

**TOWN OF MANCHESTER
PLANNING AND ZONING COMMISSION**

April 15, 2024
7:00 P.M.

Lincoln Center Hearing Room, 494 Main Street
Or virtually, via Zoom

AGENDA

This meeting will be held both in person and virtually, via Zoom. The meeting will be shown live on Cox Channel 16 and streamed live at <http://www.channel16.org/CablecastPublicSite/watch/1?channel=1>. Individuals who wish to speak at or attend the virtual meeting must complete a Request to Attend Virtually form, available at <https://manct.us/meeting> by 4:00 p.m. on the day of the meeting. These individuals will need to join the Zoom meeting and will be allowed to speak when directed by the Chairman. Zoom meeting information will be sent to individuals who complete a Request to Attend Virtually form. Only individuals who complete a Request to Attend Virtually form will be allowed to join the Zoom meeting. A physical location and electronic equipment will be provided for the public to use if a written request is received at least 24 hours in advance, via email to pzccomments@manchesterct.gov, or by mail to the Planning Department, 494 Main Street, P.O. Box 191, Manchester, CT 06045-0191.

PUBLIC HEARING:

1. [**SIDEWALK AND CURB PLAN 2024-2029**](#) *(Continued from April 1, 2024)* – Proposed amendments to the Town Sidewalk and Curb Plan.
2. [**TOWN OF MANCHESTER PUBLIC WORKS DEPT.**](#) – Expansion of Charter Oak Park facilities including a new skate park/pump track, synthetic turf playing field, and parking lot at 30 Charter Oak Street.
 - Inland Wetland Permit (IWP-0003-2024)

BUSINESS:

1. [**SIDEWALK AND CURB PLAN 2024-2029**](#) – Proposed amendments to the Town Sidewalk and Curb Plan.
2. [**TOWN OF MANCHESTER PUBLIC WORKS DEPT.**](#) – Expansion of Charter Oak Park facilities including a new skate park/pump track, synthetic turf playing field, and parking lot at 30 Charter Oak Street.
 - Inland Wetland Permit (IWP-0003-2024)
 - Erosion & Sedimentation Control Plan (ESC-0003-2024)
3. **ADMINISTRATIVE REPORTS**
 - Upcoming Training Opportunities
 - Comprehensive Zoning Regulations Update – Consultant Selection

4. **APPROVAL OF MINUTES**

- [April 1, 2024](#) – Public Hearing/Business Meeting

5. **RECEIPT OF NEW APPLICATIONS**

**TOWN OF MANCHESTER
LEGAL NOTICE**

The Planning and Zoning Commission will hold a public hearing on April 15, 2024 at 7:00 P.M., both virtually and in person in the Lincoln Center Hearing Room, 494 Main Street, Manchester, Connecticut, to hear and consider the following petition:

TOWN OF MANCHESTER PUBLIC WORKS DEPT. – Inland Wetland Permit (IWP-0003-2024) – Expansion of Charter Oak Park facilities including a new skate park/pump track, synthetic turf playing field, and parking lot at 30 Charter Oak Street, Residence AA zone.

At this hearing interested persons may be heard, either in person or virtually via Zoom, and written communications received. This meeting will be shown live on Cox Channel 16 and streamed live at <http://www.channel16.org/CablecastPublicSite/watch/1?channel=1>. Individuals who wish to speak at or attend the virtual meeting must complete a Request to Attend Virtually form, available at <https://manct.us/meeting>, by 4:00 p.m. on the day of the meeting. These individuals will need to join the Zoom meeting and will be allowed to speak when directed by the Chairman. Zoom meeting information will be sent to individuals who complete a Request to Attend Virtually form. Only individuals who complete a Request to Attend Virtually form will be allowed to join the Zoom meeting. A physical location and electronic equipment will be provided for the public to use if a written request is received at least 24 hours in advance, via email to pzcomments@manchesterct.gov, or by mail to the Planning Department, 494 Main Street, P.O. Box 191, Manchester, CT 06045-0191.

Individuals may also submit comments in writing to the Planning and Economic Development Department via email to pzcomments@manchesterct.gov, or by mail to the Planning Department, 494 Main Street, P.O. Box 191, Manchester, CT 06045-0191. All written comments received by 4:00 p.m. on the day of the meeting will be presented and recorded as part of the hearing.

A copy of this petition is in the Planning and Economic Development Department, Lincoln Center Building, 494 Main Street, and may be inspected during regular business hours (8:30 a.m. – 4:30 p.m., Monday through Friday). Information about this application will be available online at <https://Manchesterct.gov/pzc> by the Friday before the hearing.

Planning and Zoning Commission
Eric Prause, Chair

**TOWN OF MANCHESTER
PLANNING AND ECONOMIC DEVELOPMENT DEPARTMENT**

TO: Planning & Zoning Commission
FROM: Megan Pilla, Principal Development Planner *MP*
DATE: April 11, 2024
RE: Proposed Sidewalk & Curb Plan 2024-2029

This hearing is continued from April 1, 2024.

Introduction

Pursuant to Article IV, Section 279-31 of the Code of the Town of Manchester (attached for reference), the Planning & Zoning Commission must review and update the Sidewalk and Curb Plan every five (5) years. The existing Plan was most recently adopted with revisions in April 2019, and is due to be updated and adopted again now.

As described in the ordinance, the purpose of this Plan is to aid in determining whether or not developers shall be required to install sidewalks and curbs simultaneous with new building construction, and if payment of a fee in lieu of such installation is acceptable, on a case by case basis.

Proposed Update

[Changes since the April 1, 2024 meeting are highlighted in yellow.]

Proposed updates to the Plan document include the following:

- Addition of a “Purpose” section to explain the purpose of the Plan, as dictated by the referenced ordinance.
- Addition of a “Maps” section to clearly identify the maps that are included in the Plan.
- Revision of the minimum standard width for sidewalks on Rural/Scenic Roads from “8 feet” to “6-8 feet, based on site conditions.”
- Staff recommendations for streets to be added to the list of “critical path extensions” on pg. 4 of the Plan.
- Revision of the “Removal of Sidewalks” section to clarify that the Plan no longer recommends any specific sidewalks to be removed.
- Several minor language and grammar revisions.
- All references to PWD are changed to DPW for consistency with other Town documents.
- In Table One, minimum sidewalk width for Rural/Scenic Roads is revised from 6-8 feet to 5-8 feet.

- A statement is added to the end of the “*Policy for New Sidewalk Installation*” section (middle of page 3) to link this plan to DPW policies for road reconstruction projects.
- The bullet points in the “*Critical Path Extensions*” section are updated to reflect the main points of 3 of Commissioner Farina’s draft policies, which the Commission agreed could be referenced as criteria for prioritizing Town sidewalk projects (see attached with the main points highlighted in the first 3 policies). Some of this language was already there, but the additional statements make it more clear.
- In the last paragraph in the “*Critical Path Extensions*” section, the specific date range is replaced with “current 5-year” planning period.

Proposed updates to the Sidewalk Location Map, which is part of this Plan, include:

- Updates to reflect sidewalk extensions and gap closures that have been completed since 2019.
- Deletion of the term “*Bike Lane*” from the legend, because this term is incorrect in this context (the referenced trails are multi-use trails, not bike lanes).
- Deletion of the category “*Neighborhood Identified For Possible Sidewalk Removal Projects*” from the legend, because sidewalk removals are no longer recommended on a neighborhood scale.
- Removal from the Map of any trails that are not adjacent to public streets, because they are irrelevant to the purpose of this Plan.
- “Proposed Sidewalk Removal” is deleted from the legend, and all instances of this category on the map are noted to be removed.
- A proposed sidewalk extension is added to the west side of McKee Street.
- A notation is added to show pedestrian-scale lighting where it currently exists (Main Street, Hartford Road, and Broad Street). Proposed pedestrian-scale lighting can be added as a future update after it has been studied and appropriate locations are identified.

No changes are proposed to the Roadway Classification map, which accompanies this Plan only for reference.

Staff Review

As required by the ordinance, the proposed Plan update was sent to the Board of Directors for review. No comments from the Board have been received.

Attorney O’Neil will be present at the April 15, 2024 meeting to aid the discussion of legal requirements related to this Plan.

mp

R:\Planning\PZC\2024\04 - April 15\Packet\Sidewalk Plan - Memo.docx

Attach.

*Town of Manchester, CT
Wednesday, March 20, 2024*

Chapter 279. Streets and Sidewalks

Article IV. Assessments for Sidewalks and Curbs

§ 279-31. Sidewalk and curb installation on existing streets simultaneous with building construction.

[Amended 7-22-2003]

- A. The owner of lands on which any structure is hereafter erected, fronting on public highways or streets in the Town, shall install concrete sidewalks and granite curbs in accordance with the public improvement standards of the Department of Public Works, if such installation is in conformance with the sidewalk and curb plan of the Town as approved by the Town Planning and Zoning Commission. Said walks and curbs shall be installed concurrently with the construction of said structure. When, for good cause shown, the installation of either sidewalks or curbs is not advisable or desirable, the Planning and Zoning Commission may allow and/or require the owner of said lands to make a payment to the Town in lieu of the installation. The Planning and Zoning Commission shall not make its determination until it has received a recommendation from the Director of Public Works on the advisability of installing the sidewalks and/or curbs. The Director of Public Works shall make his recommendation no later than 20 working days from the receipt of the request for an opinion from the Planning and Zoning Commission or its designated staff.

[Amended 3-3-2009]

- B. When payment in lieu of installation is required, payments shall be in an amount based upon the prevailing costs of said improvements as determined by the director of public works. If a substitute material for granite curbing is required temporarily for good cause, the cost of the substitute material shall be deducted from the cost of the granite, and the balance will constitute the payment in lieu of installation.
- C. Payments in lieu of installation shall be deposited into a dedicated fund to be used solely for the installment of new sidewalks and curbs. A record of all property owners and properties subject to the payment in lieu of installation shall be kept by the Planning Department, and said property owners shall not be liable for any fee or assessment for the installation of sidewalks or curbs in front of their property in the future.

- D. The Planning and Zoning Commission shall prepare and adopt a sidewalk and curb plan for the Town. The plan shall set forth the policies for determining where sidewalks and curbs would be installed; enumerate examples of what constitutes good cause for allowing or requiring payment in lieu of installation; and recommend priority locations for the extension or removal of sidewalks. The sidewalk and curb plan shall be reviewed and amended as appropriate every five years. The plan shall be so revised and adopted within six months of the adoption of this section and every five years thereafter. Before adoption, the plan shall be submitted to the Board of Directors for review and comment, and at least one public hearing shall be held.

- E. If concrete sidewalks, concrete curbs or granite curbs are in existence abutting the land on which any structure is hereinafter erected, fronting on public highways or streets in the Town, the owner of such lands shall repair said sidewalks and curbs to the satisfaction of the Director of Public Works

or his designated representative. Existing bituminous curbs shall be removed and replaced with granite curbs.

[Amended 3-3-2009]

- F. The Building Department shall affix a copy of this regulation to all building permits issued by it.
- G. From the effective date of this section forward, any previously granted deferments may be called by the Board of Directors, and the then-owner of the property shall be required to pay the cost of said installation. Before calling a deferment, the Board of Directors shall request a written recommendation from the Planning and Zoning Commission and Director of Public Works on the advisability of calling said deferment.

Draft Policies for the Sidewalk and Curb Plan

Schoolwalk Safety Policy

To ensure the safety and well-being of our children, it is the policy of the Manchester Planning and Zoning Commission to prioritize planning for sidewalk extensions and sidewalk gap filling within a 1-mile schoolwalk distance to all elementary schools, a 1.5 mile schoolwalk distance to Iling Middle School, and a 2-mile schoolwalk distance to Manchester High School. This Schoolwalk Safety Plan is grounded in the commitment to provide safe, accessible, and pedestrian-friendly routes for students, to foster the completion of sidewalks in order to fully enact the Manchester Board of Education bussing policy, and to encourage walking as a healthy, environmentally-friendly way to go to school. The Schoolwalk Safety Plan should prioritize arterial and collector roads before other road types. The presence of well-constructed sidewalks for students to walk to school is crucial in minimizing the risks of traffic-related incidents and promoting a secure environment for our students. This policy supports the Manchester Complete Streets Policy and the Manchester Plan of Conservation and Development's broader vision of fostering community connectivity, physical activity, and the safety of all residents, particularly our schoolchildren.

"Last Mile" Transportation Policy

To foster "last mile" mile transportation goals, it is the policy of the Manchester Planning and Zoning Commission to prioritize sidewalk connectivity and gap filling within 1-mile of bus stops and should prioritize arterial and collector roads before other road types. This "last mile" sidewalk policy is grounded in a commitment to provide safe, accessible, and pedestrian-friendly routes for residents and commuters, to foster the completion of our multi-modal transportation network, and to encourage walking as a healthy, environmentally-friendly way to go to work and ride the bus. This "last mile" sidewalk policy enhances public transportation access and pedestrian safety through the improvement of non-motorized access to bus stops through the strategic expansion of sidewalk connectivity. This policy aims to ensure safe, convenient, and direct pedestrian routes to bus stops, and also removes anxiety, stress, and safety concerns. This policy supports the Manchester Complete Streets Policy and the Manchester Plan of Conservation and Development's explicit transportation goals.

Parks and Recreation Access Policy

To enhance community health and connectivity, it is the policy of the Manchester Planning and Zoning Commission to prioritize sidewalk expansion and gap filling to improve access to athletic fields and parks, ensuring safe, direct pedestrian pathways within a 1-mile radius of these recreational areas. This policy should prioritize arterial and collector roads before other road types and is rooted in a commitment to promote safe afterschool activities, active lifestyles, and equitable use of our parks and recreation amenities. This policy helps to create

a seamless and secure sidewalk network while enriching the quality of life for all residents. This policy supports the explicit goals of the Manchester Plan of Conservation and Development, and fulfills our vision of a connected, vibrant, and physically active community. This policy also underscores our dedication to enhancing public spaces, fostering community engagement, and ensuring the safety and well-being of all citizens as they enjoy Manchester's recreational assets.

Roadwork Construction Policy

To reduce costs, construction times, and road work disruptions for residents, it is the policy of the Manchester Planning and Zoning Commission to coordinate the priorities of the Sidewalk and Curb Plan with the 5-year road reconstruction and resurfacing plan of the Department of Public Works and water main replacement schedule of the Manchester Water Department. This policy will reduce long-term costs of sidewalk gap filling and extensions and foster the completion of our multi-modal transportation network. This policy to coordinate the plan with the Department of Public Works and Manchester Water Department will help ensure that roadwork initiatives include a plan for enhancing pedestrian infrastructure, thereby streamlining efforts and resources to improve walkability and safety concurrently with road improvements. The goal is to capitalize on infrastructure upgrades, reduce long-term costs, and accelerate the creation of a continuous, accessible pedestrian network that syncs individual project schedules to minimize disruptions in the community. This policy supports the explicit goals of the Complete Streets Policy and the Manchester Plan of Conservation and Development to enhance public safety, promote complete streets, and ensure that our infrastructure is improved in a way that benefits all residents.

Long-standing Prioritization Policy

To ensure that long-term goals are met, it is the policy of the Manchester Planning and Zoning Commission to note and take into consideration the number of years a sidewalk extension or gap filling priority has been a part of the Sidewalk and Curb Plan. This policy will help foster completion of the long-term goals of our multimodal transportation network and ensure our Town is making regular progress in implementing the plan.

