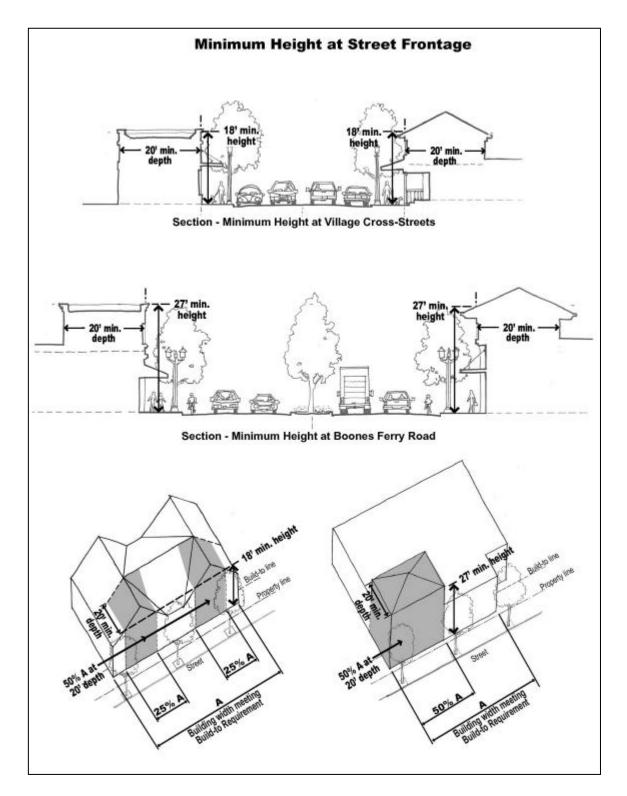


Figure 50.05.007-O: Minimum Height Standards

5. BUILDING DESIGN STANDARDS

a. Purpose

The design standards in this section are intended to promote a variety of architectural styles, encourage pedestrian access and use of streets and gathering places, and foster compatibility with existing structures of good design and adjacent residential neighborhoods.

b. Style and Character

i. Design Diversity

In order to avoid monotony of design in relation to surrounding development and create an eclectic mix of architectural styles and forms, no one dominant architectural style shall be repeated within 500 ft. of the site, as measured from either side of the site within the overlay.

ii. Permitted Architectural Styles

The permitted architectural style shall be provided in accordance with the Building Design Section of the Lake Grove Village Center Design Handbook. Until the adoption of the Lake Grove Village Center Design Handbook, permitted architectural styles within the District and its subareas are those set forth below as described in Architecture, Oregon Style, by R. Clark (Professional Book Center, Inc., Portland, Oregon, 1983):

- (1) Permitted throughout the village center: Italianate, Arts and Crafts/Art Nouveau, Oregon Rustic/National Park, Colonial/Georgian and Cape Cod.
- (2) Additional styles permitted in the GC zone without VTA overlay only: Italian Renaissance, Chicago School/Sullivanesque, Art Deco/Modern, and Richardsonian Romanesque.
- (3) Additional styles permitted in VTA 1, VTA 2 and VTA 3: Gothic Revival, Northwest Regional, Tudor/Jacobethan.
- (4) Additional styles permitted in VTA 1: Classic Revival, Second Empire (no mansard), Prairie School, Bungalow and Craftsman, and Colonial.

iii. Composition

Building components, such as windows, doors, eaves and parapets shall be of proper proportion and be placed in relationship to one another as determined by the architectural style of the building.

c. Design Features

i. Purpose: The purpose of these standards is to create building and site designs that include complex massing, balanced composition of architectural forms and elements, and richly textured and visually engaging facades.

ii. Plants and Architecture

- (1) Provide and integrate landscaping with architecture. This shall be accomplished by providing native or naturalized drought-tolerant vegetation in planters, borders, plazas, courtyards and in pots and/or planter boxes.
- (2) To help define a building's base and proportion relationships and create human-scale development, the landscape plan shall incorporate shrubs and tree species that will obtain a mature height of a minimum of 36 in. and 20 ft., respectively.

iii. Complementary Elements

Design site amenities and building elements according to the permitted architectural style chosen for the building.

d. Pedestrian Features

Purpose: These standards facilitate pedestrian access and create viable pedestrian scale streetscapes and public places inclusive of pedestrian ways, parking areas, interior courtyards and public and private outdoor areas designed to foster the comfort and enjoyment of pedestrians and other users.

i. Orientation and Entrances

- (1) Buildings shall be located within 30 ft. of a public street. Buildings located on sites adjacent to one or more transit streets shall be located within 30 ft. of at least one transit street.
- (2) Buildings within 30 ft. of one or more public streets shall have a <u>primary</u> public entrance directly from at least one public street. Buildings located on sites adjacent to one or more transit streets shall have a <u>primary</u> public entrance directly from at least one transit street.
- (3) Exceptions are permitted where locating the building as set forth in this standard is prevented by topographic constraints, existing natural resources, or where, in multi-building complexes, the configuration of the lot prevents locating all buildings within 30 ft. of a public street.
- (4) Entries shall be sheltered and emphasized through the use of canopies, overhangs, awnings, or arcades.

- (5) Primary public entrances shall be accessed directly from a sidewalk adjacent to a public street or public plaza.
- (5) Additional standards for public entrances apply as follows: Requirements set forth in the Urban Village section apply at locations identified as Urban Village on Figure <u>50.05.007-D</u>: Village Character Map. Requirements set forth in the Streetfront Environment standards apply at locations identified as Transitional Streetfront Environment – Campus Street Orientation designations on Figure <u>50.05.007-D</u>: Village Character Map.

ii. Walkways

Walkways shall be continuous, direct and free of barriers such as poles or other obstructions.

iii. Windowson Upper Stories

- (1) Upper stories shall provide a minimum of 30% fenestration.
- (2) .Tinted glass shall not be used.

iv. Rain Protection

- (1) Rain protection shall be provided over windows and entrances along the frontage of any building that abuts a public sidewalk or walkway.
- (2) Rain protection shall consist of awnings, canopies or projections extending a minimum of four ft. or half the width of the sidewalk or walkway from face of window wall. whichever is greater.
- (3) Vinyl awnings are prohibited.

v. Lighting

Purpose: Exterior building lighting is intended to enhance the pedestrian environment as part of the architectural concept.

Lighting shall comply with the following standards:

- (1) Lighting shall be shielded, directed downward, and designed to prevent glare on abutting properties.
- (2) <u>Structures Buildings</u> shall provide exterior, pedestrian-oriented building lighting along the ground floor. Along street- and public space-facing ground floors, lighting shall be installed at least every 30 ft. Pedestrian oriented lighting may include sconce or gooseneck features.

(3) When ground floor architectural bays are provided, exterior ground floor building lighting shall be integrated into the sign frieze and/or into the column/pilaster/post of the ground floor architectural bays, as illustrated in Figure 50.06.001-O.

e. Roofs

- i. Flat roofs are permitted only on buildings greater than 20 ft. in height.
- ii. Mansard roofs are prohibited.
- iii. Ecoroofs and roof gardens are encouraged to reduce runoff, promote rooftop storage for reuse, enhance building cooling and insulation, reduce heat island effect offering area cooling, improve air quality and to provide an attractive outdoor area.
- **<u>iiiiiv</u>**. Buildings shall be designed and constructed with roof angles, overhangs, flashings, and gutters to direct water away from the structure, pedestrian walkways and outdoor public places.

f. Screening and Sound Buffering

- i. Mechanical equipment shall be screened from view. Rooftop mechanical equipment shall not be visible from any adjacent street, sidewalk or any open space adjacent to the proposed development. To accomplish this, rooftop equipment shall be screened by either a parapet or architectural screen that is at least as tall as the equipment, or by setting back the equipment from the roof edges sufficient to restrict views of the equipment. Solar or other renewable energy systems are exempt from this screening requirement provided they meet the requirements in LOC 50.04.003.b. Other mechanical equipment located on site shall also be screened from view. Exception: the equipment may be placed underground.
- **ii.** Buildings and associated noise generating equipment shall be designed and constructed to limit normal noise levels on adjacent residential property to 60 dBA as measured at the property line.

g. Materials

i. Wall Materials

Materials for walls and exterior components shall be complementary to each other, durable, and of high quality. The following exterior materials are not allowed:

- (1) EIFS or other synthetic stucco material,
- (2) Metal and vinyl siding,
- (3) T-111 Paneling,

- (4) Composite wood siding of any kind,
- (5) Mirrored or tinted glass,
- (6) Standard form concrete block (not including split faced, colored or other block designs, which mimic stone, brick or other masonry),
- (7) Back-lighted fabrics,
- (8) Plastic and fiberglass except when used to replicate styles, or
- (9) Corrugated metal.

ii. Roof Materials

Materials used for roof repairs (patching) must not be readily visible. Roof materials shall be limited to:

- (1) Slate,
- (2) Tile,
- (3) Shakes or wood shingles, or
- (4) Synthetic materials (e.g. concrete, pressed wood products, metal or other materials) that are designed to and do appear to be slate, tile, shake or wood shingles.

iii. Colors

- (1) Natural or subdued building color shall be used for expanses of exterior surfaces.
- (2) Bright or primary colors for entire walls or roofs shall not be allowed.
- (3) Bright accent trim colors shall be limited to trim.

h. Special Design Standards within the Village Transition Area

<u>Purpose:</u> These standards apply within the Village Transition Area to foster<u>Foster</u> visual interest and compatibility between adjoining buildings through appropriate scale relationships within the Village <u>Transition Area</u>.

The standards below apply within the Village Transition Area:

i. Design Elements

One or more of the following design elements shall be featured:

- (1) Balconies and/or dormers to provide distinct and separate areas.
- (2) Setting back parts of the facade to reduce the mass of large buildings or row of attached dwellings.
- (3) Dormers to break up roof expanses.

ii. Building Planes Facade Design

Building planes <u>Facades</u> shall be broken down both horizontally and vertically through the use of one or more of the following design elements:

- (1) Recessed or projected entries and porches.
- (2) Combinations of roof gables and eaves facing on public sides of the building(s).
- (3) A minimum of 60% of all upper story windows shall be vertically oriented, with a minimum vertical to horizontal ratio of 1.5:1. This requirement applies to individual windows, as opposed to grouped window arrays.
- (4) Balconies.
- (5) Wall planes offset a minimum depth of two ft.

i. Urban Village Design Areas

<u>Purpose:</u> Urban Village Design Area standards promote <u>Promote</u> scale and design features appropriate to a street orientation within the Urban Village Design Area.