Street/Pedestrian Lighting Policy

The Manchester Planning and Zoning Commission supports a policy to prioritize the integration of enhanced street and pedestrian lighting in all future road reconstruction or resurfacing projects and neighborhood lighting. This initiative will function most effectively through a collaborative effort with the Department of Public Works to ensure infrastructure upgrades include a comprehensive plan for installing or upgrading streetlights, thereby improving visibility, safety, and comfort for all pedestrians, especially during evening and nighttime hours. Lighting should be consistent by neighborhood, and a new map layer

should be added to the Sidewalk and Curb Plan. By embedding lighting improvements into existing roadwork plans and planning for long-term neighborhood lighting goals, the Town can leverage these projects as opportunities to enhance our city's walkability efficiently and sync individual project schedules to minimize disruptions in the community. This policy underscores our commitment to fostering a safe, visually accessible, and welcoming environment, aligning with the Manchester Complete Streets Policy and the Manchester Plan of Conservation and Development's vision for a well-lit, safe, and accessible community.

Implementation Policy

It is the policy of the Manchester Planning and Zoning Commission to establish and recommend timelines for the implementation of the Sidewalk and Curb Plan. The Plan's recommendations for sidewalk extensions and gap filling will include proposed schedules by year, on a 5-year timeframe, and will note potentially longer-term priorities for sidewalk extension and gap filling. This policy fulfills the vision and course of action goals of the Manchester Plan of Conservation and Development, while recognizing that not everything in the Plan will happen within a 5-year period and that some priorities may take many years or even decades to complete. This implementation policy may also allow the Town to more effectively budget sidewalk improvements and more effectively apply for grant funding.

~~2019~~2024-~~2024~~2029

TOWN OF MANCHESTER
SIDEWALK AND CURB PLAN



Prepared by:

Manchester Planning Department
Manchester Public Works Department

Adopted ~~April 1, 2019~~TBD
Planning and Zoning Commission

SIDEWALK & CURB PLAN

Purpose

In accordance with Town ordinance § 279-31, which requires the installation of sidewalks and curbs by property owners simultaneous with new building construction, the purpose of this plan is:

- To set forth the policies for determining where sidewalks and curbs should be installed along public streets;
- To recommend priority locations for the extension of sidewalks; and
- To enumerate examples of what constitutes good cause for allowing or requiring payment in lieu of sidewalk installation.

Introduction

Providing paths for pedestrians has always been fundamental to community building, and while the need for and function of sidewalks has changed, it has not disappeared. The purpose of sidewalks is to provide a safe location for people to walk separated from motorized or mechanized vehicles. Sidewalks are an elemental form of transit, connecting people to public transit, schools, work, shopping, services, and cultural or recreational facilities and activities. They provide a space for spontaneous social interaction. They are increasingly used as a recreation and health amenity in themselves for walkers and joggers. For families with young children sidewalks provide a safe dedicated space for youngsters to learn to ride a bike or rollerblade.

For all of these reasons, Manchester has long required individuals and businesses developing land in Manchester to construct sidewalks. The goal is to provide pedestrian connections within neighborhoods, connections between neighborhoods, and connections from homes to services, facilities, and amenities in the community.

The Town's Sidewalk Plan was first adopted in 1980. Since that time there have been ~~at least~~ ~~two~~ ~~several~~ periods of significant new industrial, commercial and residential development in Manchester. ~~Because-As~~ development ~~has~~ ~~extend~~ ~~sed~~ to previously undeveloped areas ~~and~~ ~~away~~ ~~from~~ ~~beyond~~ the Town center, the Planning and Zoning Commission ~~periodically~~ ~~review~~ ~~sed~~ the Sidewalk Plan considering current conditions and anticipated future trends. Our sidewalk and curb policies, and the location map that ~~will~~ ~~serve~~ as a guide for the implementation of this Plan, have been revised to reflect ~~these~~ ~~new~~ ~~current~~ ~~realities~~ ~~condition~~. The Plan articulates policies for where new sidewalks should be installed; effectively and sensibly deals with new installations in developed areas; sets guidelines on how and whether to decide to remove

sidewalks in certain areas, identifies priorities for filling in gaps and/or extending the existing system; and provides guidance on what types of curbs should be installed in various locations or conditions.

Maps

The following maps are included with this Plan:

- Sidewalk Location Map – a map of all existing and anticipated future sidewalks along public streets, identified by material type.
- Roadway Classification Map – the classification of all public streets by type, to be used in accordance with the policies outlined below for locating new sidewalks.

Policy for New Sidewalk Installation

As a general policy, the Sidewalk Plan calls for concrete sidewalks on ~~all~~most streets in the Town of Manchester. The amount and location of sidewalks will vary depending on the type of street and its function. Table One describes the minimum standards and locations for sidewalks. Roadway classifications are shown on the Roadway Classification Map, Town of Manchester Sidewalk Plan, ~~dated~~revised July 2009(date TBD).¹

On all new local streets, sidewalks will be constructed as required in Table One unless the judgment is made by the Planning and Zoning Commission, with the recommendation of the ~~Public Works~~ Department of Public Works (PWDDPW), that payment in lieu of installation is warranted. On all existing local² streets, sidewalks will be required as identified on the Sidewalk Location Map, unless the judgment is made by the Planning and Zoning Commission, with the recommendation of PWDDPW, that payment in lieu of installation is warranted.

Table One

Minimum Standards for Sidewalks					
Roadway Classification	Sidewalk Width (feet)	Concrete	Bituminous or Alternative	One Side	Two Sides
Arterial Street	5	x			x
Collector Street	5	x			x
Local Street	5	x		x	
Rural/Scenic Road	*5-8		x	x	

* based on site conditions

¹ Rev. 1/21/09

² Rev. 1/21/09

Regardless of the general policy and standards recommended in this Plan and contained in Table One above, the location of sidewalks on existing streets shall be based on the Sidewalk Location Map which is a part of this Plan. Sidewalks will be required on all street segments identified as Proposed Sidewalk Extensions, and repairs, if necessary as required by the Town Sidewalk Ordinance (see Appendix A), will be required on all segments identified as existing sidewalk. The Plan may require that sidewalks be built on both sides of an existing local street if it is deemed necessary for pedestrian safety given the proximity to schools, the housing density of the neighborhood, and other factors.

When required on only one side of the street, sidewalks shall be installed on the north and east sides of the roadway to facilitate snow melt and ease of maintenance. While this general standard should always apply, there may be circumstances where the location, width, or material requirements may be waived. Conditions that may warrant a deviation from these standards include the following:

- Infill development in neighborhoods or on streets where the sidewalks should match the dominant pattern on the street or in the neighborhood.
- Physical features of the area including the available right-of-way, grades, rocks/ledge, specimen trees or other important natural features which should be preserved, etc. In these cases the Director of Public Works may recommend, and the Planning and Zoning Commission may approve, alternate locations.

All sidewalks shall be built according to the design and construction standards contained in the Town of Manchester Public Improvement Standards, as amended.

To the extent feasible based on available funding and in accordance with the Town's Complete Streets Policy, sidewalk extensions and gap filling will be included in road reconstruction projects performed by DPW.

Critical Path Extensions

The Sidewalk Plan has identified the desirable locations for sidewalks throughout the Town of Manchester. Keeping in mind this Plan will be updated every five years, the Plan identifies selected streets as priorities for the extensions of new sidewalks. Criteria for determining these critical path connections include the following:

- The desire to provide safe pedestrian connections to schools, parks and athletic fields, shopping, or transit facilities, with consideration given to the school location, residential density and nature of the streets and traffic. Sidewalk extensions and gap filling will be prioritized within a 1-mile radius of all schools, bus stops, parks and athletic fields.
- The desire to provide safe pedestrian walkways on arterial or collector streets where such pedestrian systems are not now available, in particular highly populated or highly

traveled areas. Sidewalk extensions and gap filling will be prioritized on these roadway classes.

- The desire to close gaps in sidewalk systems in areas of high pedestrian traffic.

The priority streets identified as part of this Plan are:^{3 4}

- ~~Broad Street from Middle Turnpike West to Hilliard Street~~
- Tolland Turnpike
- Keeney Street from Bush Hill Road north
- Parker Street from East Center Street to Mather Street
- Sheldon Road
- Woodland Street
- Woodbridge Street from Parker Street to Weaver Road
- East Center Street from Goodwin Street to Middle Turnpike East
- Charter Oak Street
- Wyllys Street/Porter Street
- Autumn Street
- Bush Hill Road
- Hilliard Street from Duval Street to Fleming Road

Although these are priority streets for extensions, such extensions are dependent upon the availability of funds and may not be ~~included-completed~~ during the ~~2019-2024~~current 5-year planning period. Likewise, other streets may see their sidewalks extended if funding opportunities arise, or if circumstances arise during the planning period that warrant sidewalk extensions on non-priority streets.

Removal of Sidewalks

~~In some circumstances in older residential neighborhoods where there are sidewalks on both sides of the street, it may be desirable to remove sidewalks to meet the current standard of sidewalks on only one side of the street. Removing walks on one side of the street would bring older neighborhoods in line with current practice for local streets. The removal of sidewalks within narrow rights-of-way provides opportunities to create greater separation of pedestrian and vehicular traffic. Selective removal would allow more neighborhood sidewalks to be repaired or replaced over the long term, and the Town's long-term maintenance and liability costs would be reduced. The removal of sidewalks is not generally recommended; however, the Town recognizes that unique circumstances may arise in which removals could be considered. Such instances will be reviewed on a case by case basis.~~

³ Rev. 1/21/09

Rev.7/6/09

Rev.02/13/14

⁴ Rev. 04/01/19

Sidewalk removal projects will be pursued only after an analysis of neighborhood conditions has been completed and public meetings with neighborhood residents have been held. Conditions which may warrant the removal of sidewalks include the following:

- The amount of right-of-way available in the existing public street area.
- The density of housing in the neighborhood. Generally the lower the housing density and the smaller the resident population, the more unlikely sidewalks will be necessary on both sides of the street.
- On streets with low traffic volumes and/or which are not major pedestrian corridors, sidewalks will less likely be necessary on both sides of the street.
- Neighborhoods whose distance from commercial or service uses would generate less pedestrian traffic may not require sidewalks on both sides of the street.
- Neighborhoods with wide rights-of-way, exclusively residential areas and neighborhoods not in proximity to non-residential uses.

Granite Curbing Installation

- Installation of granite curbs shall be required on all new local streets and extensions of existing local streets.
- Granite curbs shall be required on all collector and arterial streets as shown on a Town of Manchester Roadway Classification Map.
- When individual lot development occurs within a developed local street, the type of curb required should be consistent with the predominant material currently existing on that street, as determined by ~~PWD~~DPW.
- When existing roadways and sidewalks are reconstructed on local streets, curbs should be replaced with the predominant curb type that exists within the neighborhood, as determined by ~~PWD~~DPW.
- On rural/scenic roads, the determination whether to install granite curbs or alternate materials will be based upon such factors as the street grade, stormwater management considerations, and natural features and topography.

Rural/Scenic Roads

Although Manchester is a largely developed community, there are some roads that are, and should remain, rural and scenic in character. These are roads in outlying areas that are mostly identified as ~~natural or rural/agricultural~~conservation sectors on the ~~proposed character~~

~~map~~Conservation & Growth Map in the Town's Plan of Conservation and Development, ManchesterNEXT. Future development in these areas is anticipated to be at low densities that would not conflict with rural or scenic roads. In these areas a pedestrian path should be provided, but a bituminous or stone dust pedestrian path may be more appropriate than granite curbs and concrete sidewalks.

Good Cause for Payment in Lieu of Installation

~~The~~This Plan identifies desirable locations for sidewalks and curbs⁵ throughout the community. The Plan recognizes that, depending on the amount of Town financial resources available, and the development pattern in various areas, it may not be prudent or practical to install sidewalks and curbs⁶ in all new developments approved during the current five--year planning period. Where the Plan calls for sidewalk and curb installation, but where there is good cause not to install them at the time of the development, payment in lieu of installation shall be required in accordance with the Town Sidewalk Ordinance (see Appendix A).

~~Criteria~~Situations in which requirement of payment in lieu of installation may~~to~~ be considered by the Planning and Zoning Commission following a recommendation by the Director of Public Works when requiring payment in lieu of installation would include, but are not ~~be~~-limited to, the following:

- There are no other sidewalks or curbs within a reasonable distance of the area where the sidewalks would be required.
- There are no sidewalks or curbs in the area or on the street where new walks would be otherwise required.
- There is no project funded by the Town that would fill in the gaps that exist in the sidewalk and/or curb system on the street where the development is proposed.
- There are plans in the near term for major street reconstruction which would provide opportunities to fill gaps in the sidewalk and/or curb system, but it would be better to construct the required sidewalks as part of the larger reconstruction project.
- When a sidewalk and or granite curbs are to be located within the right of way of a State road and the State denies a permit for the installation⁷.