The following standards apply for properties to lots designated Urban Village on Figure 50.05.007-D: Village Character Map.

i. Primary Entrances

- (1) Primary public entrances shall be oriented to a public street and located within 30 ft. of that public street. Buildings located on sites adjacent to one or more transit streets shall have primary public entrances oriented to at least one transit street and located within 30 ft. of that transit street.
- (2) Primary public entrances shall be accessed directly from a sidewalk adjacent to a public street or public plaza.

ii. Side and Rear Wall Planes

A side or rear <u>elevationfacade</u> greater than 750 sq. ft. in area must be divided into distinct wall planes of 750 sq. ft. or less, resulting in a change in plane surface which projects or recedes at least two ft. from the <u>adjacent abutting plane</u>, for a length of at least six ft.

iii. Public Plaza

A public plaza may be provided as set forth below:

- (1) Build-to Line and Minimum Street Frontage standards may be changed or waived as set forth in Streetfront Environment, LOC <u>50.05.007.4.e</u>.i and ii when a public plaza meeting the requirements set forth in this subsection is provided.
- (2) Property within a public plaza easement area may be counted as landscaped area for compliance with requirements set forth in LOC <u>50.05.007.6.k</u>.i, Minimum Area Requirement, when a public plaza meeting the requirements set forth in this subsection is provided.
- (3) Property within a public plaza easement area may be included as lot area for compliance with lot coverage standards set forth in LOC <u>50.05.007.4.c</u>.ii, Lot Coverage, when a public plaza meeting the requirements set forth in this subsection is provided.
- (4) Easement. A public access easement shall be granted for a public plaza prior to issuance of the permit, the property owner shall furnish documentation of agreement to defend, indemnify and hold harmless the City, its officers and employees, from any claims for damages to property or injury to the satisfaction of the City Manager. Prior to the issuance of the permit, the property owner shall furnish documentation of insurance to the satisfaction of the City Manager.
- **(5)** When provided, the_public plaza shall meet the minimum requirements set forth below and illustrated in Figure <u>50.05.007-P</u>: Public Plazas:
 - (a)(1) The public plaza shall <u>abut</u>be adjacent to the public right-of-way.
 - (b)(2) Buildings shall front on the public plaza with windows and entrances facing the plaza.
 - (c)(3) The plaza shall be a minimum of 500 sq. ft. in size and shall have an open area with dimensions adequate for inscribing a 13-ft. diameter circle. No plaza dimension shall be less than ten ft.
 - (d)(4) The public plaza shall be primarily hardscape and incorporate small unit pavers or concrete scoring based on a maximum two-ft. by two-ft. unit. Trees and plants shall be

provided in planting beds, raised planters or pots. One seat shall be provided for every 75 sq. ft. of plaza area.

(5) Easement: A public easement shall be granted for public plaza use prior to issuance of the permit; the property owner shall furnish documentation of agreement to defend, indemnify and hold harmless the City, its officers and employees, from any claims for damages to property or injury due to the acts or omissions of the owner to the satisfaction of the City Manager. Prior to the issuance of the permit, the property owner shall furnish documentation of general liability insurance of not less than \$2M / \$3M (2016 amount) or greater amounts as indexed in manner provided by ORS 30.272(4), unless lesser amounts accepted by the City Manager.

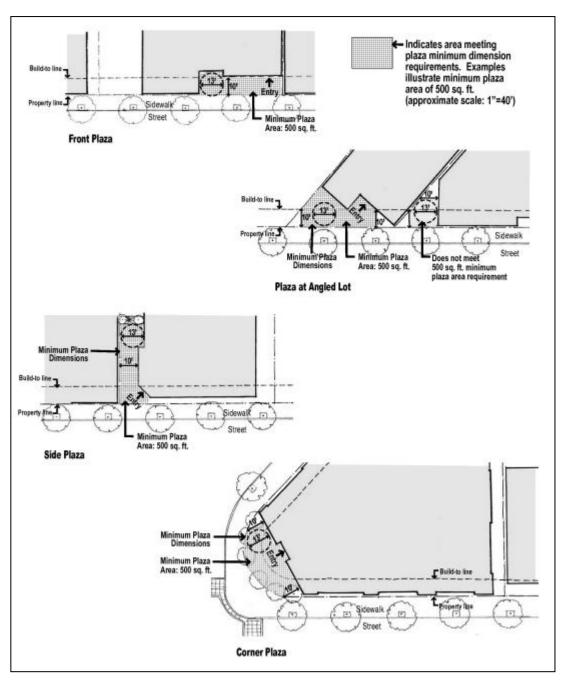


Figure 50.05.007-P: Public Plazas

ijiv. Urban Village Standards for Buildings Exceeding 35 Ft. or Two and One-half Stories

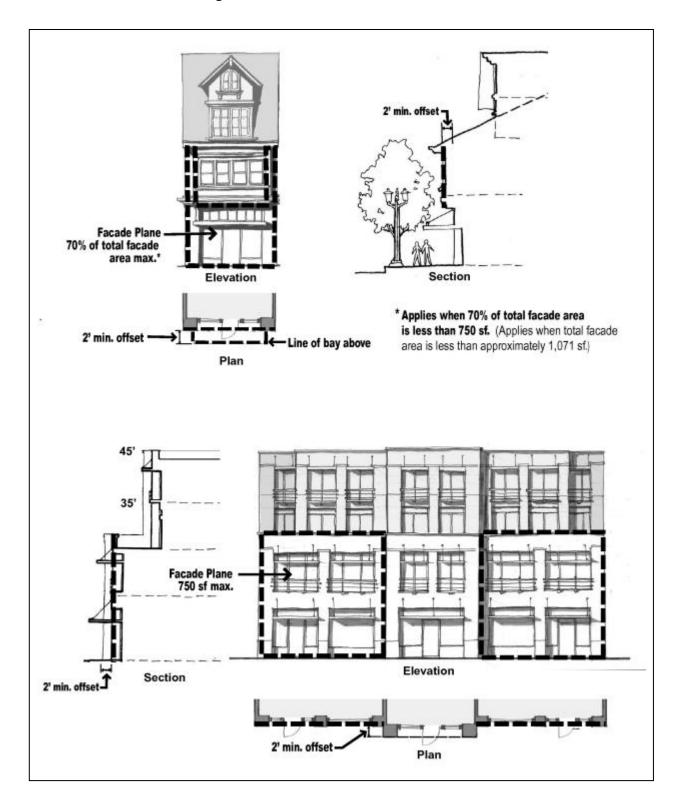
<u>Purpose:</u> These standards e<u>E</u>nsure taller buildings contribute appropriately to village scale and character.

The following standards apply to buildings exceeding 35 ft. or two and one-half stories at locations designated Urban Village on Figure <u>50.05.007-D</u>: Village Character Map.

(1) Facade Plane

This standard requires building mass be broken up at a scale that contributes to a pedestrian environment and is compatible with adjacent residential neighborhoods as illustrated in Figure 50.05.007-Q: Facade Planes.

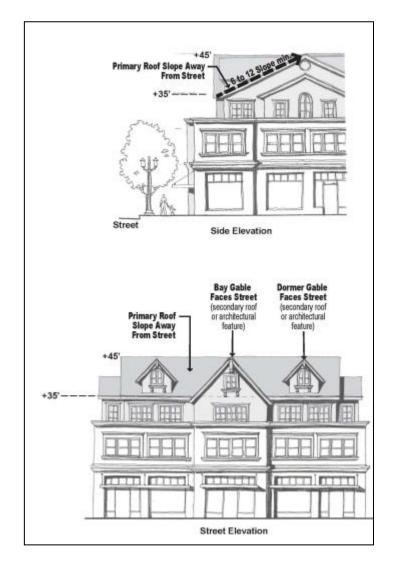
- (a) Applies when total facade area is less than approximately 1,071 sq. ft.
- **(b)** Building facades must be divided into distinct planes not to exceed an area of 750 sq. ft. or 70% of the total facade area, whichever is smaller.
- (c) Wall planes shall be a minimum width of six ft. and offset by a minimum of two ft.

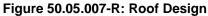


(<u>1</u>2) Design Features

A minimum of two of the following design features are required. (Desired design features are to be illustrated in the Building Design section of the Lake Grove Village Center Design Handbook.)

(a) <u>A</u>The primary roof <u>with a</u>shall feature a minimum six-to-12 slope. The roof shall <u>that</u> slopes up and away from any abutting street. Secondary roof forms or architectural features such as dormers or bays may form gables facing the street. <u>Mansard roofs are prohibited</u>. This requirement is illustrated in Figure 50.05.007-R: Roof Design.





- (b) To create the perception of depth and establish a human scale, <u>upper story facades shall</u> <u>incorporate</u> true windows, transparent balconies or entries to balconies or terraces-shall be incorporated into upper story facades.
- (c) A public plaza shall be provided. The plaza shall meeting the requirements of LOC <u>50.05.007.5.i</u>.iii, Public Plaza. Notwithstanding these requirements, provided that the plaza is shall not be less than 4% of the lot area or 500 sq. ft., whichever is greater.

vii. Storefront Window

<u>Purpose:</u> These standards provide a storefront appearance on the ground floor and maximize the opportunity for window shopping.

The following standards apply to commercial development at Urban Streetfront Environment locations designated as Storefront Window Boones Ferry Road and Storefront Window Village Cross Street on the Village Character Map, Figure 50.05.007-D: Village Character Map.

- (1) Awnings, signs, and lights shall be designed to define first floor retail frontage.
- (2) A break in <u>building wall planes</u>, awning or canopy construction shall occur at intervals not to exceed 25 ft. as shown in Figure <u>50.05.007-S</u>: Awnings.



Figure 50.05.007-S: Awnings

- (3) A minimum of <u>65</u>80% (linear measurement) of the exterior ground floor that is at or within the build-to line or abutting public plazas shall be designed as storefront with display-windows and entry features.
- (4) Display windows shall have a depth of at least two ft. and shall allow views of activity within the building. Views of activity within the building are preferred but views to displays that illustrate activities related to uses within the building may be permitted.

(4)(5) The bottom edge of windows abutting the street frontage and public plazas shall be constructed no more than 30 in. above and no less than 12 in. above grade.

j. Village Campus Design Areas

<u>Purpose:</u> Village Campus standards prevent smooth, undifferentiated facades and promote building scale and articulation appropriate for development in a context where buildings are set back from the street. Buildings may be set back from the street to create a desired character as set forth in the Transitional Streetfront Environment and Park Streetfront Environment. Existing site conditions may call for flexibility in building location to preserve natural features including trees, riparian areas, and topography, and to accommodate curved or limited street frontage or large through-lots adjacent to Kruse Way.

The following standards apply <u>on</u> for properties <u>lots</u> designated Village Campus on the Village Character Map, Figure <u>50.05.007-D</u>: Village Character Map.

i. Offset Architectural Elements

Buildings facades shall be designed to reference the rhythm of structural elements by offsetting bays, projecting columns, recessing windows, or providing a combination of two of these features. The minimum depth for offset structural elements shall be 18 in. for a minimum width of 18 in.

ii. Base, Middle and Top

Buildings facades shall be designed to reference a human scale by providing a tripartite facade division with an identifiable base, middle and top.

iii. Significant Shadows

Building facades shall be designed to create significant shadows. Two or more architectural features exhibiting a minimum depth of 12 in. shall be employed. Architectural features may include, and are not limited to, the following:

- (1) Bays,
- (2) Columns,
- (3) Recessed windows,
- (4) Eaves,
- (5) Cornices,
- (6) Awnings,

- (7) Balconies,
- (8) Trellises,
- (9) Window boxes, and
- (10) Articulated building base.

6. SITE PLANNING STANDARDS

a. Purpose

The site planning standards in this section ensure:

- i. Enhanced safety, convenience and attractiveness for walking, transit use and bicycling;
- **ii.** Safe access to businesses and minimizing pedestrian conflicts through driveway consolidation over time;
- iii. Redevelopment and growth potential through shared off-street parking arrangements;
- iv. Village identity based on the conservation and promotion of large-scale native trees such as Douglas fir and unified landscape and streetscape elements;
- v. Buffering of low density residential neighborhoods from abutting dissimilar uses; and
- vi. An attractive, quality environment to foster community pride and attract desired development and uses.

b. Streets, Circulation, and Pedestrian System

<u>Purpose</u>: In accordance with the standards set forth below, dDevelopment shall occur in a manner to ensure the phased construction of the planned circulation and access system and in no circumstance shall prevent the development of a cohesive access and circulation system.

i. Transportation System Maps

Streets, alleys, traffic controls, crossings, pedestrian, bike and transit facilities, parking facilities and access shall be provided and developed in accordance with the following:

(1) Figure 50.05.007-T: Street Network Map;

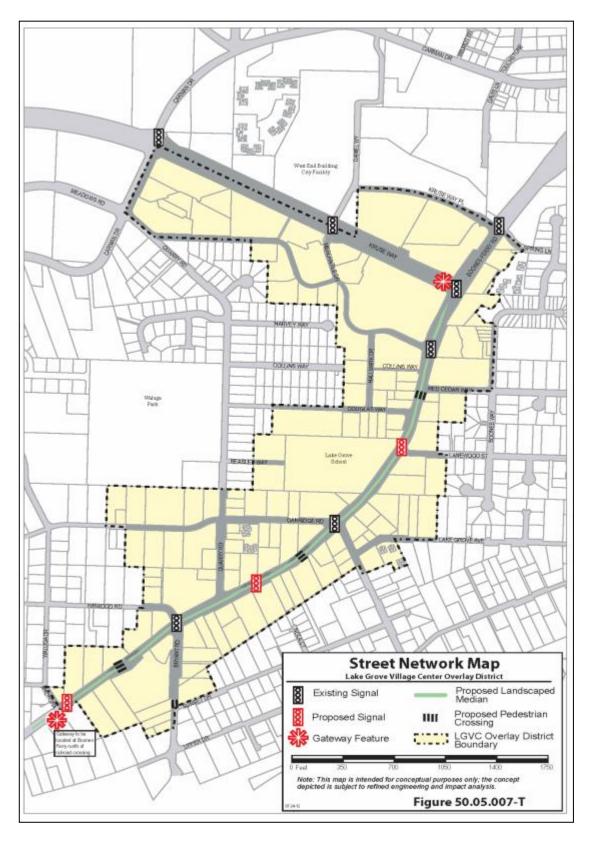


Figure 50.05.007-T: Street Network Map

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(2) Figure <u>50.05.007-U</u>: Parking Facilities and Access Coordination Map;

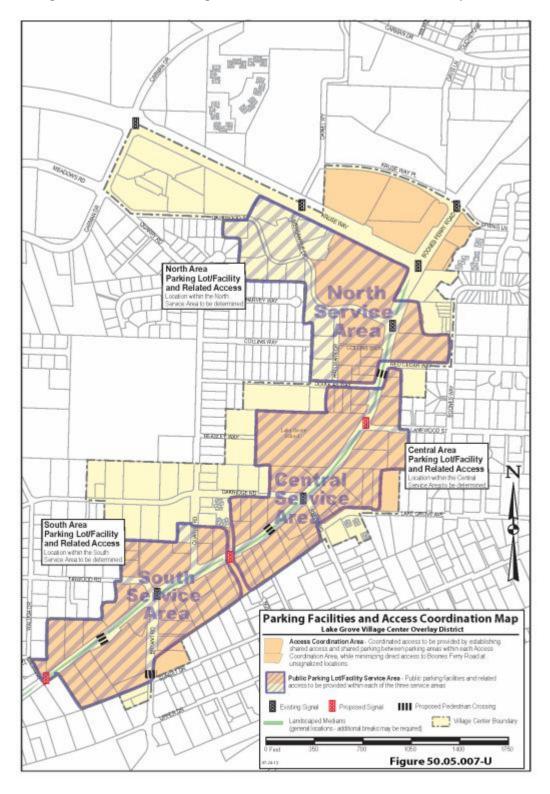


Figure 50.05.007-U: Parking Facilities and Access Coordination Map

(3) Figure <u>50.05.007-V</u>: Pedestrian Facilities and Streetscape Map;

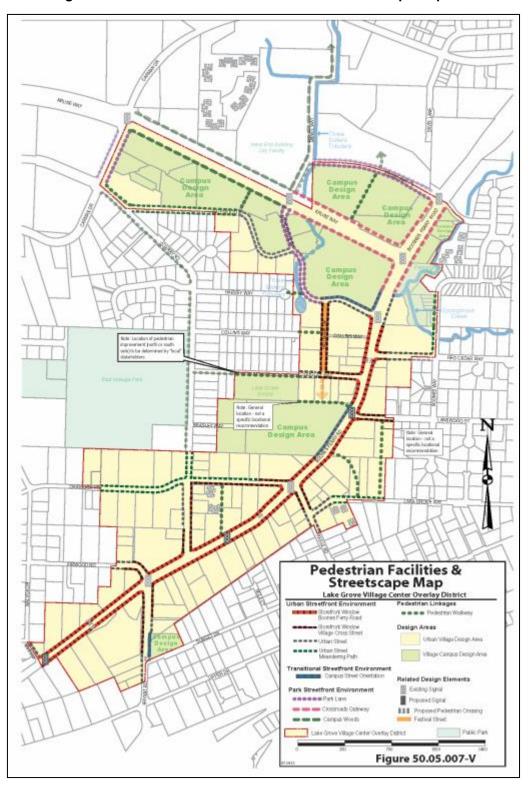


Figure 50.05.007-V: Pedestrian Facilities and Streetscape Map

(4) Figure 50.05.007-W: Bike Facilities Map; and

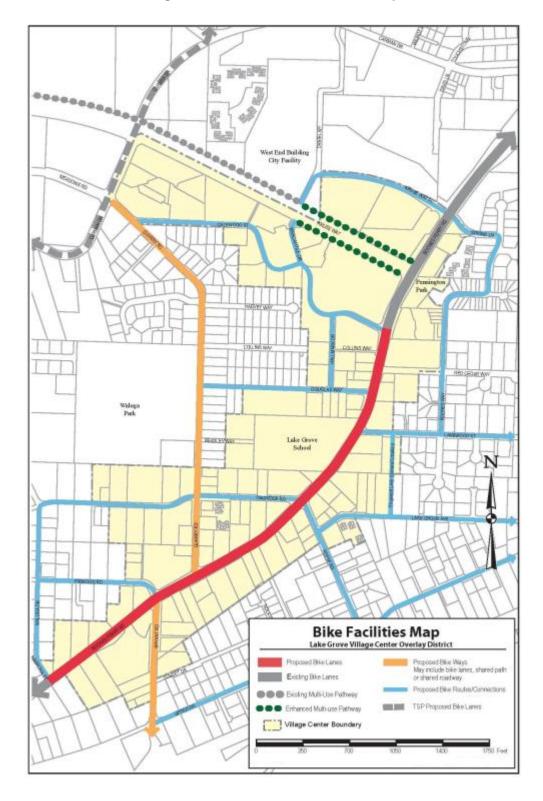


Figure 50.05.007-W: Bike Facilities Map

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(5) Figure <u>50.05.007-X</u>: Transit Map.

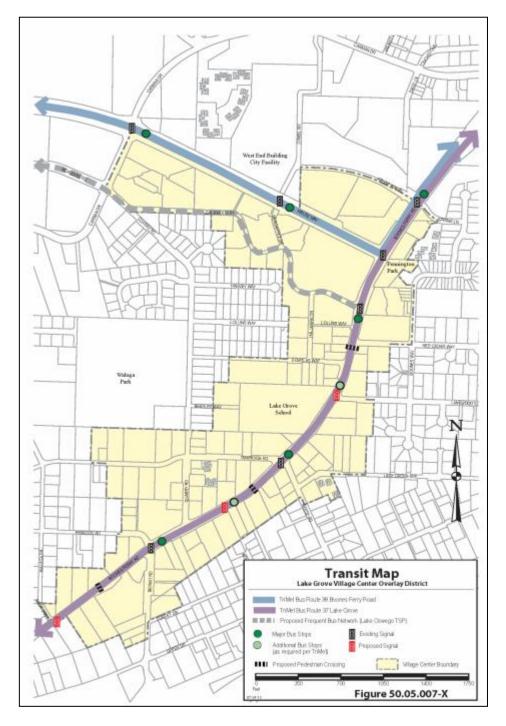


Figure 50.05.007-X: Transit Map

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ii. Design Standards

Design treatment for auto, bike and pedestrian facilities shall be in accordance with minimum standards set forth in LOC Article 42.03, Street Design Standards, LOC Article 42.08, Sidewalks, and with the standards below at applicable locations.

(1) Generally Applicable Standards

The following design standards shall be generally applicable.