⁵ Rev. 1/21/09

⁶ Rev. 7/06/09

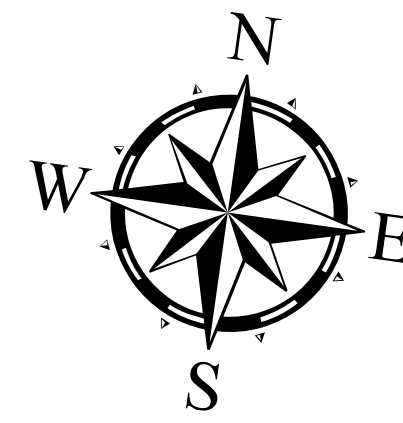
⁷ Rev. 7/06/09

R:\Planning\SIDEWALK AND CURB PLAN\Revision - March 2024\DRAFT Sidewalk Plan update 2024.doc



SIDEWALK LOCATION MAP

TOWN OF MANCHESTER SIDEWALK & CURB PLAN



SCALE: 1 inch = 1000 feet

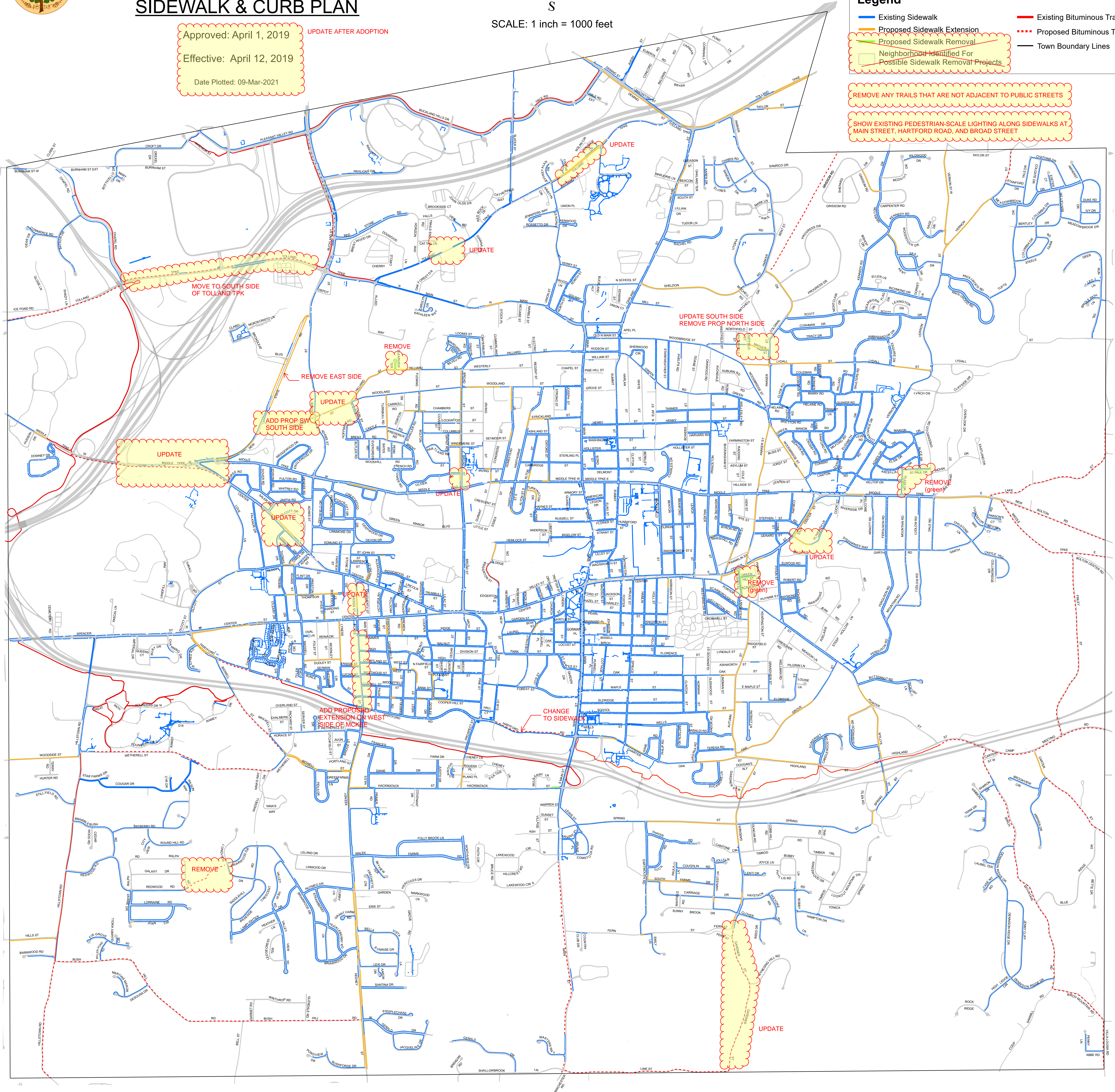
Approved: April 1, 2019 UPDATE AFTER ADOPTION
Effective: April 12, 2019
Date Plotted: 09-Mar-2021

Legend

- Existing Sidewalk
- Proposed Sidewalk Extension
- Proposed Sidewalk Removal
- Neighborhood Identified For Possible Sidewalk Removal Projects
- Existing Bituminous Trail/Bike Lane
- - - Proposed Bituminous Trail/Bike Lane
- Town Boundary Lines

REMOVE ANY TRAILS THAT ARE NOT ADJACENT TO PUBLIC STREETS

SHOW EXISTING PEDESTRIAN-SCALE LIGHTING ALONG SIDEWALKS AT MAIN STREET, HARTFORD ROAD, AND BROAD STREET



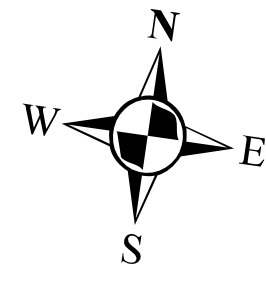


ROADWAY CLASSIFICATION MAP

TOWN OF MANCHESTER SIDEWALK & CURB PLAN

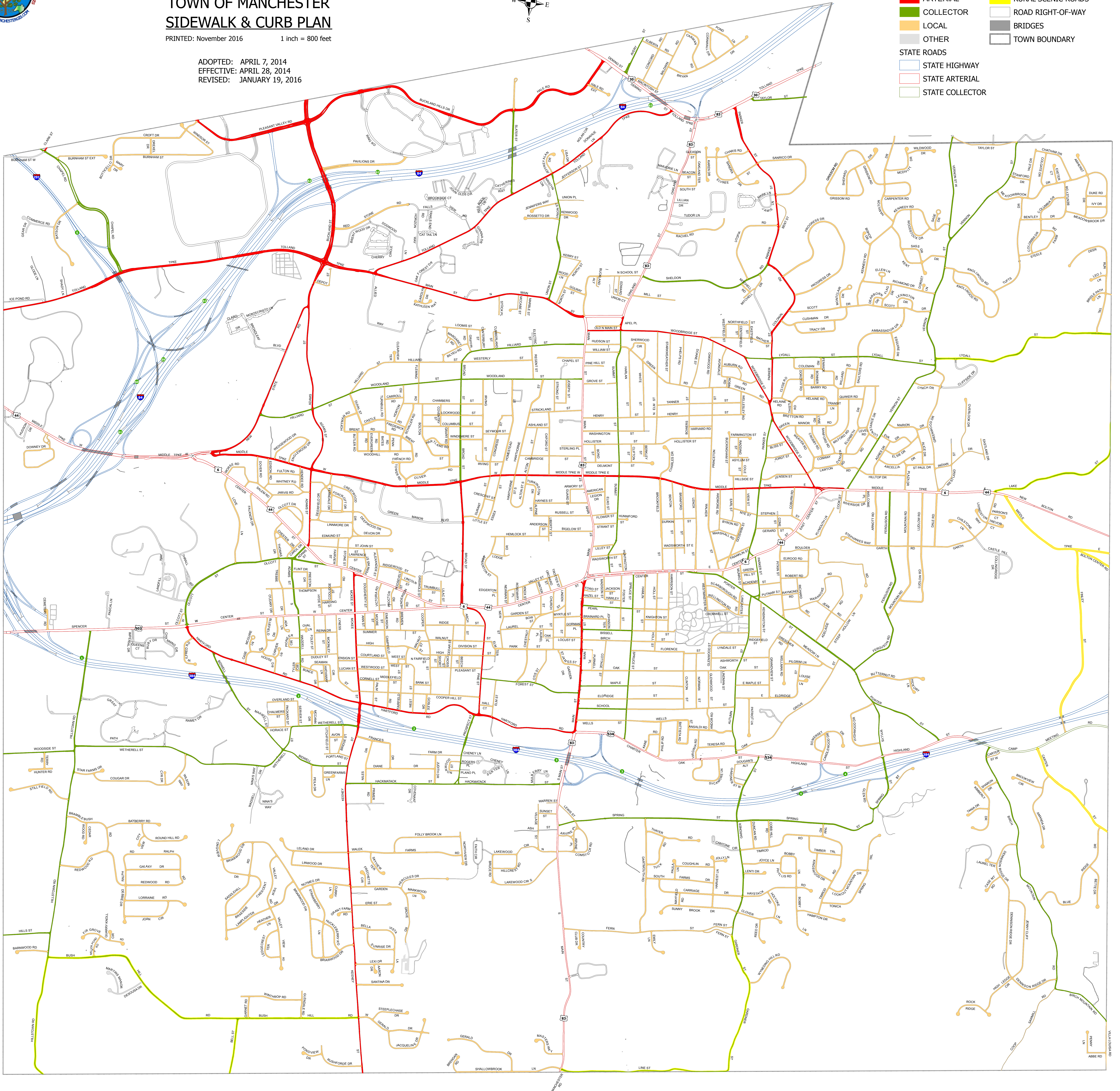
PRINTED: November 2016 1 inch = 800 feet

ADOPTED: APRIL 7, 2014
EFFECTIVE: APRIL 28, 2014
REVISED: JANUARY 19, 2016




LEGEND

- █ ARTERIAL
- █ COLLECTOR
- █ LOCAL
- █ OTHER
- █ STATE HIGHWAY
- █ STATE ARTERIAL
- █ STATE COLLECTOR
- █ RURAL-SCENIC ROADS
- █ ROAD RIGHT-OF-WAY
- █ BRIDGES
- █ TOWN BOUNDARY



**TOWN OF MANCHESTER
PLANNING AND ECONOMIC DEVELOPMENT DEPARTMENT**

TO: Planning & Zoning Commission

FROM: Megan Pilla, Principal Development Planner 

DATE: April 11, 2024

RE: Town of Manchester Public Works Department – 30 Charter Oak Street
Erosion & Sedimentation Control Plan (ESC-0003-2024)
Inland Wetlands Permit (IWP-0003-2024)

Introduction

The applicant is requesting approval of an inland wetlands permit and certification of an erosion and sedimentation control plan for improvements to the western portion of Charter Oak Park, including construction of a new parking lot, synthetic turf field, and skate park/pump track at 30 Charter Oak Street. The parcel is located in the Residence AA zone (RAA).

Project Description

The approximately 7.4-acre parcel is bounded by Charter Oak Street to the north, South Main Street to the west, the I-384 Exit 3 westbound ramps to the east, and the Charter Oak Greenway and Hop Brook to the south. There is no parking on the site, which is currently used for municipal soccer fields.

The applicant is proposing the following improvements to this portion of Charter Oak Park:

- Construction of a 390-ft. by 240-ft. synthetic turf playing field
- Construction of turf field amenities, including bleachers, benches, a scoreboard, goal posts, vinyl-coated chain link fence, etc.
- Construction of a 74-stall parking lot with new curb cut on Charter Oak Street
- Construction of a 42-ft. by 16-ft. concrete block utility and storage building
- Installation of site and field LED lighting
- Construction of a combination skate park/pump track

The skate park/pump track will be designed by a consultant, but the footprint is indicated on the submitted plans.

The proposed parking lot includes storm drainage via a series of catch basins, with a hydrodynamic separator at the southwest corner of the site to treat runoff before it enters the existing storm drainage outfall pipe that discharges into Hop Brook. Runoff from the synthetic

turf field will also be treated and conveyed by this system after being captured by a subsurface drainage system beneath the field.

The utility and storage building includes ADA-accessible public restrooms.

Erosion and Sedimentation Controls

Proposed erosion and sedimentation controls (highlighted on sheet 6 of the attached plan set) include silt fence along the southern project limits downgrade of construction, silt sacks at all drainage structures, a temporary soil stockpile area with silt fence along the downgrade side, and a dedicated construction entrance with anti-tracking pad.

The total area of disturbance is approximately **4.2 acres**.

Determination of Significant Impact

At the April 1, 2024 meeting, the Commission determined that the proposed project may have a significant impact on the adjacent wetlands, and therefore requires a public hearing. The applicant is expected to discuss functions and values of the wetland and any alternatives that were considered at the April 15 meeting.

Inland Wetlands Permit

The adjacent wetland includes Hop Brook and a portion of its bank to the south of the Charter Oak Greenway. No work is proposed within the wetland or watercourse.

A portion of the project falls within the 100-foot upland review area of the wetland, including the southern portions of the parking lot, drainage system, turf field, and skate park/pump track as well as the proposed utility building. All disturbance is to the north of the greenway.

The total area of proposed disturbance within the upland review area is **0.77 acres**.

Staff Review

Town staff has reviewed the plans and documents submitted with the application and the status of any outstanding comments will be provided at the April 15, 2024 meeting.

mp

R:\Planning\PZC\2024\04 - April 15\Packet\IWP-0003, ESC-0003-2024 (30 Charter Oak) - Memo.docx

Attach.

Town of Manchester, CT



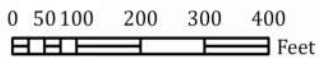
Geographic Information Systems 30 CHARTER OAK STREET

Legend

-  Overlay Zone
- Zoning**
-  AA - Residence AA
-  B3 - Business III
-  CBD - Central Business District
-  H - Historic
-  NB - Neighborhood Business
-  PRD - Planned Residence Development
-  RB - Residence B
-  RR - Rural Residence

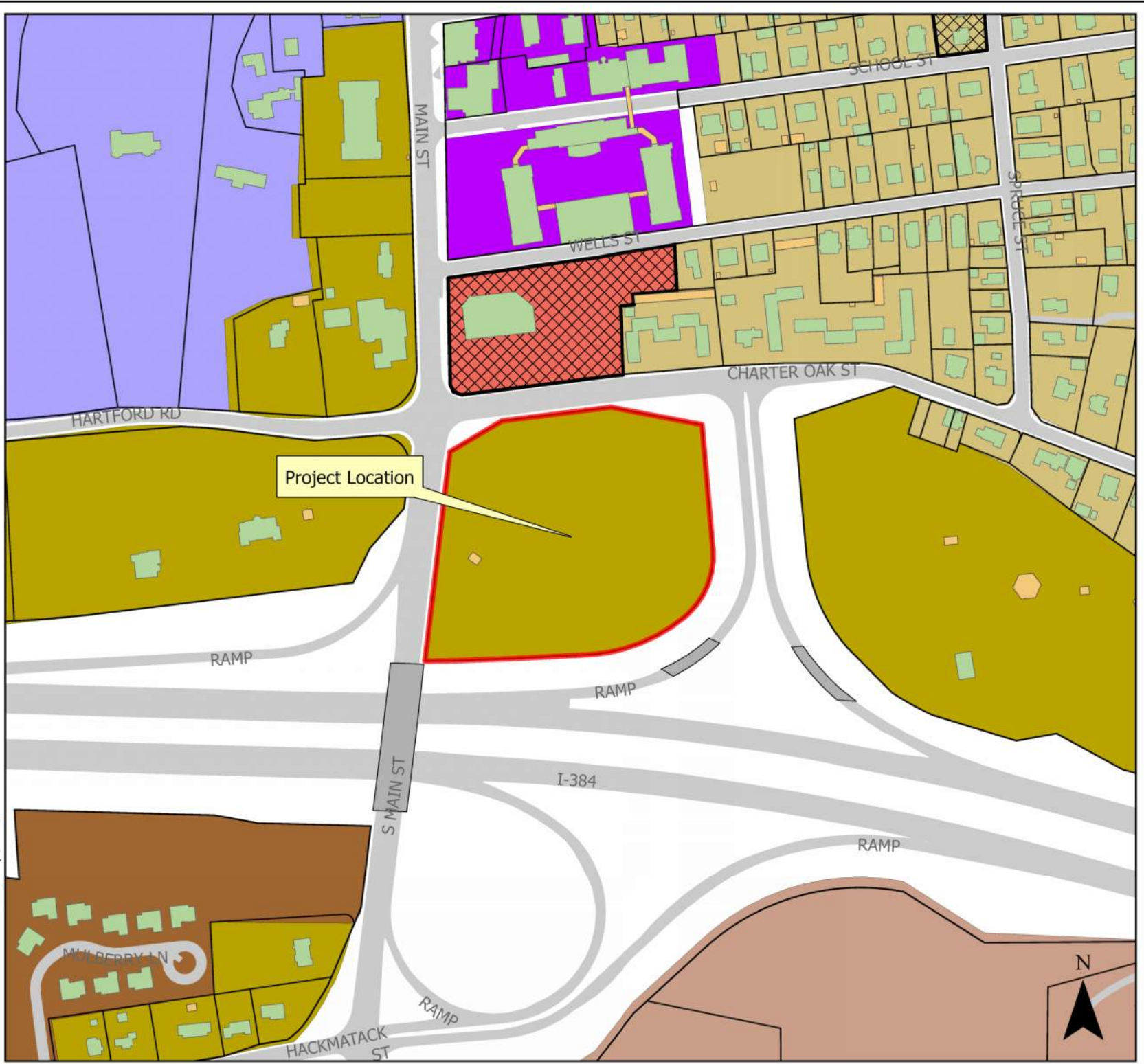
DISCLAIMER:
The Town of Manchester, CT assumes no legal responsibility for the information contained in this map. This map is provided 'AS IS' without warranty of any kind.