- (a) Street sections, below, illustrate streetscape standards for locations designated Storefront Window on Boones Ferry Road. Additional streetscape standards apply. Refer to Pedestrian Facilities and Streetscape Map, Figure <u>50.05.007-V</u> and applicable standards for sidewalk dimensions and streetscape treatments, Figures <u>50.05.007-BB</u> through LOC <u>50.05.007-II.</u>
- (b) Final street section features, dimensions and required right-of-way to be determined by City Engineer.
- (c) Boones Ferry Typical Mid Block Street Section;

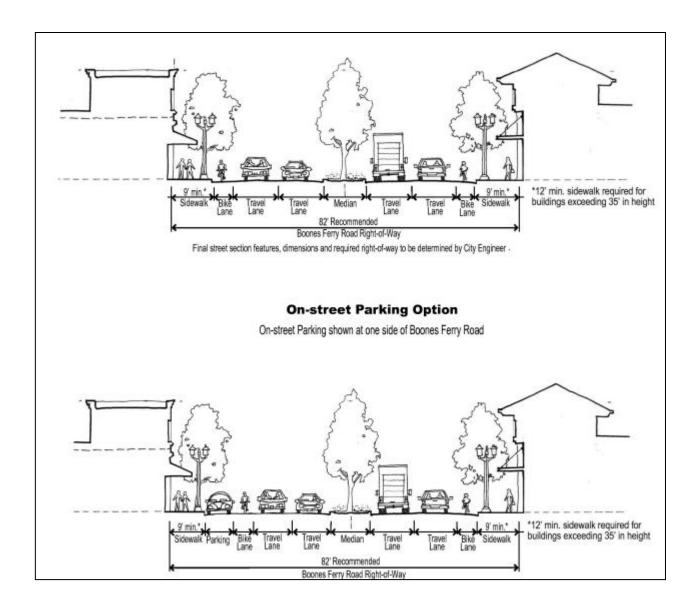


Figure 50.05.007-Y: Boones Ferry Typical Mid-Block Street Section

(d) Boones Ferry Typical Signalized Intersection Street Section; and

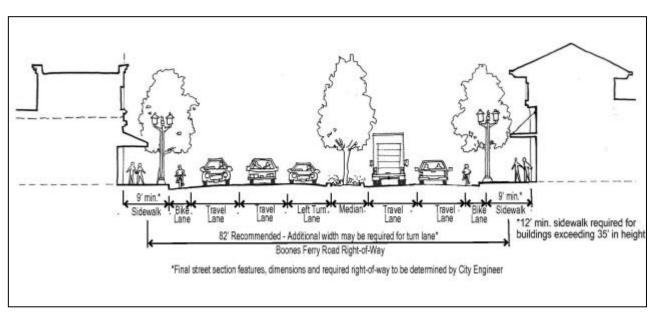
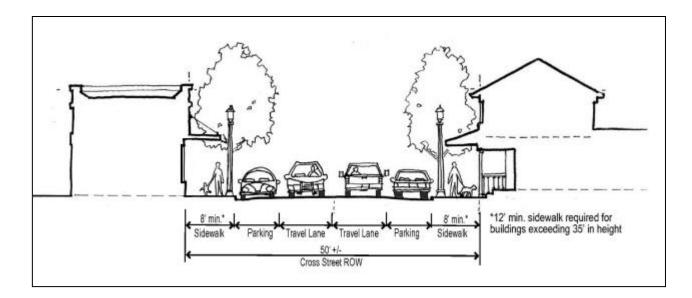


Figure 50.05.007-Z: Boones Ferry Typical Signalized Intersection Street Section

(e) Village Cross Street Section at Transition to Boones Ferry Road.

Figure 50.05.007- AA: Village Cross Street Section at Transition to Boones Ferry Road



(2) Map Specific Locations

(a) The Pedestrian Walkway standard, Figure <u>50.05.007-BB</u>: Pedestrian Walkway, applies at locations identified on Figure <u>50.05.007-T</u>: Street Network Map or Figure <u>50.05.007-V</u>: Pedestrian Facilities and Streetscape Map. Final features, dimensions and required easement or right-of-way to be determined by City Engineer.

(i) Two Pedestrian Walkway Options

- (A) Meandering Path comprised of an asphalt walkway, trees and landscaping.
- (B) Urban Sidewalk comprised of a concrete sidewalk, trees and landscaping.

(ii) Dimensions

- (A) The total width of the path/sidewalk, trees and landscaping shall be 12 ft. minimum and 16 ft. maximum.
- (B) The width of the path/sidewalk shall be six ft. minimum and eight ft. maximum.

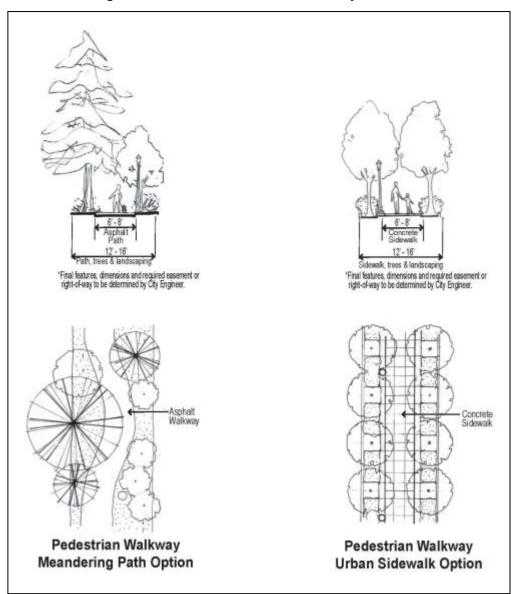


Figure 50.05.007-BB: Pedestrian Walkway

(b) Urban Streetfront Environment

The following standards apply to locations designated Urban Streetfront Environment on the Pedestrian Facilities and Streetscape Map, Figure <u>50.05.007-V</u>. Final features, dimensions and required right-of-way to be determined by City Engineer.

(i) Storefront Window Streetscape

Sidewalks and streetscape elements shall be provided as set forth in design standards for Storefront Window Streetscape, Figure 50.05.007-CC, below, and in the Lake Grove Village Center Design Handbook. Until the adoption of the Lake Grove Village Center

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Design Handbook, equivalent standards of the West Lake Grove Design District shall apply.

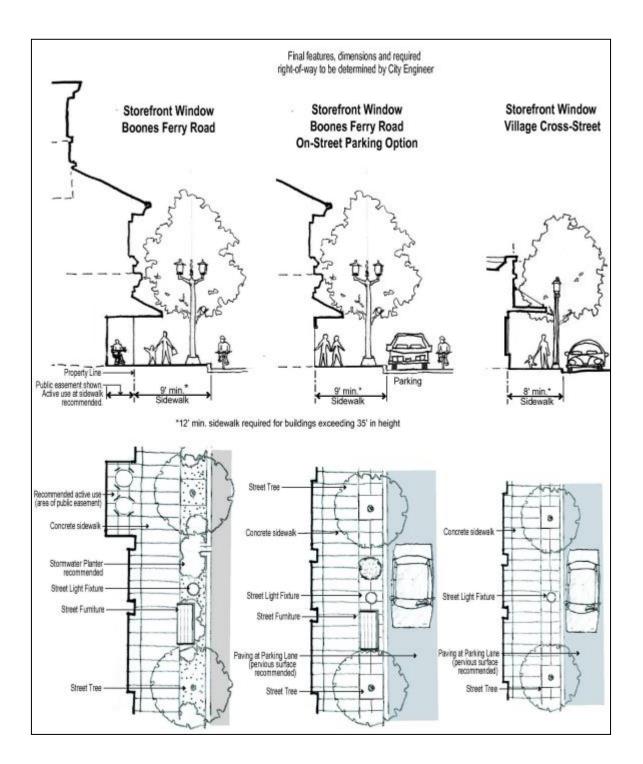


Figure 50.05.007-CC: Storefront Window Streetscape

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TABLE 50.05.007-7: STOREFRONT SIDEWALK ZONE REQUIREMENTS					
Location	Sidewalk Width	Pedestrian Thru Zone	Street Tree Stormwater Planting/Furniture Zone		
Boones Ferry Road	12 ft.	6 ft. min.	4 ft. – 6 ft.		
Boones Ferry Road	9 ft.	5 ft. 6 in. min.	3 ft. 6 in.		
Village Cross Street	8 ft.	4 ft. – 5 ft. min.	3 ft. – 4 ft.		

(ii) Storefront Sidewalk Zone Requirements

(A) Pedestrian Thru Zone

The area of the sidewalk where pedestrians can pass through without obstructions side-by-side.

(B) Furnishing/Stormwater Planter Zone

The space along the length of the sidewalk for elements encouraging pedestrian use of the right-of-way. Such elements include landscaping including street trees and stormwater, planters. Lighting, benches, bike racks and transit shelters. In all pervious areas, landscaping must be soft (grass or ground-cover). Vertical elements such as "loading zone" signs, permanently located in the Furnishing zone must be set back 18 in. from the curb.

(C) A.D.A Requirements

Curb ramps, access routes, warning strips, etc., must conform to the requirements of the Americans with Disabilities Act as determined by the City Engineer.

Pedestrian Thru Zone

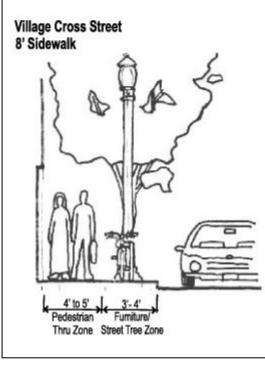
The Pedestrian Thru Zone is the area of the sidewalk where pedestrians can pass through without obstructions side-by-side.

Furnishing/Stormwater Planter Zone

The Furnishing/Stormwater Planter Zone is the space along the length of the sidewalk for elements encouraging pedestrian use of the right-of-way. Such elements include landscaping including street trees and stormwater, planters, lighting, benches, bike racks and transit shelters. In all pervious areas, landscaping must be soft (grass or ground-cover). Vertical elements such as "loading zone" signs, permanently located in the Furnishing Zone must be set back 18" from the curb.

A.D.A. Requirements

Curb ramps, access routes, warning strips, etc., must conform with the requirements of the Americans with Disabilities Act as determined by the City Engineer.



 Final features, dimensions and required right-of-way to be determined by City Engineer

 Soones Ferry Road

 (2' Sidewalk

 (3' Sidewalk

 (4' to 6')

 (5' min.

 (6' min.

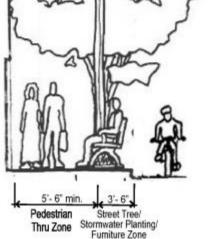
 (7' Sidewalk)

 (7' Sidewalk)

 (7' Sidewalk)

 (7' Sidewalk)

 (7' Sidewalk)



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(iii) Urban Street Orientation Streetscape

At street frontage locations designated as Urban Street or Urban Street Meandering Path on the Pedestrian Facilities and Streetscape Map, Figure 50.05.007-V, sidewalks and streetscape elements shall be provided as set forth in design standards for Urban Street Orientation Streetscape, Figure 50.05.007-EE, and the Lake Grove Village Center Design Handbook. Until the adoption of the Lake Grove Village Center Design Handbook, equivalent standards of the West Lake Grove Design District shall apply.

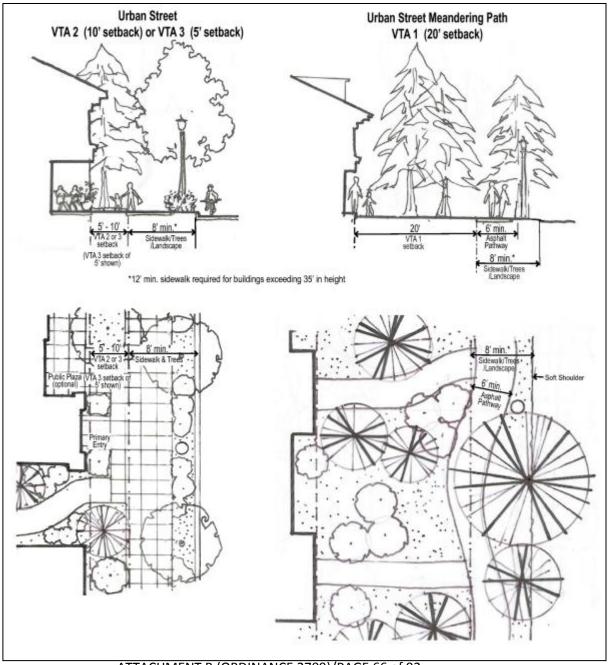


Figure 50.05.007-EE: Urban Street Orientation Streetscape

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(c) Transitional Streetfront Environment

The Campus Street Orientation Streetscape, Figure <u>50.05.007-FF</u>: Campus Street Orientation Streetscape, standard applies to locations designated Transitional Streetfront Environment on the Pedestrian Facilities and Streetscape Map, Figure <u>50.05.007-V</u>.