NOTES:
Planimetric and topographic information were compiled by stereo photogrammetric methods from photography dated April 24, 1999 in accordance with ASPR accuracy standards for 1"=40' large scale Class I maps. Real property compiled from recorded deeds, subdivision plans and other public records. Utility networks compiled from record plans, as-builts and/or field survey data. Aerial photography dated April 24, 1999.



1 inch = 300 ft

Date: 3/28/2024



PROJECT NARRATIVE
FOR
CHARTER OAK PARK WEST

March 15, 2024

by

Jeff LaMalva, P.E.,
Town Engineer



Town of Manchester
Department of Public Works
Engineering Division

INLAND WETLANDS AND EROSION & SEDIMENTATION CONTROL PERMIT: APPLICATION SUPPLEMENT

PROPOSED PROJECT INFORMATION:

The Town of Manchester Department of Public Works proposes to expand Charter Oak Park with the construction of a 390' x 240' synthetic turf playing field, a combination skate park/pump track and associated 74-stall parking lot at 30 Charter Oak Street.

EXISTING CONDITIONS:

30 Charter Oak Street is a Town owned parcel that is bordered to the north by Charter Oak Street (State Route 534), to the east by the Interstate 384 Exit 3 Westbound ramps, to the south by the Charter Oak Greenway and Hop Brook and to the west by South Main Street (State Route 83). The 7.4 acre site is zoned Residence AA and currently used for municipal soccer fields. There is no parking on site. The Charter Oak Greenway, a 10' wide bituminous concrete shared use path, runs southerly and westerly of the existing fields. The site is not located within an aquifer protection area. A preliminary screening through the Natural Diversity Database (NDDDB) indicated no critical habitats have been documented in close proximity to the site.

SCOPE OF WORK:

The project primarily includes the following work:

- Construction of a 390' x 240' synthetic turf playing field;
- Construction of turf field amenities, including bleachers, scoreboard, benches, etc.;
- Construction of a 74-stall parking lot with new curb cut on Charter Oak Street;
- Construction of a 42' x 16' concrete block utility and storage building;
- Installation of site and field LED lighting;
- Construction of a combination skate park/pump track;

TRAFFIC STATEMENT:

The project is not expected to have a significant impact on traffic. The proposed curb cut has been reviewed and approved by the State of Connecticut Department of Transportation.

UTILITY STATEMENT:

The project includes the installation of a new electric service and a new water service coming off of Charter Oak Street. There is an existing sanitary sewer main that traverses the site. A new sanitary sewer service for the utility building is proposed that will connect to the existing main. Other than the resetting of manhole frames to grade, there are no expected impacts to the existing utility.

PLANS:

The following plan is included as part of the submission:

1. Charter Oak Park West, Inland Wetlands and Erosion and Sedimentation Control Permit, dated March 2024, prepared by Town of Manchester Public Works Department Engineering Division

EROSION & SEDIMENTATION CONTROL:

Control measures will include installation of silt fence along the southern project limits downgrade of the proposed construction. Further installation of silt fencing and/or silt sacks will be as needed at the direction of the Engineer. All sedimentation and erosion control devices will be installed in accordance with the 2002 Connecticut Guidelines for Soil Erosion and Sediment Control prior to the start of construction and maintained or replaced by the Contractor.

DISTRUBANCES:

The overall total site disturbance is approximately 4.2 acres. Approximately 33,500 sf, or 0.77 acres of the overall disturbance is located within the 100' upland review area. No work is proposed within the wetlands.

STORMWATER MANAGEMENT:

The proposed project will increase the site's impervious area coverage by approximately 0.75 acres. The project includes the construction of a hydrodynamic separator at the southwest corner of the site designed to treat the runoff from the parking lot and synthetic turf field before discharging into an existing storm drainage outfall pipe that discharges to Hop Brook. Per the Town's Public Improvement Standards, stormwater detention is not required due to the close proximity to the watercourse.

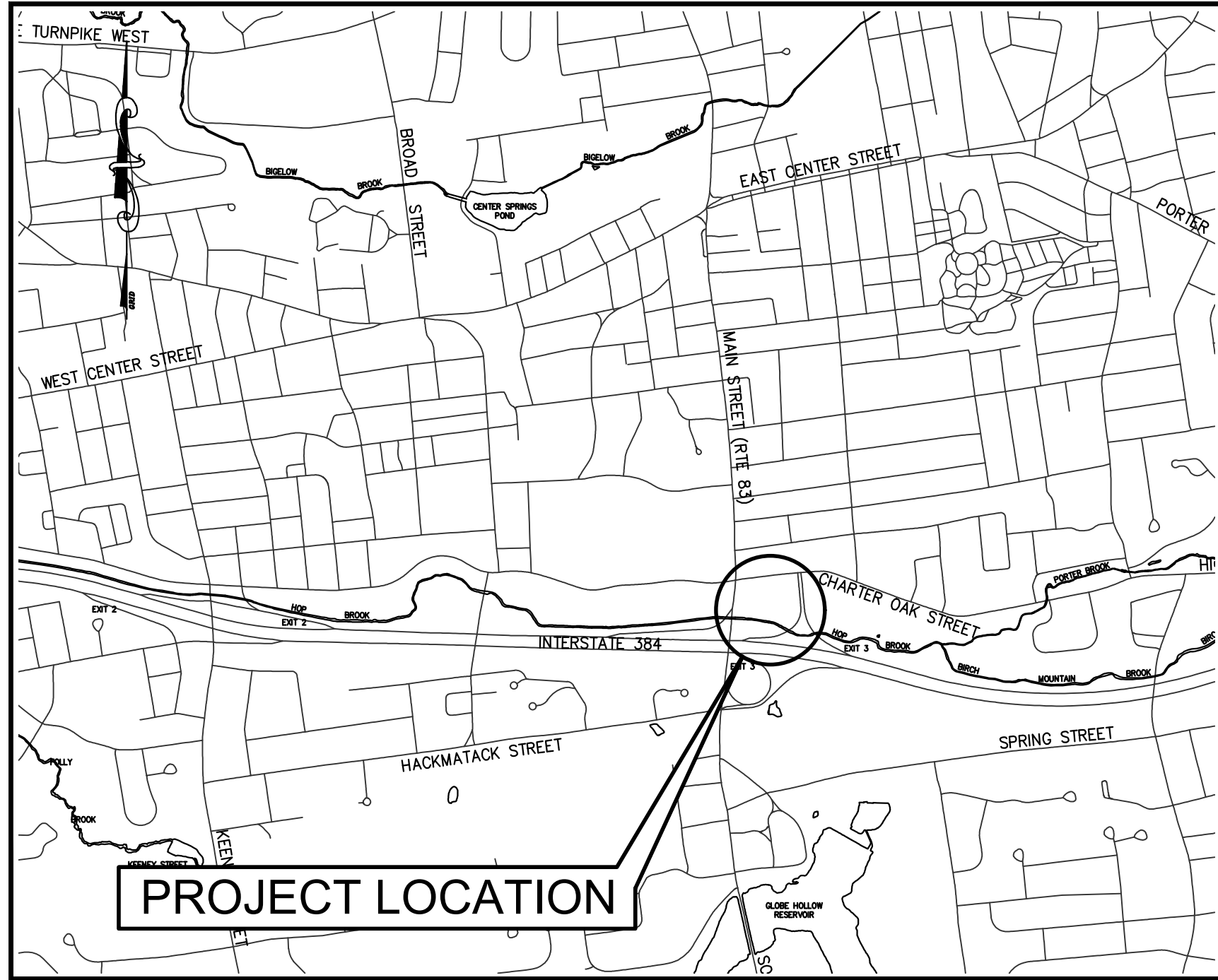
PROJECT SCHEDULE:

The project will be constructed in two phases. The first phase, which includes the synthetic turf field, utility building and parking lot is anticipated to commence this summer and take approximately six months to complete. The second phase, which includes the construction of the skate park/pump track will be completed in 2025.

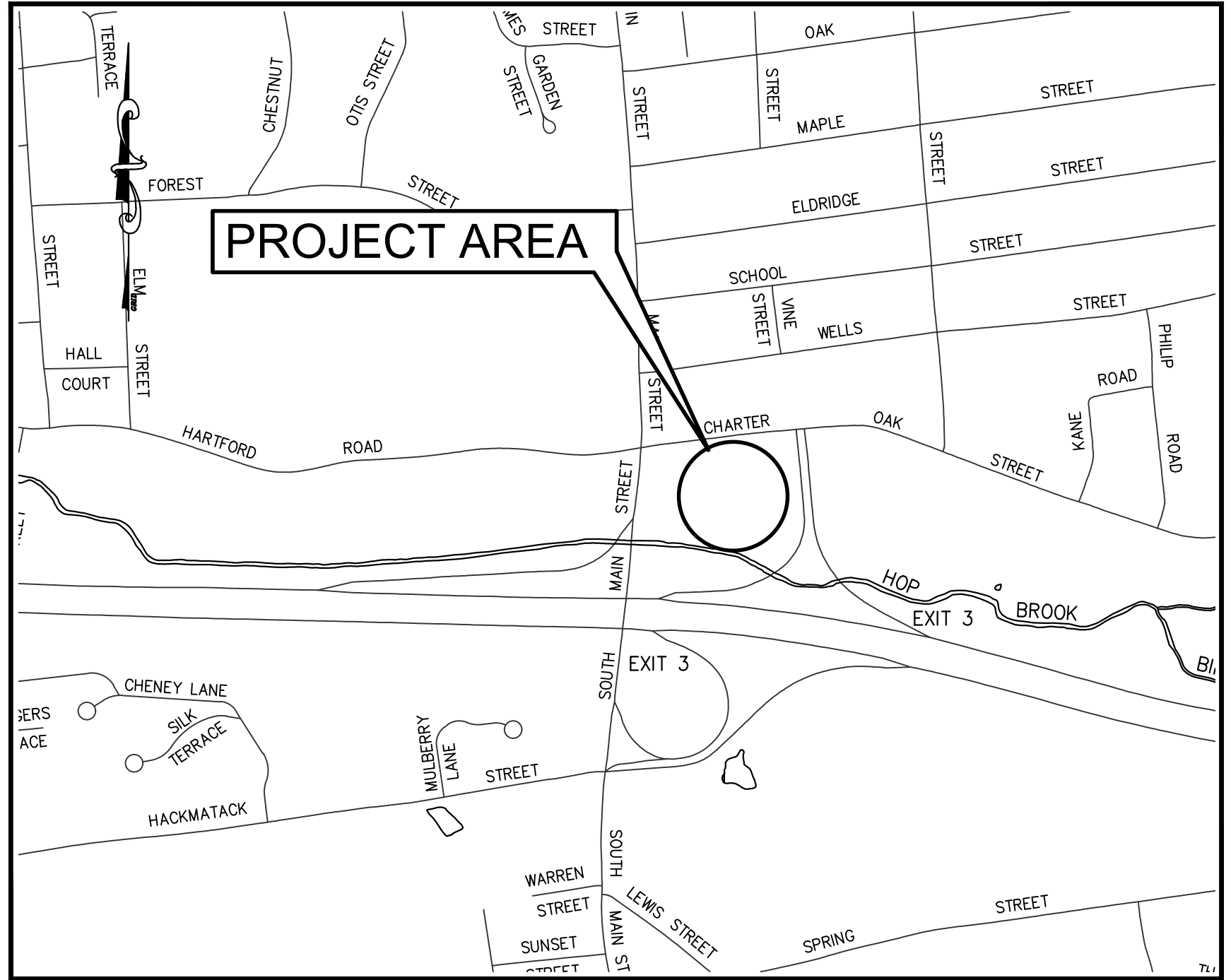
TOWN OF MANCHESTER

PUBLIC WORKS DEPARTMENT

ENGINEERING DIVISION



LOCATION MAP
1" = 1500'



SITE MAP
1" = 600'

PARK IMPROVEMENTS

CHARTER OAK PARK WEST

INLAND WETLANDS PERMIT

EROSION AND SEDIMENTATION CONTROL PERMIT

MARCH 2024

LIST OF DRAWINGS	
SHEET NO.	DESCRIPTION
1	COVER SHEET
2	SITE LAYOUT PLAN
3	UTILITY PLAN
4	GRADING PLAN
5	PAVEMENT MARKINGS AND SIGNING PLAN
6	TURF ESTABLISHMENT AND EROSION CONTROL PLAN
7	FIELD MARKING PLAN
8	UTILITY BUILDING PLAN
9-15	DETAILS
16	EXISTING CONDITIONS PLAN

DESIGN STANDARD : TOWN OF MANCHESTER PUBLIC IMPROVEMENT STANDARDS, EFFECTIVE DATE OCTOBER 31, 2020, AS AMENDED

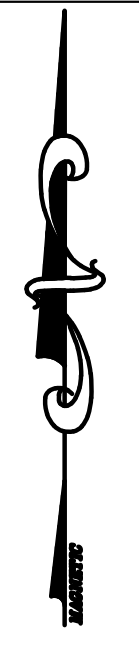
DATUMS : HORIZONTAL DATUM: TOWN OF MANCHESTER CONTROL NETWORK (NAD83 AS ESTABLISHED IN 1998)
VERTICAL DATUM: TOWN OF MANCHESTER CONTROL NETWORK (NAVD88 USING GEOID 96)

STANDARD SPECIFICATIONS : SEE CONTRACT DOCUMENTS

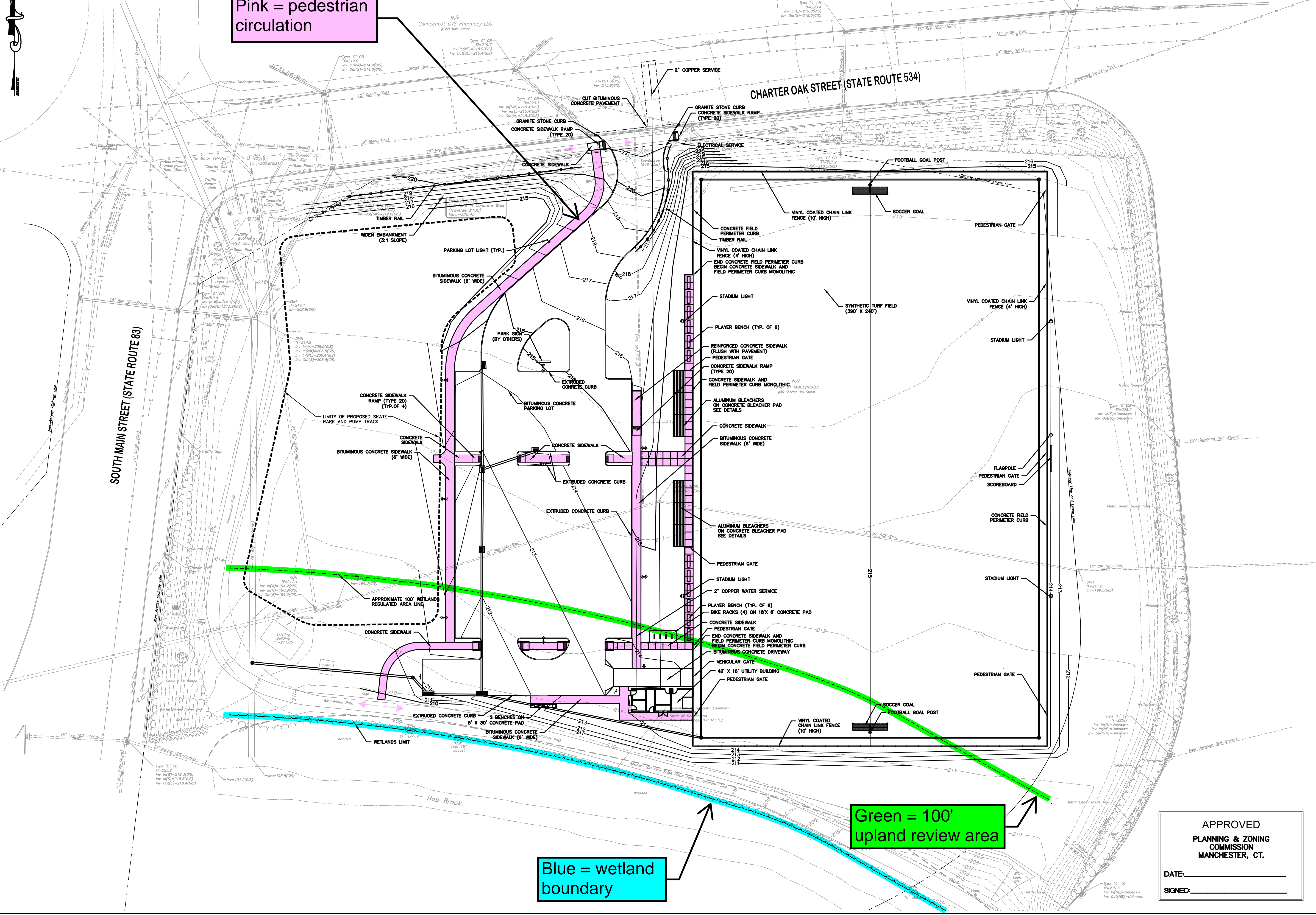
DESIGN SCALES : PLAN: 1" = 30'
OTHER SCALES AS NOTED

DESIGNED BY:
TOWN OF MANCHESTER
ENGINEERING DIVISION

JEFF LAMALVA
TOWN ENGINEER
P.E. NO. 20967



Pink = pedestrian circulation



Blue = wetland boundary

Green = 100' upland review area



TOWN OF MANCHESTER
PUBLIC WORKS DEPARTMENT
ENGINEERING DIVISION
494 MAIN STREET - P.O. BOX 191
MANCHESTER, CT 06045-0191

LEGEND

---	WETLANDS BOUNDARY	☆	LIGHT POLE
---	RETAINING WALL	⊗	CONIFEROUS TREE
---	GRASS WALL	⊗	DECIDUOUS TREE
---	STONE WALL	⊗	SANITARY MANHOLE
---	STOCKADE FENCE	⊗	DRAINAGE MANHOLE
---	WIRE FENCE	⊗	CATCH BASIN
---	CHAIN LINK FENCE	⊗	CULVERT END
---	PROPERTY LINE	⊗	HYDRANT
---	RAILROAD TRACKS	⊗	CURB STOP
---	SILT FENCE	⊗	WATER VALVE
---	CONCRETE MONUMENT	⊗	BUTTERFLY VALVE
---	GRANITE MONUMENT	⊗	BLOW OFF
---	IRON PIPE	⊗	SIGN
---	IRON ROD	⊗	DOUBLE POST SIGN
---	CONTROL POINT	⊗	MAIL BOX
---	DRILL HOLE	⊗	ROLLBAR
---	UTILITY POLE	⊗	CONTROLLER CABINET
---	UTILITY POLE WITH LIGHT	⊗	GAS GATE
---	TRAFFIC SPAN POLE	⊗	TELEPHONE BOX
---	ELECTRIC BOX	⊗	CATV TUBE
---	WETLAND FLAG		

PROJECT NUMBER
2021078

FILENAME
2021078-PLAN-PZC.DWG

NO.	DATE	FILE
-	03/15/24	PZC PERMIT SUBMISSION

DRAWN BY:
CHECKED BY: JL
RELEASED BY: TB

DRAWING SCALE
HORIZONTAL: 1" = 30'
VERTICAL: ---
OR AS NOTED
30 15 0 30
GRAPHIC SCALE

DATUM
HORIZONTAL: NAD83 VERTICAL: NAVD88

PROJECT LOCATION
CHARTER OAK STREET
MANCHESTER, CT

PROJECT TITLE
PARK IMPROVEMENTS
CHARTER OAK WEST

SHEET TITLE
SITE PLAN

SHEET NUMBER
2 of 16

APPROVED
PLANNING & ZONING
COMMISSION
MANCHESTER, CT.
DATE: _____
SIGNED: _____



TOWN OF MANCHESTER
PUBLIC WORKS DEPARTMENT
ENGINEERING DIVISION
494 MAIN STREET - P.O. BOX 191
MANCHESTER, CT 06045-0191

LEGEND

--- WETLANDS BOUNDARY	☆ LIGHT POLE
--- RETAINING WALL	⊛ CONIFEROUS TREE
--- CURB WALL	⊛ DEODOROUS TREE
--- STONE WALL	⊛ SANITARY MANHOLE
--- STOCKADE FENCE	⊛ DRAINAGE MANHOLE
--- WIRE FENCE	⊛ CATCH BASIN
--- CHAIN LINK FENCE	⊛ CULVERT END
--- PROPERTY LINE	⊛ HYDRANT
--- RAILROAD TRACKS	⊛ CURB STOP
--- SILT FENCE	⊛ WATER VALVE
⊛ CONCRETE MONUMENT	⊛ BUTTERFLY VALVE
⊛ GRANITE MONUMENT	⊛ BLOW OFF
⊛ IRON PIPE	⊛ SIGN
⊛ IRON ROD	⊛ DOUBLE POST SIGN
⊛ CONTROL POINT	⊛ MAIL BOX
⊛ DRILL HOLE	⊛ BOLLARD
⊛ UTILITY POLE	⊛ CONTROLLER CABINET
⊛ UTILITY POLE WITH LIGHT	⊛ GAS GATE
⊛ ELECTRIC BOX	⊛ TELEPHONE BOX
⊛ WETLAND FLAG	⊛ CATV TUBE

PROJECT NUMBER
2021078

FILENAME
2021078-PLAN-PZC.DWG

NO.	DATE	FILE
-	03/15/24	PZC PERMIT SUBMISSION

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GRAPHIC SCALE

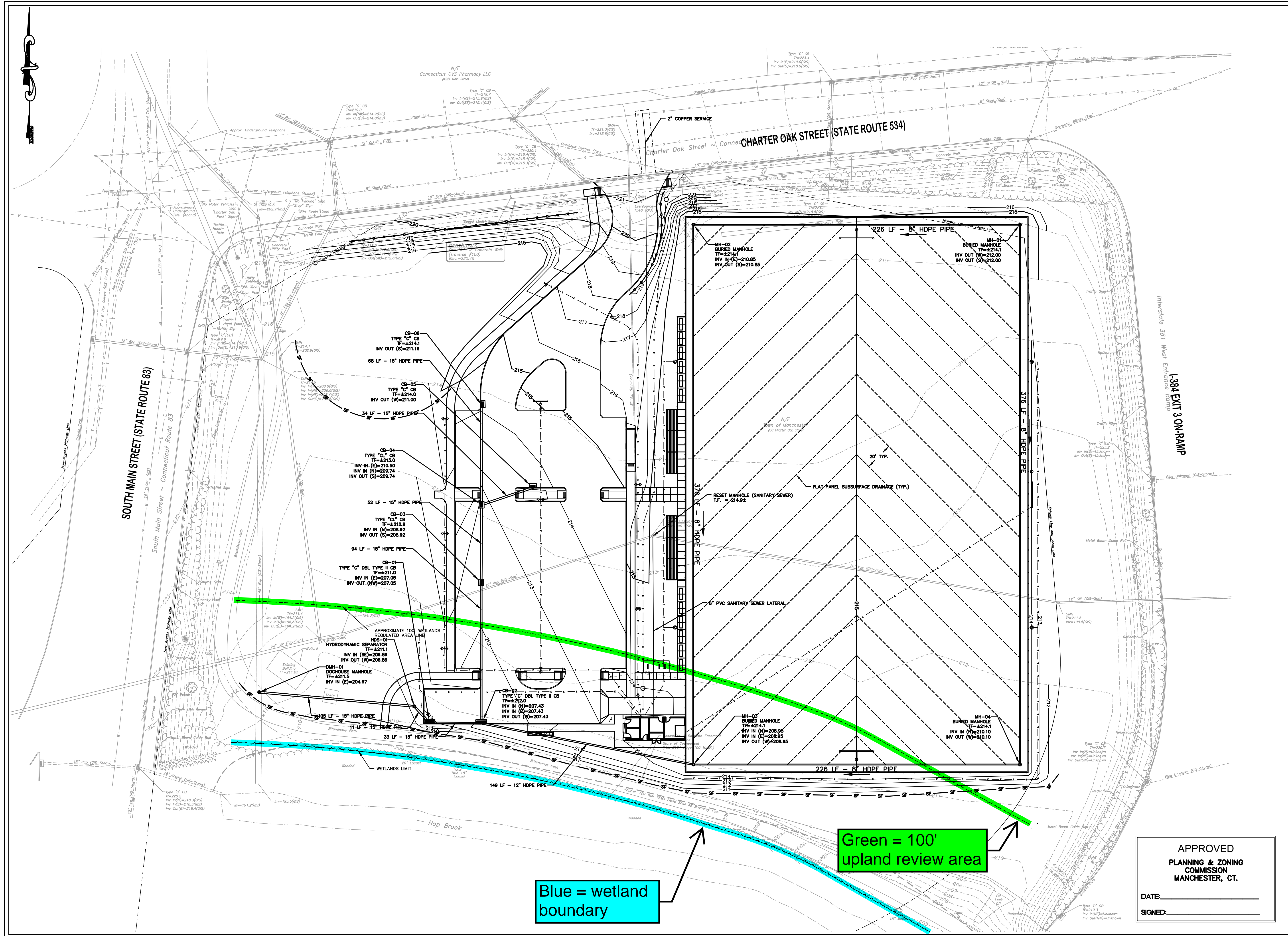
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PROJECT LOCATION
**CHARTER OAK STREET
MANCHESTER, CT**

PROJECT TITLE
**PARK IMPROVEMENTS
CHARTER OAK WEST**

SHEET TITLE
UTILITY PLAN

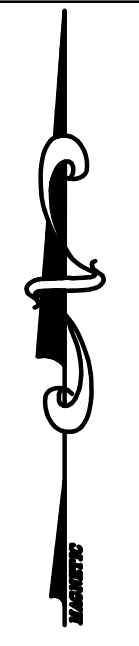
SHEET NUMBER
3 of 16



Blue = wetland boundary

Green = 100' upland review area

APPROVED
PLANNING & ZONING
COMMISSION
MANCHESTER, CT.
DATE: _____
SIGNED: _____



TOWN OF MANCHESTER CONNECTICUT SEAL

TOWN OF MANCHESTER
PUBLIC WORKS DEPARTMENT
ENGINEERING DIVISION
494 MAIN STREET - P.O. BOX 191
MANCHESTER, CT 06045-0191

LEGEND

--- WETLANDS BOUNDARY	☆ LIGHT POLE
--- RETAINING WALL	⊛ CONIFEROUS TREE
--- QUOTE WALL	⊛ DEODOROUS TREE
--- STONE WALL	⊛ SANITARY MANHOLE
--- STOCKADE FENCE	⊛ DRAINAGE MANHOLE
--- WIRE FENCE	⊛ CATCH BASIN
--- CHAIN LINK FENCE	⊛ CULVERT END
--- PROPERTY LINE	⊛ HYDRANT
--- RAILROAD TRACKS	⊛ CURB STOP
--- SILT FENCE	⊛ WATER VALVE
⊛ CONCRETE MONUMENT	⊛ BUTTERFLY VALVE
⊛ GRANITE MONUMENT	⊛ BLOW OFF
⊛ IRON PIPE	⊛ SIGN
⊛ IRON ROD	⊛ DOUBLE POST SIGN
⊛ CONTROL POINT	⊛ MAIL BOX
⊛ DRILL HOLE	⊛ BOLLARD
⊛ UTILITY POLE	⊛ CONTROLLER CABINET
⊛ UTILITY POLE WITH LIGHT	⊛ GAS GATE
⊛ TRAFFIC SPAN POLE	⊛ TELEPHONE BOX
⊛ ELEC BOX	⊛ CATV TUBE

PROJECT NUMBER
2021078

FILENAME
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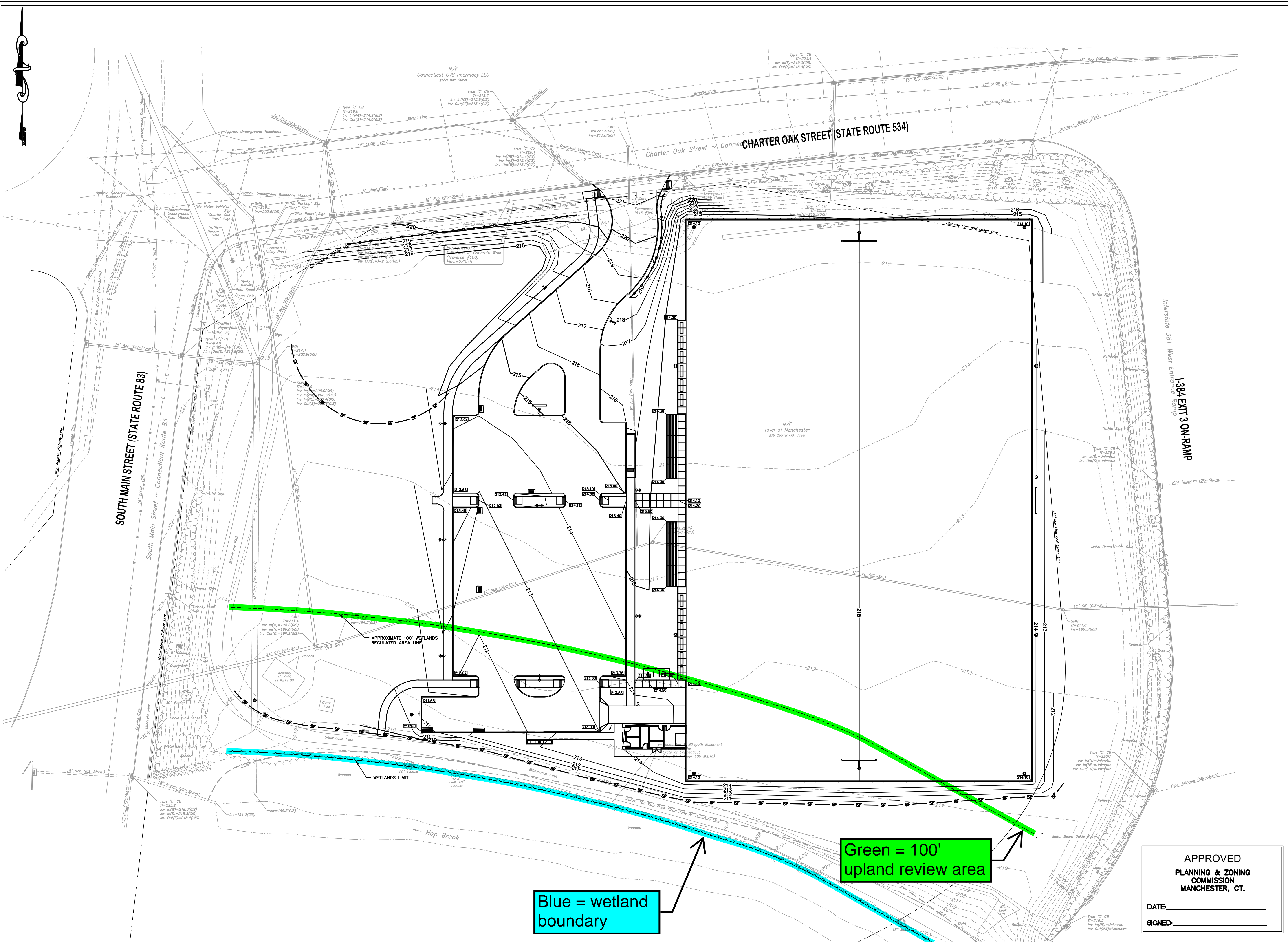
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PROJECT LOCATION
**CHARTER OAK STREET
MANCHESTER, CT**