At street frontage locations designated Campus Street Orientation on the Pedestrian Facilities and Streetscape Map, Figure 50.05.007-V, sidewalks and streetscape elements shall be provided as set forth in design standards for Campus Street Orientation Streetscape, Figure 50.05.007-FF, and the Lake Grove Village Center Design Handbook. Until the adoption of the Lake Grove Village Center Design Handbook, streetscape elements are subject to approval by the review authority.

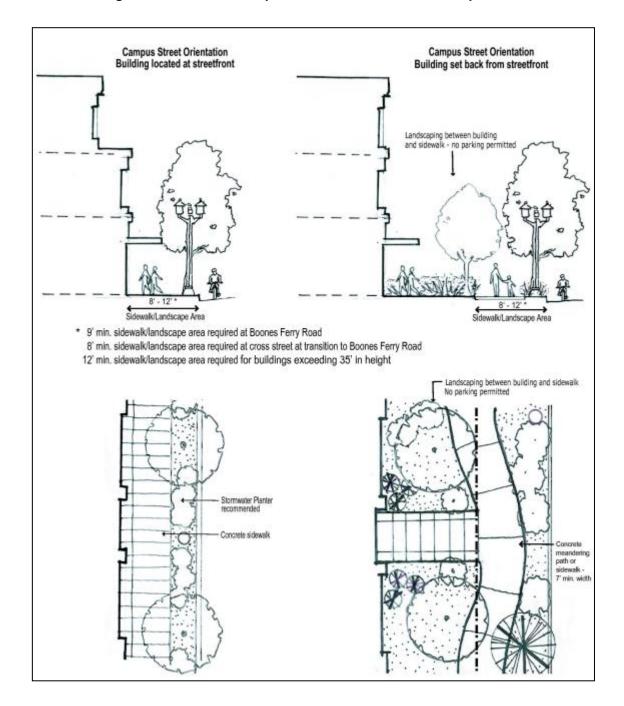


Figure 50.05.007-FF: Campus Street Orientation Streetscape

(d) Park Streetfront Environment

The following standards apply to locations designated Park Streetfront Environment on the Pedestrian Facilities and Streetscape Map, Figure <u>50.05.007-V</u>.

(i) Park Lane Streetscape,

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Sidewalk and streetscape elements shall be provided as set forth in design standards for Park Lane Streetscape, Figure 50.05.007-GG, and the Lake Grove Village Center Design Handbook. Until the adoption of the Lake Grove Village Center Design Handbook, streetscape elements are subject to approval by the Review Authority.

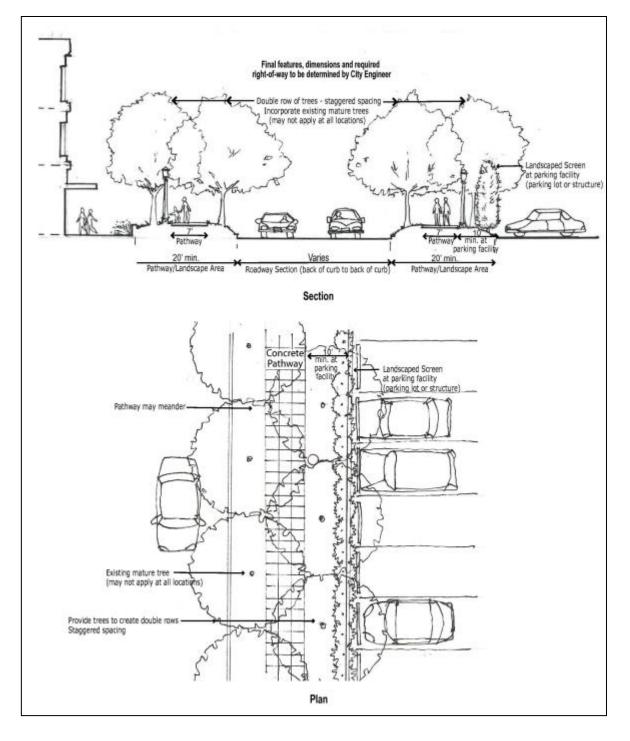


Figure 50.05.007-GG: Park Lane Streetscape

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(ii) Crossroads Gateway Streetscape, and

Sidewalks and streetscape elements shall be provided as set forth in design standards for Crossroads Gateway, Figure 50.05.007-HH, and the Lake Grove Village Center Design Handbook. Until the adoption of the Lake Grove Village Center Design Handbook, streetscape elements are subject to approval by the Review Authority.

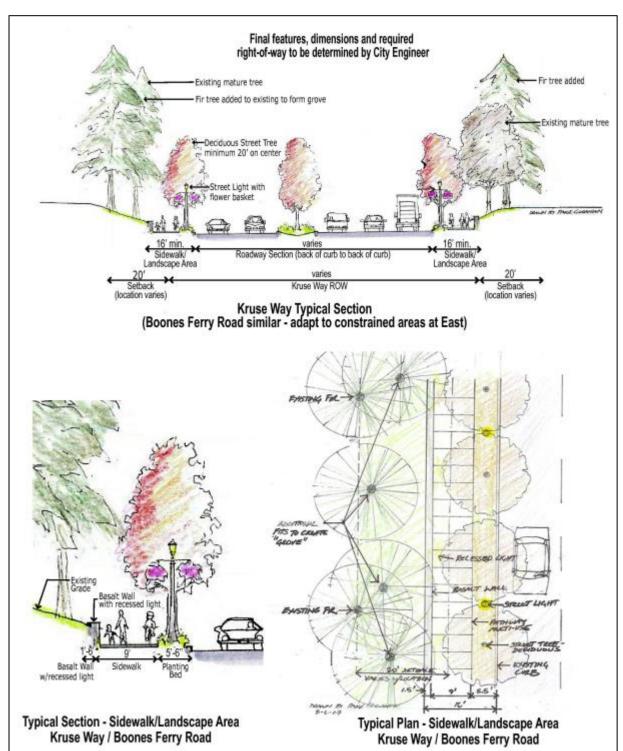
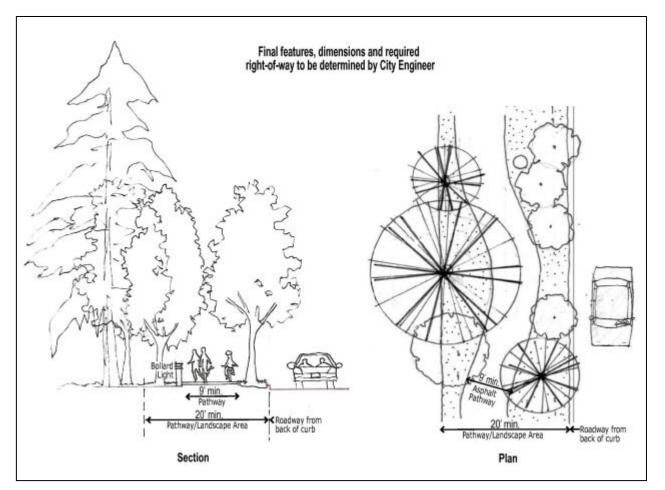


Figure 50.05.007-HH: Crossroads Gateway Streetscape

(iii) Campus Woods Streetscape.

Sidewalks and streetscape elements shall be provided as set forth in design standards for Campus Woods Streetscape, Figure 50.05.007-II, and the Lake Grove Village Center Design Handbook. Until the adoption of the Lake Grove Village Center Design Handbook, streetscape elements are subject to approval by the review authority.

Figure 50.05.007-II: Campus Woods Streetscape



(e) Sidewalks and Pathways

Continuous and connecting hard-surface pedestrian sidewalks and pathways shall be provided in accordance with the locations, designs and widths identified in this section.

(f) Easement

A public easement of sufficient size for required sidewalk or pathway construction and maintenance shall be provided as needed. For any property within this required public easement, the following shall apply:

- (i) Property within the required public easement may be counted as landscaped area for compliance with the standards of this section set forth in LOC <u>50.05.007.6.k</u>, Landscape Requirements.
- (ii) Property within the required public easement may be included as lot area for compliance with lot coverage standards provided in LOC <u>50.05.007.4.c</u>, Lot Coverage and FAR.

(g) Driveways

The number of access points on Boones Ferry Road shall be reduced through the consolidation of driveways as follows:

- (i) Driveway spacing shall not be less than 150 ft. when a center median is present. If a center median is not present, driveway spacing shall not be less than 300 ft.
- (ii) The location of consolidated access shall be determined by the review authority based upon consideration of the following factors:
 - (A) The Street Network Map, Figure 50.05.007-T;
 - (B) The Parking Facilities and Access Coordination Map, Figure 50.05.007-U;
 - (C) The ability to serve multiple properties lots and unified sites;
 - (D) Traffic safety and operational characteristics; and
 - (E) Any approved access master plan for abutting propertieslots and unified sites.
- (iii) Property owners shall construct the consolidated driveway at the time of development, or execute a recordable covenant to share in the cost of the consolidated driveway at such future time when sufficient land area is developed to make driveway consolidation practical.
- (iv) The review authority may approve interim individual driveway access to Boones Ferry Road when circumstances on abutting lots prevent driveway consolidation.
- (v) When abutting lots develop and the location of the permanent shared access is not the same location as the interim access, the interim driveway shall be abandoned and the area landscaped or otherwise integrated into the design of the subject site

under the provisions of this section and other applicable Lake Oswego codes, standards and regulations. Landscaping to be provided within abandoned driveway may be counted toward minimum site landscaping requirements.

- (vi) Driveway consolidation shall require the execution of reciprocal, nonrevocable easements in a form necessary to ensure unimpeded property access and driveway maintenance.
- **Exception Lake Grove Elementary School**: This subsection 6.b.ii(2)(g)(vi) applies to the Lake Grove Elementary School site only when the development and uses are not for a public school.

(h) Abutting Parking Facilities Connections

Driveways and parking aisles shall be designed to create connections to abutting commercially zoned parking facilities within the Lake Grove Village Overlay District when practical. The location of access connections shall be based on the Parking Facilities and Access Coordination Map, Figure <u>50.05.007-U</u>.

(i) Access Coordination – Access Master Plans

When shared driveway access to Boones Ferry Road (as required by LOC <u>50.05.007.6.f</u>) or connections between parking facilities on abutting properties <u>lots or unified sites</u> (as required by LOC <u>50.05.007.6.g</u>) are dependent on redevelopment of the abutting property, the required shared access and/or property connections may be temporarily postponed provided that an access master plan is approved by the review authority.