PROJECT TITLE
**PARK IMPROVEMENTS
CHARTER OAK WEST**

SHEET TITLE
GRADING PLAN

SHEET NUMBER
4 of 16



Blue = wetland boundary

Green = 100' upland review area

APPROVED
PLANNING & ZONING
COMMISSION
MANCHESTER, CT.
DATE: _____
SIGNED: _____



TOWN OF MANCHESTER
PUBLIC WORKS DEPARTMENT
ENGINEERING DIVISION
494 MAIN STREET - P.O. BOX 191
MANCHESTER, CT 06045-0191

LEGEND

--- WETLANDS BOUNDARY	☆ LIGHT POLE
--- RETAINING WALL	⊗ CONIFEROUS TREE
--- QUOTE WALL	⊗ DEODOROUS TREE
--- STONE WALL	⊗ SANITARY MANHOLE
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--- PROPERTY LINE	⊗ HYDRANT
--- RAILROAD TRACKS	⊗ CURB STOP
--- SILT FENCE	⊗ WATER VALVE
⊗ CONCRETE MONUMENT	⊗ BUTTERFLY VALVE
⊗ GRANITE MONUMENT	⊗ BLOW OFF
⊗ IRON PIPE	⊗ SIGN
⊗ IRON ROD	⊗ DOUBLE POST SIGN
⊗ CONTROL POINT	⊗ MAIL BOX
⊗ DRILL HOLE	⊗ BOLLARD
⊗ UTILITY POLE	⊗ CONTROLLER CABINET
⊗ UTILITY POLE WITH LIGHT	⊗ GAS GATE
⊗ TRAFFIC SPAN POLE	⊗ TELEPHONE BOX
⊗ ELECTRIC BOX	⊗ CATV TUBE
⊗ WETLAND FLAG	

PROJECT NUMBER
2021078

FILENAME
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NO.	DATE	FILE
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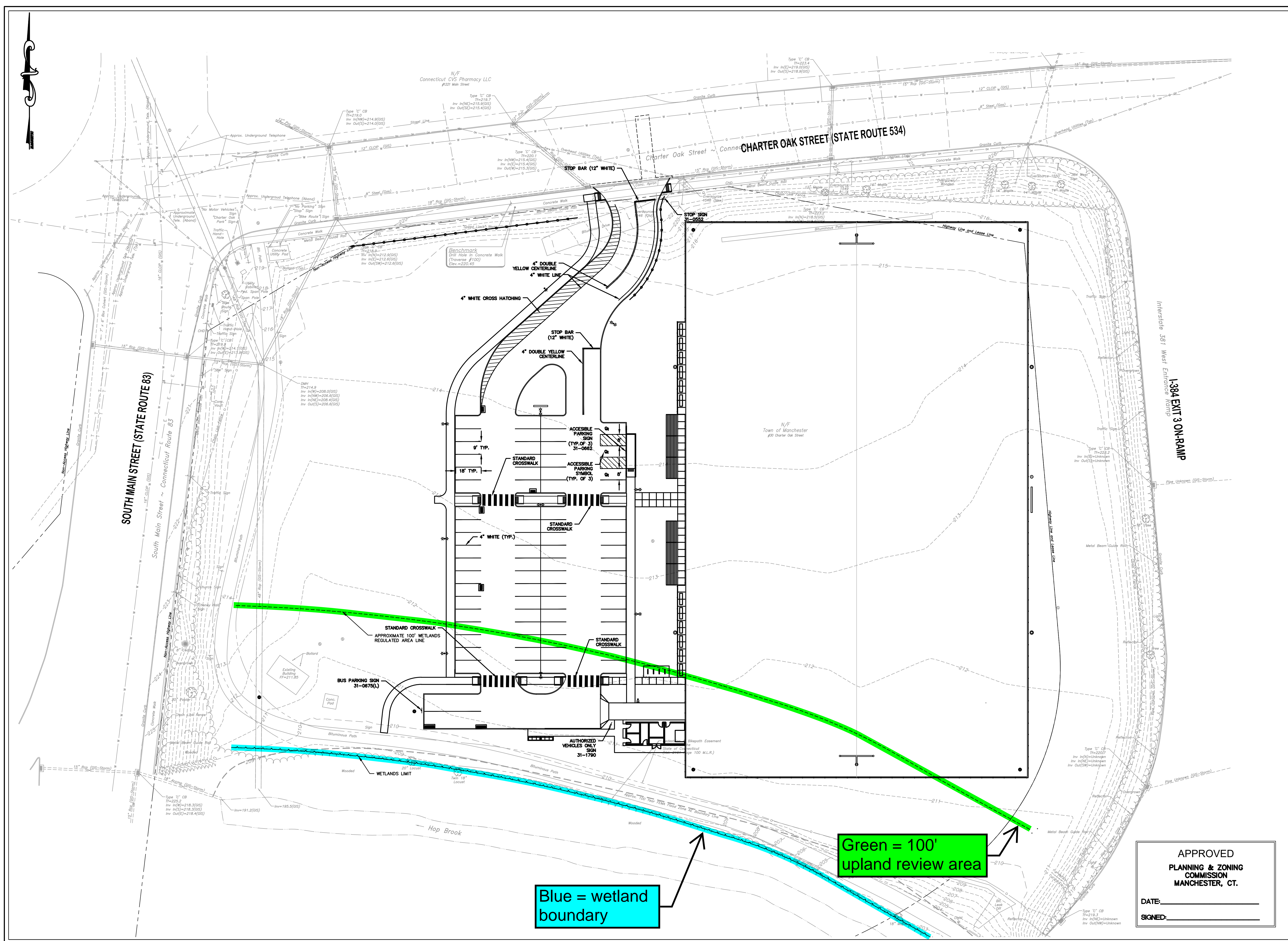
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PROJECT LOCATION
**CHARTER OAK STREET
MANCHESTER, CT**

PROJECT TITLE
**PARK IMPROVEMENTS
CHARTER OAK WEST**

SHEET TITLE
**PAVEMENT MARKING
AND SIGNING PLAN**

SHEET NUMBER
5 of 16



APPROVED
PLANNING & ZONING
COMMISSION
MANCHESTER, CT.
DATE: _____
SIGNED: _____



TOWN OF MANCHESTER
PUBLIC WORKS DEPARTMENT
ENGINEERING DIVISION
494 MAIN STREET - P.O. BOX 191
MANCHESTER, CT 06045-0191

LEGEND

--- WETLANDS BOUNDARY	☆ LIGHT POLE
--- RETAINING WALL	⊗ CONIFEROUS TREE
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⊗ GRANITE MONUMENT	⊗ BUTTERFLY VALVE
⊗ IRON PIPE	⊗ BLOW OFF
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⊗ TRAFFIC SPAN POLE	⊗ GAS GATE
⊗ ELECING BOX	⊗ TELEPHONE BOX
⊗ WETLAND FLAG	⊗ CATV TUBE

PROJECT NUMBER
2021078

FILENAME
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NO.	DATE	FILE
-	03/15/24	PZC PERMIT SUBMISSION

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GRAPHIC SCALE

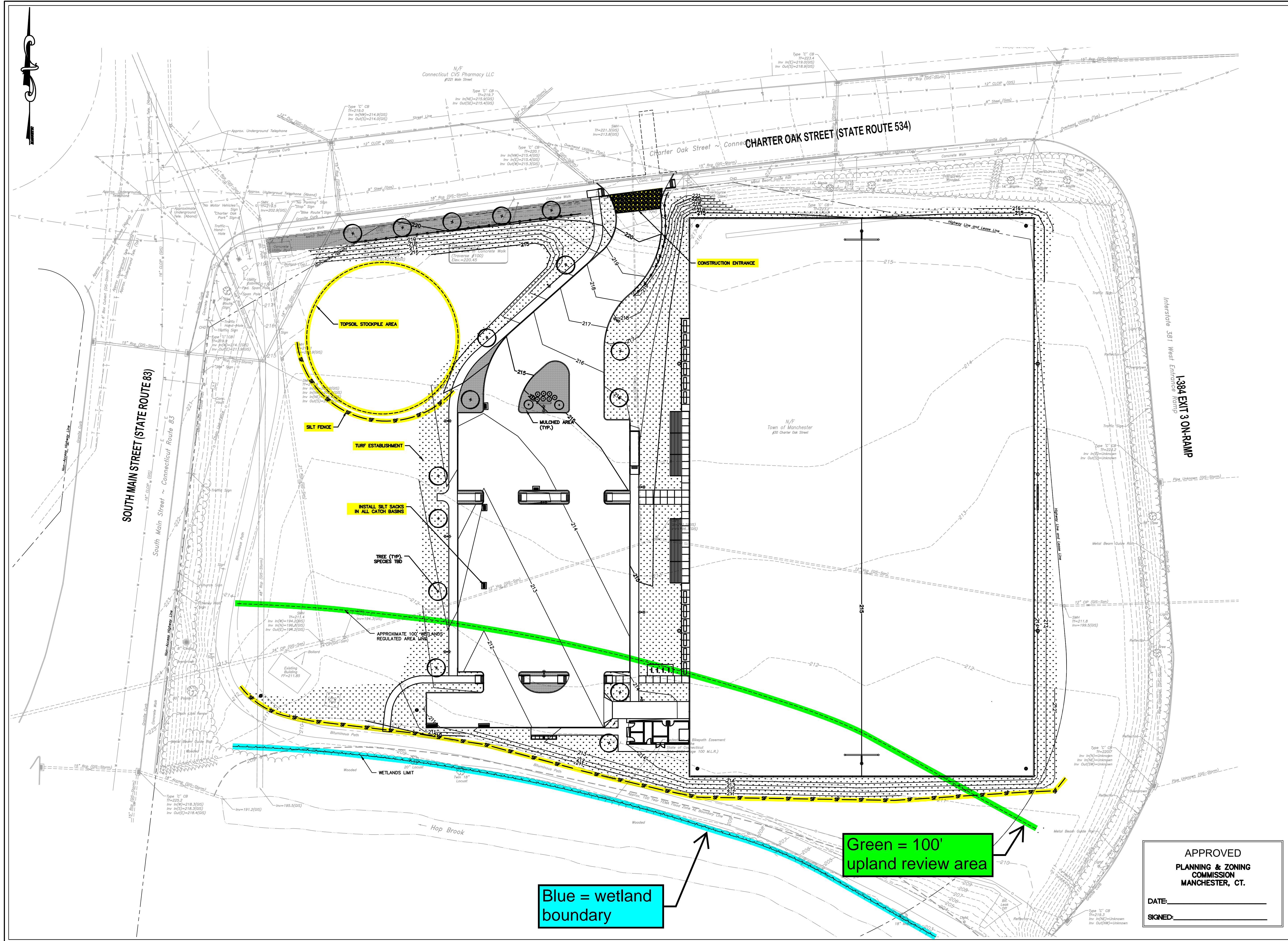
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PROJECT LOCATION
**CHARTER OAK STREET
MANCHESTER, CT**

PROJECT TITLE
**PARK IMPROVEMENTS
CHARTER OAK WEST**

SHEET TITLE
**TURF ESTABLISHMENT &
EROSION/SEDIMENTATION
CONTROL PLAN**

SHEET NUMBER
6 of 16



Blue = wetland boundary

Green = 100' upland review area

APPROVED
PLANNING & ZONING
COMMISSION
MANCHESTER, CT.
DATE: _____
SIGNED: _____



TOWN OF MANCHESTER
PUBLIC WORKS DEPARTMENT
ENGINEERING DIVISION
494 MAIN STREET - P.O. BOX 191
MANCHESTER, CT 06045-0191

LEGEND

- - -	WETLANDS BOUNDARY	☆	LIGHT POLE
- - -	RETAINING WALL	⊗	CONIFEROUS TREE
—	GRASS WALL	⊙	DECIDUOUS TREE
—	STONE WALL	⊕	SANITARY MANHOLE
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—	PROPERTY LINE	⊗	HYDRANT
—	RAILROAD TRACKS	⊗	CURB STOP
—	SILT FENCE	⊗	WATER VALVE
□	CONCRETE MONUMENT	⊗	BUTTERFLY VALVE
■	GRANITE MONUMENT	⊗	BLOW OFF
○	IRON PIPE	⊗	SIGN
●	IRON ROD	⊗	DOUBLE POST SIGN
△	CONTROL POINT	⊗	MAIL BOX
●	DRILL HOLE	⊗	BOLLARD
○	UTILITY POLE	⊗	CONTROLLER CABINET
⊗	UTILITY POLE WITH LIGHT	⊗	GAS GATE
⊗	TRAFFIC SPAN POLE	⊗	TELEPHONE BOX
⊗	ELECTRIC BOX	⊗	CATV TUBE
⊗	WETLAND FLAG		

PROJECT NUMBER
2021078

FILENAME
2021078-PLAN-PZC.DWG

NO.	DATE	FILE
-	03/15/24	PZC PERMIT SUBMISSION

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VERTICAL: ---
OR AS NOTED
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GRAPHIC SCALE