(i) Access master plans shall:

- (A) Be filed with the site plan for development or redevelopment;
- (B) Illustrate how shared access and vehicular connections will be provided with abutting commercially zoned properties lots or unified sites;
- **(C)** Illustrate how pedestrian connections will be provided with all abutting commercial and medium density or high density zoned property;
- (D) Illustrate how shared access and vehicular connections will not interfere with development or redevelopment of abutting <u>lots or unified sitesproperties</u> in a manner that is consistent with the broad objectives of the Village Center plan and the overlay zone;
- (E) Set forth the timing and conditions under which the access or connection improvements shall be constructed and implemented; and

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- **(F)** Be signed and recorded by the owners of abutting property for which access is being coordinated. In the event that abutting owners refuse to sign the master plan, the applicant shall demonstrate that an effort was made to meet with and coordinate with the abutting owners.
- (ii) Upon approval of an access master plan by the review authority, development or redevelopment on abutting property shall be designed in accordance with the approved access master plan.

c. Parking

- i. Within the Lake Grove Village Center, on-street parking spaces directly abutting a property may be used to satisfy the off-street parking requirements of a business or residential use located on the property.
- **ii.** Driveways and parking aisles shall include pedestrian safety features such as changes in surface material, signage and lighting to alert drivers to the potential presence of pedestrians.
- iii. To maximize development potential, shared off-street parking between different businesses and land uses shall be required when practical as set forth in LOC <u>50.06.002</u>, Parking. <u>Shared</u> <u>parking may be used per LOC 50.06.002.2.a.iv(2)</u>, but is not required. Access to parking facilities shall be based on the Parking Facilities and Access Coordination Map, Figure <u>50.05.007-U</u>.
- **Exception Lake Grove Elementary School:** This subsection 6.b.ii(c)(iii) applies to the Lake Grove Elementary School site only when the development and uses are not for a public school.
- **iv.** Within 500 ft. of a public parking facility, the minimum parking requirement shall be 75% of the total required for each use pursuant to parking requirements in LOC <u>50.06.002</u>.
- v. A <u>10%</u> reduction in required parking shall be <u>allowed for any lot or unified site on which a Village</u> <u>Gathering Place is provided in accordance with LOC <u>50.05.007.7.c.</u>ii, Village Gathering Places.</u>
- vi. For the portion of property purchased or condemned by City after October 4, 2012, for the purpose of improvements to Boones Ferry Road right-of-way that are consistent with the Lake Grove Village Center Plan: If the loss of property eliminates parking, the amount of parking lost may be counted toward meeting the parking standard for building expansions, if applied for within five years of the purchase or condemnation.
- <u>vii. On sites designated as Urban Streetfront Environment or Transitional Streetfront Environment on</u> <u>the Village Character Map, Figure 50.05.007-D, no off-street parking shall be permitted between</u> <u>the front of the primary building and the public right-of-way.</u>

d. Street Lighting

- i. Cobra-head light fixtures are prohibited.
- ii. Lighting shall be shielded, directed downward, and designed to prevent glare.
- iii. Street lighting shall be provided as set forth in LOC <u>50.05.007.7.d</u>, Village Streetfront.

e. Landscape Requirements

<u>Purpose:</u> These standards r<u>R</u>einforce village scale and character, buffer dissimilar uses, and ensure a unified streetscape theme and continuity within the Lake Grove Village Center Overlay District as illustrated in the Village Theme section of the Lake Grove Village Center Design Handbook.

i. Minimum Area Requirement

Landscaping shall be installed on at least 15% of the development site on which buildings are constructed. This is inclusive of area provided in required village gathering places, landscaping required for parking lots, landscaping within any required buffer areas, and landscaping within public plazas and gathering places. Landscaping may include courtyards, raised beds and planters.

- (1) <u>Property within required public easements for sidewalks or pathway construction and</u> maintenance may be counted toward this minimum landscaping requirement.
- (2) <u>A reduction in required landscaping equal to the area set aside for a Village Gathering</u> Place is allowed, up to a maximum reduction of 2,500 sq. ft.
- (3) <u>A reduction by an amount equal to the area located in a Village Commons if the Village</u> <u>Commons is located on privately owned land with an easement for public use.</u>

Exception: For the portion of property purchased or condemned by City after October 4, 2012, for the purpose of improvements to Boones Ferry Road right-of-way that are consistent with the Lake Grove Village Center Plan: If the loss of property limits the ability to expand the existing development, the square footage of the lost property may be counted toward meeting the landscaping standard for building expansions, if applied for within five years of the purchase or condemnation.

ii. Regional Trees and Vegetation

The landscape plan shall incorporate large-scale native northwest trees such as Douglas-fir (*Pseudotsuga menziesii*), western red cedar (*Thuja plicata*), western hemlock (*Tsuga heterophylla*), California incense cedar (*Calocedrus decurrens*), Oregon white oak (*Quercus garryana*) or bigleaf maple (*Acer macrophyllum*). Drought resistant plant materials that are either native or have naturalized to the locale shall be featured. Nuisance and invasive plants, as identified in Appendix 50.11.004-D and the Invasive Tree Species List on file at the Planning

Department, are prohibited. Emphasis shall be on informal, natural arrangements of plant materials that, over time, will form groves of trees around buildings, parking lots, and within required landscaped buffers.

iii. Preserved Vegetation

Existing significant vegetation which is preserved as part of an approved development application shall be counted towards fulfillment of this section.

iv. On-Site Tree Requirements

One tree with a minimum of two to three in. <u>diameter</u> at 54 in. above grade shall be provided for every 500 sq. ft. of landscaped area. Where site conditions warrant, native trees such as Douglas fir, western red cedar, western hemlock, oak and maple of at least two in. diameter in caliper and which reach a mature height of at least 70 ft. shall be planted.

v. Shrub Requirements

At least 15 shrubs of a minimum two-gallon in size for shall be provided for every 500 sq. ft. of landscaped area. All remaining areas shall be treated with suitable mulch applied to a depth of no less than three in.

vi. Irrigation

All landscaping shall be provided with appropriate irrigation. Permanent irrigation may not be required if the applicant demonstrates that drought-tolerant plants have been installed and established.

vii. Street Trees

(1) Location and Spacing

- (a) Street trees shall be provided within or immediately adjacent to the public right-of-way or a public easement at an average of every 30 linear ft. along the entire development site frontage.
- (b) Notwithstanding the requirements in (vii.1.a) of this subsection, above, street trees shall be provided as set forth in LOC <u>50.05.007.6.eb.ii</u>, Design Standards, at locations identified in the Pedestrian Facilities and Streetscape Map, Figure <u>50.05.007-V</u>.
- (2) Street trees shall be a minimum of two in. in caliper, measured 54 in. above grade when planted and shall be from the approved street tree list unless otherwise provided in this section.

- (3) When trees are not planted in a planter strip or landscaped area, tree wells, with approved permeable material that provides a minimum of 12 sq. ft. of surface area, shall be provided for each tree.
- (4) Alternative arrangements to a linear street pattern may be implemented as set forth in LOC <u>50.05.007.6.b</u>.ii at locations identified in the Pedestrian Facilities and Streetscape Map, Figure <u>50.05.007-V</u>, or at the discretion of the review authority.
- (5) Existing preserved trees within 20 ft. of the public right-of-way shall be counted towards fulfillment of this standard. In order to provide for a more natural and informal setting, groupings of trees may be allowed at locations identified in the Pedestrian Facilities and Streetscape Map, Figure <u>50.05.007-V</u>, or at the discretion of the review authority.
- (6) Exemptions from street tree requirements may be granted by the approval authority provided the following conditions exist:
 - (a) Trees would create problems with existing above or underground utilities.
 - (b) Trees would conflict with clear vision requirements, or;
 - (c) There is inadequate space in which to plant trees. However, the approval authority may require the applicant to plant street trees elsewhere within the Village Center in lieu of trees which would normally be required for a specific development. If trees cannot be planted due to inadequate space or line clearance, the commensurate planting of shrubs or small trees more appropriate to the area may be required.

viii. Parking Areas

Landscape standards for parking areas are as follows:

(1) Natural Features

Landscaping shall emphasize naturalistic groupings utilizing plant materials that are either native or have naturalized to the locale.

(2) Buffering and Screening

Landscape design shall buffer and screen off-street parking areas from adjoining adjacent residential uses.

(3) Trees

Trees shall be integral to parking lot design and the overall site plan to provide for aesthetics and shade.

- (a) Special consideration shall be taken to preserve significant trees. Alternative surface treatments and pervious surfaces shall be allowed as appropriate to preserve existing mature trees.
- (b) Trees planted to meet the landscaping requirements for parking lots shall be deciduous shade trees of at least two in. diameter in caliper which reach a minimum mature height of at least 30 ft. and have the canopy and structure necessary to cast moderate to dense shade.
- (c) Where adequate room is available, large scale evergreen trees such as Douglas fir, western red cedar, western hemlock, and incense cedar of at least six to eight ft. in height and reach a mature height of at least 70 ft. shall be incorporated into the landscape theme.
- (d) Exceptions to requirements of (3)(b) and (c) of this section may be allowed for circumstances that limit placement of trees such as overhead lines, underground utilities and confined spaces.

(4) Parking Bays

- (a) Parking areas shall be divided into bays of not more than eight parking spaces. Between and/or at the end of each parking bay there shall be curbed planters of at least five ft. in width.
- (b) Each planter shall contain one shade tree of at least two in. in caliper.
- (c) The planter shall also be planted with appropriate ground cover or shrubs at a rate of two two2-gallon plants for every 20 sq. ft. of landscape area. The intervening area between plantings shall be mulched with an appropriate material to a minimum depth of three in.

(5) Parking Facility Edge Requirements

(a) Parking facilities shall be separated from a driveway, sidewalk, or pathway by a minimum 5-ft. wide landscaped area. Within this area, shade trees shall be planted every 30 ft. Low sitting walls, benches and other pedestrian amenities oriented to sidewalks or pathways may be provided in the landscaped area.

Additional standards set forth in LOC <u>50.05.007.7.f</u>.iii, Park Lane Special Requirements, apply adjacent to parking facilities at street frontage locations designated Park Lane on the Village Character Map, Figure <u>50.05.007-D</u>.

(b) Parking facilities shall be separated from the exterior wall of a structure by a minimum of a ten ft. buffer which may include a pedestrian pathway and/or landscaped strip. Parking areas or driveways shall be separated from abutting residential zones by a minimum ten

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ft. landscape buffer pursuant to LOC <u>50.05.007.6.1</u>.i, Requirements adjacent to Low-Density Residential.

(6) Irrigation

All parking area landscaping shall be provided with an appropriate irrigation system. Permanent irrigation may not be required if the applicant demonstrates that droughttolerant plants have been installed and established.

ix. Tree Preservation

Purpose: Tree preservation shall preserve and promote a wooded character.

<u>The following standards shall apply</u>, in accordance with <u>addition to</u> the LOC Chapter <u>55</u> (Tree Code) and with the provisions below. In case of a conflict, the more restrictive standard applies.