DATUM
HORIZONTAL: NAD83 VERTICAL: NAVD88

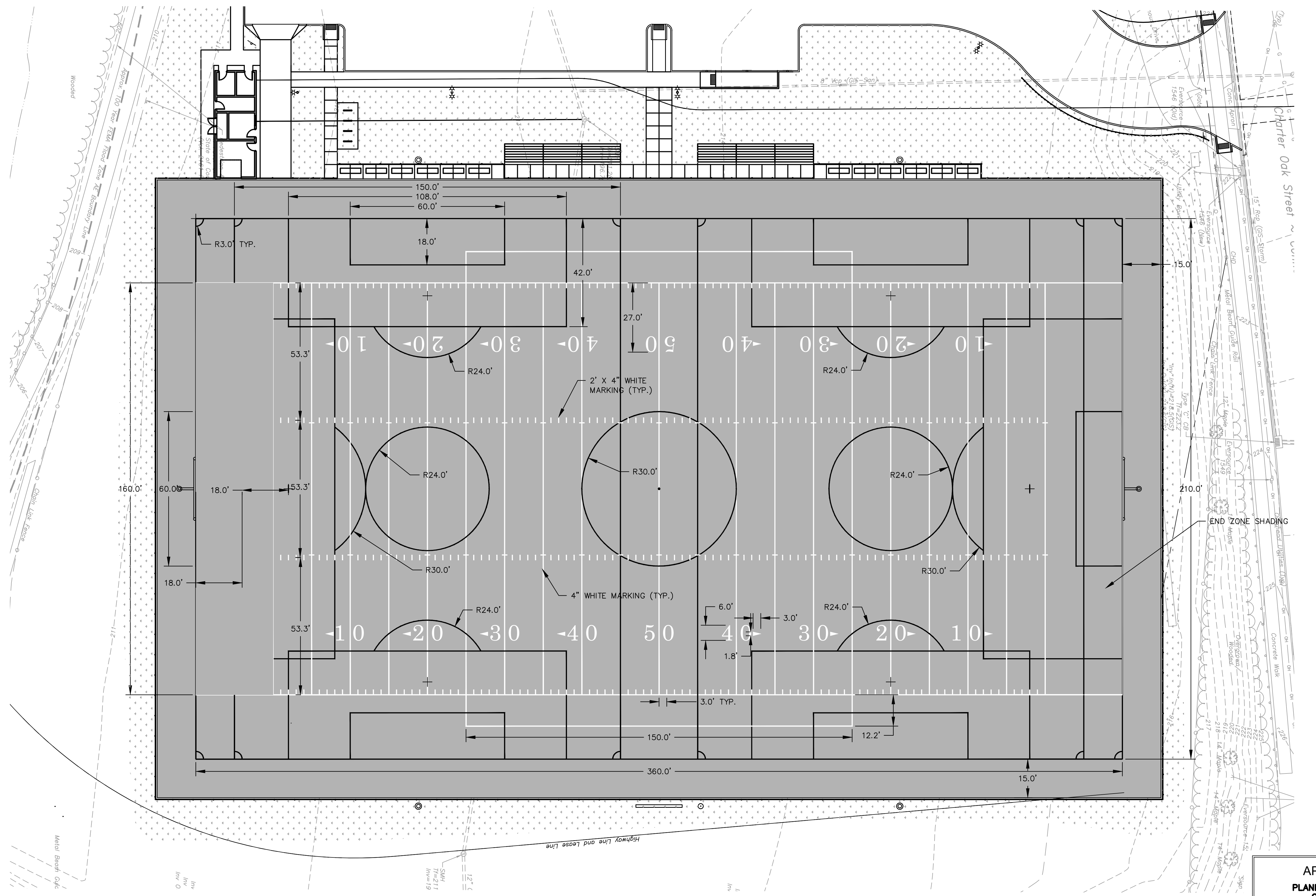
PROJECT LOCATION
**CHARTER OAK STREET
MANCHESTER, CT**

PROJECT TITLE
**PARK IMPROVEMENTS
CHARTER OAK WEST**

SHEET TITLE
**FIELD MARKINGS
PLAN**

SHEET NUMBER
7 of 16

APPROVED
PLANNING & ZONING
COMMISSION
MANCHESTER, CT.
DATE: _____
SIGNED: _____





TOWN OF MANCHESTER
PUBLIC WORKS DEPARTMENT
ENGINEERING DIVISION
494 MAIN STREET - P.O. BOX 191
MANCHESTER, CT 06045-0191

LEGEND

—	RETAINING WALL	☆	LIGHT POLE
—	STONE WALL	⊗	CONIFEROUS TREE
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—	STOCKADE FENCE	⊕	SANITARY MANHOLE
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—	RAILROAD TRACKS	⊗	CULVERT END
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⊗	CONCRETE MONUMENT	⊗	CURB STOP
⊗	GRANITE MONUMENT	⊗	WATER VALVE
⊗	IRON PIPE	⊗	BUTTERFLY VALVE
⊗	IRON ROD	⊗	BLOW OFF
⊗	CONTROL POINT	⊗	SIGN
⊗	DRILL HOLE	⊗	DOUBLE POST SIGN
⊗	UTILITY POLE	⊗	MAL. BOX
⊗	UTILITY POLE WITH LIGHT	⊗	BOLLARD
⊗	ELECTRIC BOX	⊗	GAS GATE
⊗	WETLAND FLAG	⊗	CONTROLLER CABINET
⊗		⊗	TELEPHONE BOX
⊗		⊗	CATV TUBE

PROJECT NUMBER
2021078

FILENAME
2021078-PLAN-PZC.DWG

NO.	DATE	FILE
—	03/15/24	PZC PERMIT SUBMISSION

DRAWN BY:
CHECKED BY: JL
RELEASED BY: TB

DATUM
HORIZONTAL: NAD83 VERTICAL: NAVD88

PROJECT LOCATION
**CHARTER OAK STREET
MANCHESTER, CT**

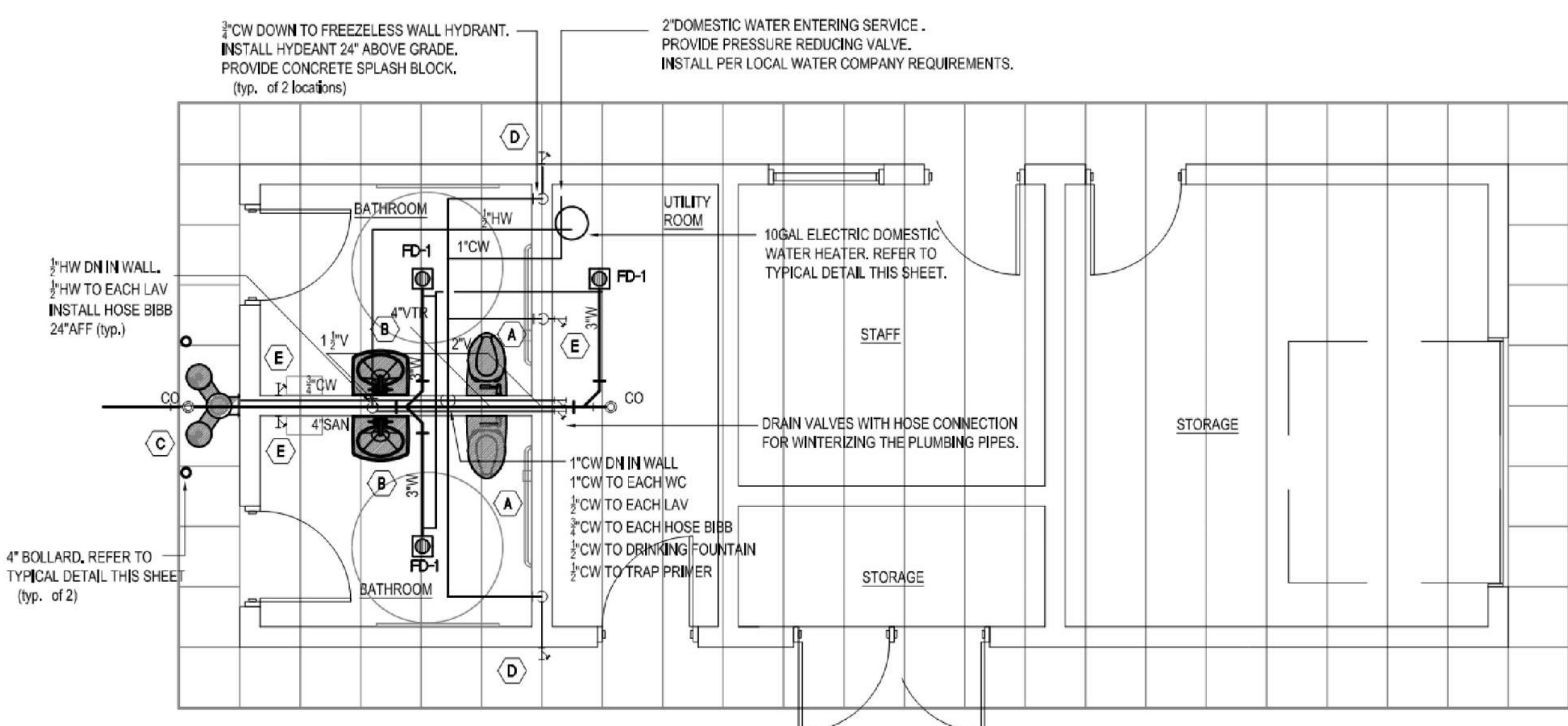
PROJECT TITLE
**PARK IMPROVEMENTS
CHARTER OAK WEST**

SHEET TITLE
**UTILITY BUILDING
PLAN**

SHEET NUMBER
8 of 16

PLUMBING SYMBOL LIST

SYMBOL	ABBREY.	DESCRIPTION
—	CW	COLD WATER
—	HW	HOT WATER
—	V	VENT
—	SAN or W	SANITARY OR WASTE ABOVE SLAB/FLOOR
—	SAN or W	SANITARY OR WASTE BELOW SLAB/FLOOR
—		SHUT-OFF VALVE
—		RISER DOWN
—		RISER UP
—		TEE DOWN
—		PIPE CAP
—		FITCH DOWN IN DIRECTION OF ARROW
—		FLOW IN DIRECTION OF ARROW
⊗	FD	FLOOR DRAIN WITH P-TRAP
⊗	CO	FLOOR CLEAN OUT
⊗	WCO	WALL CLEAN OUT



- GENERAL NOTES:**
- DRAWINGS ARE DIAGNOSTIC AND SHOW GENERAL INTENT OF WORK. ALL CONTRACTORS MUST COORDINATE WITH OTHER TRADES OTHER TRADES BEFORE PROCEEDING WITH ANY WORK.
 - THE CONTRACTOR SHALL COORDINATE THE ROUTING AND INSTALLATION OF PLUMBING SYSTEMS TO AVOID CONFLICTS.
 - THE CONTRACTOR SHALL VISIT THE SITE AND FAMILIARIZE HIMSELF WITH CONDITIONS PRIOR TO SUBMITTING HIS BID.
 - INSTALL ALL HANDICAP ACCESSIBLE FIXTURES IN ACCORDANCE WITH ADA, UFAS AND ANSI REQUIREMENTS.
 - ALL WASTE PIPE 3" AND LARGER SHALL HAVE A FITCH OF 1/8" PER FT.
 - ALL UNDER SLAB DRAINAGE PIPING SHALL BE IN PVC.
 - MINIMUM SIZE FOR ALL UNDER SLAB DRAINAGE PIPING SHALL BE 3 INCH.
 - PROVIDE ESCUTCHEON PLATE @ ALL WALL/CILING PENETRATIONS.
 - THIS CONTRACTOR SHALL PROVIDE CLEANOUTS WITH COVER (ACCESS DOORS) ON WASTE STACKS @ CHANGE OF DIRECTION AND SLAB PENETRATIONS.
 - THERE SHALL BE NO EXPOSED PIPING. PIPES SHALL RUN CONCEALED ABOVE THE CEILING OR IN WALLS, WHERE NOT POSSIBLE, THE GENERAL CONTRACTOR SHALL PROVIDE PIPE CHASSES, ON EXTERIOR WALLS, PIPES SHALL RUN ON THE WARM SIDE OF THE INSULATION AND HAVE 2" OF INSULATION, PROVIDE PVC COVER FOR INSULATED PIPES.