- (1) Development plans shall preserve existing mature, Douglas Fir evergreen trees and other significant trees except where unavoidable due to compliance with the requirements of this Code. Significant trees include are trees measuring 18 in. diameter or greater at a height of 54 in. above mean ground level at the base of the trunk, or of unique horticultural quality or historic importance to the greatest extent practicable.
- (2) The approval authority shall have the discretion to allow modifications or require changes to paving standards such as the use of pervious surfaces to preserve mature trees.

(32) Mitigation

- (a) Mitigation trees shall be planted so that over time, groves of trees will be established.
- (b) Mitigation shall require trees with a diameter of five caliper in. or greater which are removed be replaced with trees of the same or approved variety with a diameter no less than three caliper in. each, to equal or exceed the caliper inches of trees removed.
- (c) If in the City's determination there is insufficient available space on the subject property, replacement planting shall occur in an open space tract, or City-owned or dedicated property within the Lake Grove Village Center Overlay District subject to the approval of authorized property owners. If in the City's determination no feasible alternative exists to replant required mitigation, the applicant shall pay into the tree fund as provided in LOC <u>55.02.084</u>, Mitigation Required.
- (d) Replacement planting shall increase the amount of tree canopy and the extent of tree grove area in excess of the amount lost through development.

- (4) Tree protection and maintenance provisions of LOC Article <u>55.08</u>, Tree Protection, shall apply to all development applications.
- **(5)** Additional tree retention standards apply as set forth in LOC <u>50.05.007.7.f</u>, Park Streetfront Environment, for designations on the Village Character Map, Figure <u>50.05.007-D</u>.

f. Buffer Area and Screening

i. Requirements Adjacent to Low-Density Residential

Where a property within a commercial, R-0 or R-3 zone abuts an R-7.5 or R-10 zone, the following shall be provided:

- (1) Minimum ten-ft. wide landscaped buffer area, and
- (2) A 6-ft. tall wood or masonry, sight-obscuring fence or wall along the property line. The unfinished or structural side of the fence shall face the use to be screened.

ii. Permitted in Buffer Area

A buffer area may only be occupied by utilities, screening and landscaping. No buildings, access ways or parking areas shall be allowed in a buffer area except to allow direct ingress or egress to a site.

iii. Landscape Requirements

The buffer area shall be landscaped as follows:

- (1) One row of two- to three-in. caliper deciduous trees, spaced no more than 15 ft. apart; or one row of evergreen trees not less than six ft. tall and spaced no more than 15 ft. apart; or a mix of evergreen and deciduous trees planted 15 ft. apart.
- (2) Shrubs planted at appropriate spacing for the species. Shrubs shall attain a height of at least six ft. within three years of planting.
- (3) The remaining area shall be planted in ground cover and mulched with a suitable material to a depth of three in.

iv. Screening Required

Storage, trash collection areas and equipment shall be oriented away from abutting residential districts and screened by sight obscuring fencing or landscaping.

v. Additional Requirements

Additional requirements apply for buildings exceeding three stories as set forth in LOC <u>50.05.007.4.b.iii</u>, Standards for Buildings Greater than Three Stories.

vi. Special Buffer Requirements

The commercial area bounded by Bryant, Boones Ferry, Reese, and the right-of-way north of Sunset shall comply with the following special requirements:

- (1) A minimum 5-ft. buffer shall be required measured from the southern commercial property line abutting the existing 6-ft. right-of-way. Landscaping in accordance with this section shall be provided. Existing landscaping may be used to satisfy this requirement provided it currently provides screening between the commercial and residential uses in accordance with this section.
- (2) Residential <u>lots properties</u> abutting the existing 6-ft. right-of-way shall be screened from adjacent commercial uses by a single 6-ft. tall, sight-obscuring fence or wall. Unless waived by the abutting residential property owner, the fence or wall shall be located at the abutting property line of the residential zone and within the right-of-way. Fence or wall construction within the right-of-way shall require an encroachment permit as set forth in LOC Article <u>42.18</u>, Public Rights-of-way and Easements. Fence design is subject to approval of the City Manager. Gates may be provided in the fence or wall if requested by the abutting residential property owner and approved by the owner of the adjacent commercial property. Locating parallel fences or walls at both commercial and residential property lot lines abutting the right-of-way is prohibited.

g. Noise Mitigation

The provisions of LOC 34.10.539, Specific Noise Prohibitions and the following shall apply:

i. Noise Mitigation Plan

Any development for auto-oriented uses, commercial drive-through uses, restaurants and drinking establishments, or amusement uses shall require a noise mitigation plan.

ii. Noise Reduction Design

The development proposal shall incorporate noise reduction into development design, and/or provide for additional noise reduction procedures to be implemented so as not to unreasonably increase ambient residential levels.

h. Stormwater

Storm drainage and surface water management facilities shall be required for parking areas pursuant to LOC <u>50.06.006</u>, Geologic Hazard and Drainage and shall ensure stormwater is not directed onto adjacent properties.

7. SPECIAL REQUIREMENTS AND STANDARDS

a. Applicability

The following special requirements and standards apply to specific sites areas and uses within the Lake Grove Village Center Overlay District:

- i. Village Transition Area (VTA) Uses
- ii. Village Commons and Gathering Places
- iii. Village Streetfront

b. Village Transition Area Uses

The following special requirements apply to the areas mapped as Village Transition Areas (VTA) on the Village Transition Area Map, Figure <u>50.05.007-B</u>.

i. General Allowance

Except as noted below in Table 50.03.002-2, uses allowed in the underlying zone are allowed in the VTA.

ii. Village Transition Area (VTA) Restrictions

Within the VTA, auto-oriented uses are restricted as provided in Table <u>50.03.002-2</u>. Prohibited uses include gas stations, auto dealerships, vehicle storage lots, quick vehicle servicing, quick lubes, car washes, DEQ test sites, and businesses with drive through services, (e.g., such as fast food restaurants and banks with drive through service windows). Vehicle repair shops located entirely within an enclosed building are allowed as a conditional use subject to conditions set forth in Table <u>50.03.002-2</u>.

iii. Bryant Road/Sunset Drive Site

The following provisions shall apply to the approximately 0.85 acre property located at the intersection of Bryant Road and Sunset Drive (situs address 16400 Bryant Road). The uses permitted on the site are:

- (1) As provided in the underlying PF zone.
- (2) The following uses₁ subject to the limitations and requirements below shall be outright permitted uses (without requiring public ownership of the site under the PF Zone):
 - (a) Labor, civic, social, fraternal, charitable uses and community-based uses and organizations including accessory and incidental uses shall be permitted in the existing

structure existing as of May 1, 2008; and provided that the structure is on the Landmark Designation List per 50.06.009, Historic Preservation.

- (b) Educational uses, including an exhibit area related to history and safety, accessory and incidental uses.
- (c) A gathering place as identified on the Village Commons and Gathering Places Map, Figure <u>50.05.007-C</u>, subject to the requirements of LOC <u>50.05.007.7.c</u>.ii and iii, and preservation of the Heritage Trees Grove designated on the property.

c. Village Commons and Gathering Places

i. Village Commons

- (1) The Village Commons shall be a public space suitable to host community events such as farmer's markets, art fairs, holiday events, and other similar activities. It may include any of the following features: a plaza, village square, park, natural area, natural water feature, open space area, or other similar area intended for common use of area residents, employees, customers, and visitors.
- (2) The Village Commons Area shall be no less than 10,000 sq. ft. and of sufficient size and dimensions to safely and comfortably accommodate at least 500 persons.
- (3) The Village Commons shall not abut or front on Boones Ferry Road.
- (4) The Village Commons Area shall be located within 500 ft. of a public parking area and shall have easy access from the parking area.
- (5) If the Village Commons is located on privately owned land with an easement for public use, the amount of landscaping required for overall site development may be reduced by an amount equal to the area of the Village Commons.
- (5)(6) Locate the Village Commons in close proximity to Hallmark Drive, Three Sisters Creek, and Lake Grove School in such a way as to minimize vehicular impacts to adjacent neighborhoods.

ii. Village Gathering Places

(1) A Village Gathering Place shall be a small space for common use of residents, employees, customers, or visitors of the site where people can sit, congregate, and enjoy the urban design amenities or natural features of the site. A Village Gathering Place may be publicly or privately owned.

- (2) A Village Gathering Place shall be incorporated and integrated into the design and redevelopment of any site identified with a Village Gathering Place symbol on the Village Commons and Gathering Places Map, Figure <u>50.05.007-C</u>.
 - (a) On sites with the hardscape symbol, the Village Gathering Place shall include a plaza or courtyard.
 - (b) On sites with the greenscape symbol, the Village Gathering Place shall be designed as a park or natural area and shall incorporate natural features of the site.
 - (c) Sites identified as both hardscape and greenscape shall incorporate elements of both types of gathering place.
- (3) The minimum size of a Village Gathering Place shall be 7.5% of the site area, except that no more than 2,500 sq. ft. shall be required. No single dimension shall be less than 20 ft.
- (4) Up to 20% of the area of a Village Gathering Place may be used for push cart sales and kiosks provided that such uses are located within the hardscape portion of the Village Gathering Place.
- (5) On any parcel where a Village Gathering Place is provided, the following bonuses may be incorporated into the development of the site:
 - (a) Lot coverage may be increased up to five percent in GC and OC zones without a Village Transition Area overlay not to exceed maximums set forth in Table <u>50.05.007-2</u>: Lot Coverage.
 - (c) A ten percent reduction in the amount of required parking.

iii. General Standards for Village Common Area and Village Gathering Places

The Village Commons area and Village Gathering Places shall:

- (1) Be open and clearly visible from the public right-of-way. If the Village Gathering Place is designed as a courtyard enclosed by buildings, the opening to the public right-of-way shall be satisfied by a pedestrian accessway that is a minimum of 15 ft. wide. In such cases, the accessway shall be designed to be inviting to the public.
- (2) Provide seating appropriate for the site, with no less fewer than four seats provided.
- (3) Be designed to break up hardscape areas with landscaping and trees. For every 2000 sq. ft. of hardscape or fraction thereof, 200 sq. ft. of landscaping shall be provided.

- (4) Include a focal point/design feature such as a clock tower, fountain, monument, sculpture, or similar feature in any hardscape area. Greenscape areas shall be designed to highlight their natural features.
- (5) Include lighting consistent with the Lake Grove Village design theme. Lighting shall be directed away from adjoining residentially zoned neighborhoods and shall be designed to incorporate dark sky standards.

d. Village Streetfront

Village Streetfront standards are intended to ensure spatial enclosure and a distinct character along public streets.

i. General Requirements

The following standards apply to all streetfront locations within the Lake Grove Village Center Overlay District:

(1) Applicability

The Village Character Map, Figure <u>50.05.007-D</u>, sets forth locations where site dimensional, pedestrian facility design, streetscape, landscape and special standards apply along public street frontage for the following designations:

- (a) Urban Streetfront Environment, including special requirements for frontage designated as Storefront Window Orientation or Urban Street Orientation;
- (b) Transitional Streetfront Environment, including special requirements for frontage designated as Campus Street Orientation; and
- **(c)** Park Streetfront Environment, including special requirements for frontage designated as Park Lane, Crossroads Gateway or Campus Woods.

(2) Pedestrian Facilities and Streetscape

Pedestrian facility and streetscape design treatment shall be provided as designated on the Pedestrian Facilities and Streetscape Map, Figure <u>50.05.007-V</u>.

(3) Easement

A public easement of sufficient size to construct required pedestrian facilities and design concepts shall be provided as needed. Provisions set forth in LOC <u>50.05.007.6.b</u>.ii.2.f, Easement, shall apply.

ii. Urban Streetfront Environment

These standards ensure that buildings create spatial enclosure and an appropriate scale along public streets.

(1) Parking Location

No off-street vehicle parking shall be permitted between the front of the primary building and the public right-of-way.

(2) Streetfront Environmental Dimensional Standards

Streetfront Environment dimensional standards set forth in LOC 50.05.007.4.e apply:

- (a) Build-to Line;
- (b) Minimum Street Frontage;
- (c) Minimum Height at Streetfront; and
- (d) Step Back Above Second Story.

(3) Public Plaza

A public plaza may be provided as set forth in LOC 50.05.007.5.i.iii, Public Plaza.

(4) Storefront Window Orientation

- (a) At street frontage locations designated as Storefront Window Boones Ferry Road or Storefront Window Village Cross Street on the Pedestrian Facilities and Streetscape Map, Figure <u>50.05.007-V</u>, sidewalks and streetscape elements shall be provided as set forth in design standards for Storefront Window Streetscape, Figure <u>50.05.007-CC</u>, and the Lake Grove Village Center Design Handbook. Until the adoption of the Lake Grove Village Center Design Handbook, equivalent standards of the West Lake Grove Design District shall apply.
- (b) Street furniture, lighting, and tree wells shall be located within the street furniture zone of the sidewalk as illustrated in Storefront Sidewalk Zone Requirements, Figure <u>50.05.007-</u> <u>DD</u>.
- (c) Storefront windows shall be provided as set forth in LOC 50.05.007.5.i.vii.

iii. Urban Street Orientation

At street frontage locations designated as Urban Street or Urban Street Meandering Path on the Pedestrian Facilities and Streetscape Map, Figure <u>50.05.007-V</u>, sidewalks and streetscape elements shall be provided as set forth in design standards for Urban Street Orientation Streetscape, Figure <u>50.05.007-EE</u>, and the Lake Grove Village Center Design Handbook. Until the adoption of the Lake Grove Village Center Design Handbook, equivalent standards of the West Lake Grove Design District shall apply.

de. Transitional Streetfront Environment

<u>Purpose:</u> These standards pProvide flexibility in locating buildings within Campus Design Areas while ensuring a street orientation at key locations; <u>The standards apply at street frontage designated</u> Campus Street Orientation on the Village Character Map, Figure <u>50.05.007-D</u>. Transitional Streetfront Environment standards provide a transitional character between the Urban Streetfront Environment and Park Streetfront Environment, and require streetscape and landscape elements to ensure spatial enclosure and a pedestrian scale.

<u>These standards apply at street frontage designated Campus Street Orientation on the Village</u> <u>Character Map, Figure 50.05.007-D.</u>

i. Parking and Driveway Location

No off-street vehicle parking or driveway shall be permitted between the front of the primary building and the public right-of-way.

ii. Building May Abut Sidewalk

If a building is located within ten ft. of the property line at the street and exceeds 35 ft. in height or two and one-half stories, stories above the second story shall be stepped back from the adjacent lower story facing a street by a minimum of eight ft. This standard is illustrated in Figure 50.05.007-G: Step Back Above 2nd Story.

iii. Campus Street Orientation

Notwithstanding the provisions of this subsection, Streetfront Environment dimensional standards do not apply at street frontage designated as Campus Street Orientation.

<u>ivii</u>. Landscaping

If a building does not directly abut the sidewalk, landscapinge treatment is required between the building and the sidewalk.

v. Primary Public Entrance

- (1) Primary public entrances shall be oriented to a public street and located within 30 ft. of that public street. Buildings located on sites adjacent to one or more transit streets shall have primary public entrances oriented to at least one transit street and located within 30 ft. of that transit street.
- (2) Primary public entrances shall be accessed directly from a sidewalk adjacent to a public street or public plaza.

vi. Public Plaza

A public plaza may be provided as set forth in Urban Village Design Areas LOC <u>50.05.007.5.i</u>.iii, Public Plaza.

vii. Campus Street Orientation Streetscape

At street frontage locations designated Campus Street Orientation on the Pedestrian Facilities and Streetscape Map, Figure <u>50.05.007-V</u>, sidewalks and streetscape elements shall be provided as set forth in design standards for Campus Street Orientation Streetscape, Figure <u>50.05.007-FF</u>, and the Lake Grove Village Center Design Handbook. Until the adoption of the Lake Grove Village Center Design Handbook, streetscape elements are subject to approval by the review authority.

ef. Park Streetfront Environment

<u>Purpose:</u> These standards e<u>E</u>nsure trees and streetscape elements create spatial enclosure and desired character along public streets for street frontage designated Park Lane, Crossroads Gateway, or Campus Woods.

<u>These standards apply to lots with street frontage designated Park Lane, Crossroads Gateway, or</u> <u>Campus Woods.</u>

i. Setbacks

Park Streetfront setbacks apply as set forth in LOC 50.05.007.4.d.iii.

ii. Tree Retention

No tree with a trunk diameter of at least eight in. at 54 in. above grade located within 30 ft. of the Kruse Way right-of-way or within 20 ft. of any other public street right-of-way shall be removed unless determined to be dead or hazardous pursuant to LOC Chapter <u>55</u>, Tree Code, or located within area required to provide the separated pathway and retaining wall set forth in subsection 7.f.iv of this section, <u>50.05.007.7.f</u>, Crossroads Gateway Special Requirements.

iii. Park Lane Special Requirements

<u>Purpose:</u> At street frontage locations designated Park Lane on the Pedestrian Facilities and Streetscape Map, Figure <u>50.05.007-V</u>, special standards set forth below apply. Park Lane standards e<u>E</u>nsure a park character and a landscaped edge at street frontage locations where buildings may be set back to preserve existing trees, riparian areas, and topography, and to accommodate curved or limited street frontage conditions or development on large through-lots adjacent to Kruse Way.

These standards apply to lots with street frontage designated Park Lane on the Pedestrian Facilities and Streetscape Map, Figure 50.05.007-V.

(1) Park Lane Streetscape

Sidewalk and streetscape elements shall be provided as set forth in design standards for Park Lane Streetscape, Figure <u>50.05.007-GG</u>, and the Lake Grove Village Center Design Handbook. Until the adoption of the Lake Grove Village Center Design Handbook, streetscape elements are subject to approval by the Review Authority.

(2) Edge at Parking Facility

Parking facilities including both surface lots and structures adjacent to street frontage designated Park Lane shall be separated from the required pathway by a minimum ten-ft. wide landscaped area, and be visually screened from the adjacent street and pathway by landscaping_{\pm} including evergreen trees and shrubs. Trees and shrubs must be of sufficient size to provide an effective visual screen at the time of planting.

iv<u>ii</u>. Crossroads Gateway Special Requirements

<u>Purpose:</u> At street frontage locations designated as Crossroads Gateway on the Pedestrian Facilities and Streetscape Map, Figure <u>50.05.007-V</u>, special standards set forth below apply. These standards pProtect pedestrians and cyclists from vehicular movement on Kruse Way and Boones Ferry Road<u>: and</u> .-They also coordinate existing topography and mature trees with additional fir trees and basalt walls to create a distinctive gateway at a significant intersection.

<u>These standards apply to lots with street frontage designated Crossroads Gateway on the</u> <u>Pedestrian Facilities and Streetscape Map, Figure 50.05.007-V.</u>

(1) Crossroads Gateway Streetscape

Sidewalks and streetscape elements shall be provided as set forth in design standards for Crossroads Gateway, Figure <u>50.05.007-HH</u>, and the Lake Grove Village Center Design Handbook. Until the adoption of the Lake Grove Village Center Design Handbook, streetscape elements are subject to approval by the Review Authority.

- (2) Provide additional fir trees as needed to combine with existing trees to create natural groupings of fir trees.
- (3) New direct access, whether permanent or temporary, from Kruse Way is prohibited unless the applicant can demonstrate that but for the additional Kruse Way access, the traffic impacts associated with the proposed development would result in the other streets accessible to the development, or their intersections, exceeding permitted service level standards for those streets and intersections. In determining the impact upon the other streets accessible to the development, or their intersections, the traffic expected to result from future development accessing the other streets and their intersections shall be taken into account.

iv. Campus Woods Special Requirements

<u>Purpose:</u> At street frontage locations designated as Campus Woods on the Pedestrian Facilities and Streetscape Map, Figure <u>50.05.007-V</u>, special standards set forth below apply. These standards m<u>M</u>aintain a wooded character along Kruse Way and provide safe, convenient access for pedestrians, cyclists and transit riders to and within the Village Center and along arterials.

<u>These standards apply to lots with street frontage designated as Campus Woods on the</u> <u>Pedestrian Facilities and Streetscape Map, Figure 50.05.007-V.</u>

- (1) Campus Woods Streetscape. Sidewalks and streetscape elements shall be provided as set forth in design standards for Campus Woods Streetscape, Figure <u>50.05.007-II</u>, and the Lake Grove Village Center Design Handbook. Until the adoption of the Lake Grove Village Center Design Handbook, streetscape elements are subject to approval by the review authority.
- (2) Provide additional trees as needed to combine with existing trees to create natural groupings.

LOC 50.10: Definitions and Rules of Measurement

50.10.003 DEFINITIONS

1. INTERPRETATION

a. For the purposes of this Code certain terms and words are defined as follows: the words "used for" include "designed for" and vice-versa; words used in the present tense include the future, the singular tense includes the plural and vice-versa; the word "shall" is always mandatory; the word "may" is discretionary; the masculine gender includes the feminine gender, except as otherwise provided.

b. In the case of conflict between two similar or identical terms, the term identified as Historic Preservation or Solar Access shall take precedence over the general term when applied to historic preservation or solar access requirements.

(Ord. 2579, Repealed and Replaced, 03/20/2012)

2. DEFINITION OF TERMS

The following terms shall mean:

100-Year Flood

See LOC 50.05.011.3.c, "Flood, Base."

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Bodies of Water

Any outdoor swimming pool, spa or hot tub as defined by the State of Oregon One and Two Family Dwelling Specialty Code.

Build-to Line

The maximum distance a building may be set back from the front property line.

Building

Any structure used or intended for supporting or sheltering any use or occupancy. If the building is located across separate parcels or lots and the portions of the building are separately owned, the "building" shall be considered to be only that portion of the structure that is used or intended for supporting or sheltering any use or occupancy that is occurring within the boundaries of the parcel or lot.

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