PLUMBING FIXTURE SCHEDULE- UNITS

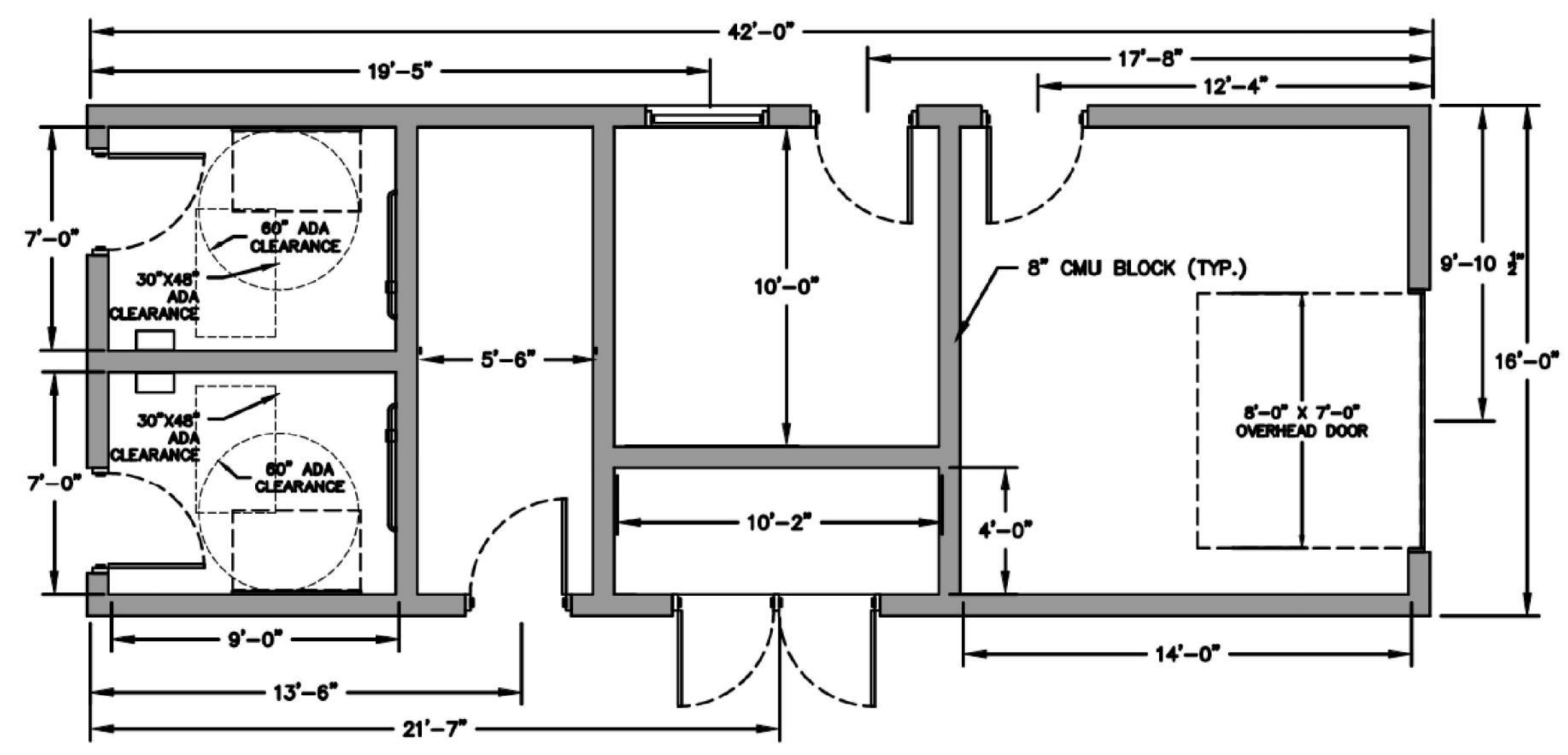
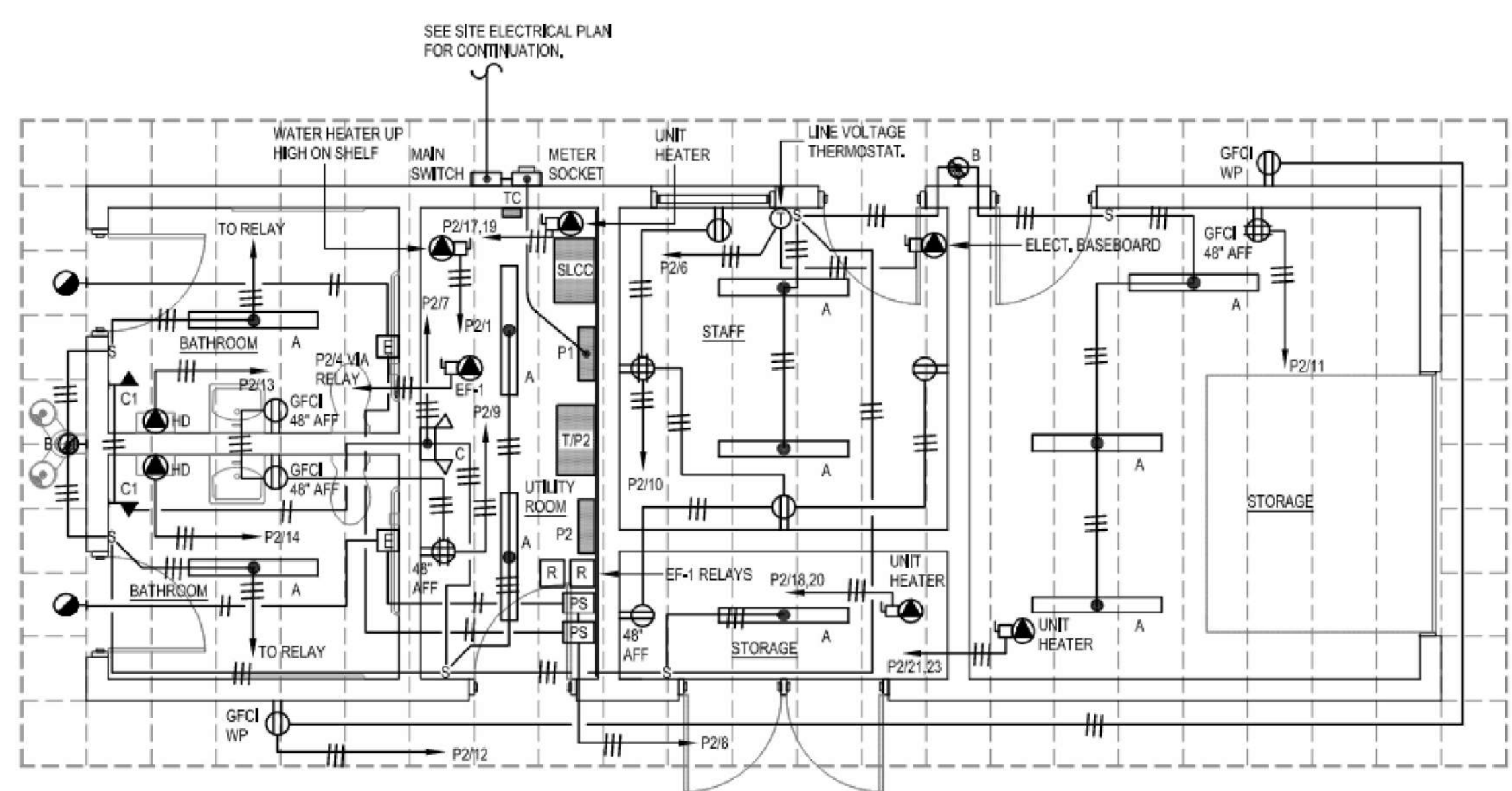
FIXTURE TYPE	DESCRIPTION	MANUF.	MODEL	CONNECTION				REMARKS
				CW	HW	W/SAN	V	
A	WATER CLOSET (ACCESSIBLE)	KOHLER	KINGSTON ULTRA K-6432S	1"	—	4"	2"	1.2
B	FLUSH VALVE (ACCESSIBLE)	SLOAN	WES-111-J/L1	1"	—	4"	2"	1.2
C	LAVATORY (ACCESSIBLE)	KOHLER	FINIX K-2035-1	1/2"	1/2"	1 1/2"	1 1/4"	1.3, 4
D	FAUCET	CHICAGO	3500-4E285ABCP	1/2"	—	1 1/2"	1 1/4"	1.2
E	DRINKING FOUNTAIN (ACCESSIBLE)	ELKAY	LK4408FP	1/2"	—	1 1/2"	1 1/4"	1.2
F	FREEZELESS WALL HYDRANT	WOODFORD	65	3/4"	—	—	—	—
G	HOSE BIBB	WOODFORD	B26	3/4"	—	—	—	—

- REMARKS:
- REFER TO ARCHITECTURAL DRAWINGS FOR FIXTURE MOUNTING HEIGHT.
 - TOILET SEAT SHALL BE MODEL K-4731 BY KOHLER.
 - PROVIDE FRIBRO PIPE INSULATION FOR ALL ACCESSIBLE LAVATORIES AND SINKS.
 - SINGLE FAUCET HOLE.
 - PROVIDE WITH QUICK CONNECT HAND HELD SHOWER.
 - PROVIDE MODEL 830 AA FAUCET, HOSE AND HOSE BRACKET, WOP HANGER, 3" QUICK DRAIN CONNECTOR, VINYL BUMPER GUARD, STAINLESS STEEL WALL GUARD, PROVIDE AIR ADMITTANCE VALVE BY STUDDORVENT.
 - PROVIDE STAINLESS STEEL TOP ENCLOSURE COVER TO MATCH SHOWER SYSTEM.

FLOOR DRAIN SCHEDULE

UNIT No.	MANUF.	MODEL	SIZE (OUTLET/ STRAINER)	LOCATION	TRAP PRIMER	REMARKS
FD-1	JAY R SMITH	20052944MB-43	3"0	TOILET RM.	P250	1.3
CO	JAY R SMITH	4623				2.3
WCO	JAY R SMITH	4422				4

- REMARKS:
- FLOOR DRAINS SHALL HAVE DUCO CAST IRON BODY WITH 4" HUB OUTLET, DEEP SEAL TRAP SEDIMENT BUCKET, NICKEL BRONZE STRAINER, VANDAL PROOF SCREWS TRAP PRIMER CONNECTION.
 - FLOOR CLEANOUT WITH BRONZE TAPER THREAD PLUG, ADJUSTABLE WEDGE, BRONZE TOP.
 - FLOOR DRAINS AND CLEANOUTS TO MATCH FINISHED FLOOR USAGE AND FINISH SURFACE.
 - WALL CLEANOUT WITH STAINLESS STEEL COVER.



APPROVED
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MANCHESTER, CT.

DATE: _____
SIGNED: _____



TOWN OF MANCHESTER
PUBLIC WORKS DEPARTMENT
ENGINEERING DIVISION
494 MAIN STREET - P.O. BOX 191
MANCHESTER, CT 06045-0191

LEGEND

—	WETLAND BOUNDARY	☆	LIGHT POLE
—	RETAINING WALL	⊗	CONIFEROUS TREE
—	STONE WALL	⊗	DECIDUOUS TREE
—	STOCKADE FENCE	⊗	SANITARY MANHOLE
—	WIRE FENCE	⊗	DRAINAGE MANHOLE
—	CHAIN LINK FENCE	⊗	CATCH BASIN
—	PROPERTY LINE	⊗	CULVERT END
—	RAILROAD TRACKS	⊗	HYDRANT
—	SILT FENCE	⊗	CURB STOP
—	CONCRETE MONUMENT	⊗	WATER VALVE
—	GRANITE MONUMENT	⊗	BUTTERFLY VALVE
—	IRON PIPE	⊗	BLOW OFF
—	IRON ROD	⊗	SIGN
—	CONTROL POINT	⊗	DOUBLE POST SIGN
—	DRILL HOLE	⊗	MAIL BOX
—	UTILITY POLE	⊗	BOLLARD
—	UTILITY POLE WITH LIGHT	⊗	CONTROLLER CABINET
—	TRAFFIC SPAN POLE	⊗	GAS GATE
—	ELECTRIC BOX	⊗	TELEPHONE BOX
—	WETLAND FLAG	⊗	CATV TUBE

PROJECT NUMBER
2021078

FILENAME
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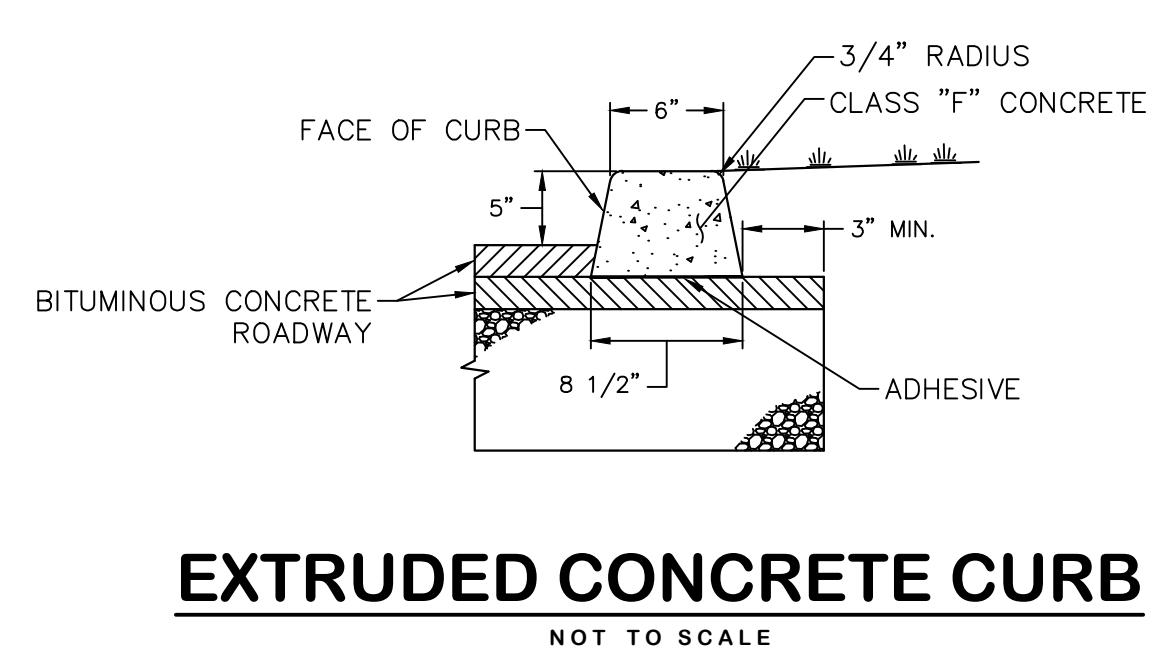
DATUM
HORIZONTAL: NAD83 VERTICAL: NAVD88

PROJECT LOCATION
**CHARTER OAK STREET
MANCHESTER, CT**

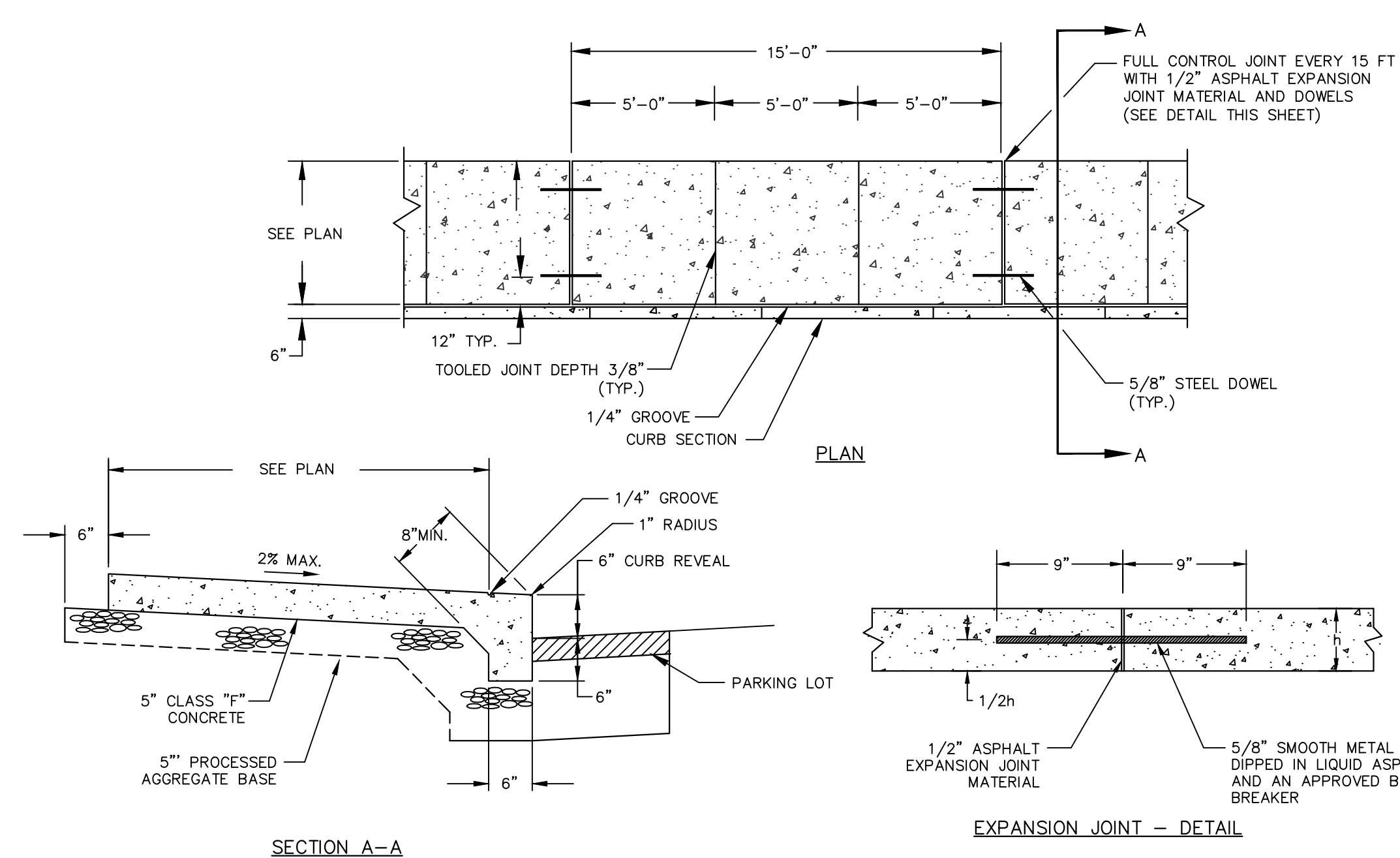
PROJECT TITLE
**PARK IMPROVEMENTS
CHARTER OAK WEST**

SHEET TITLE
DETAILS

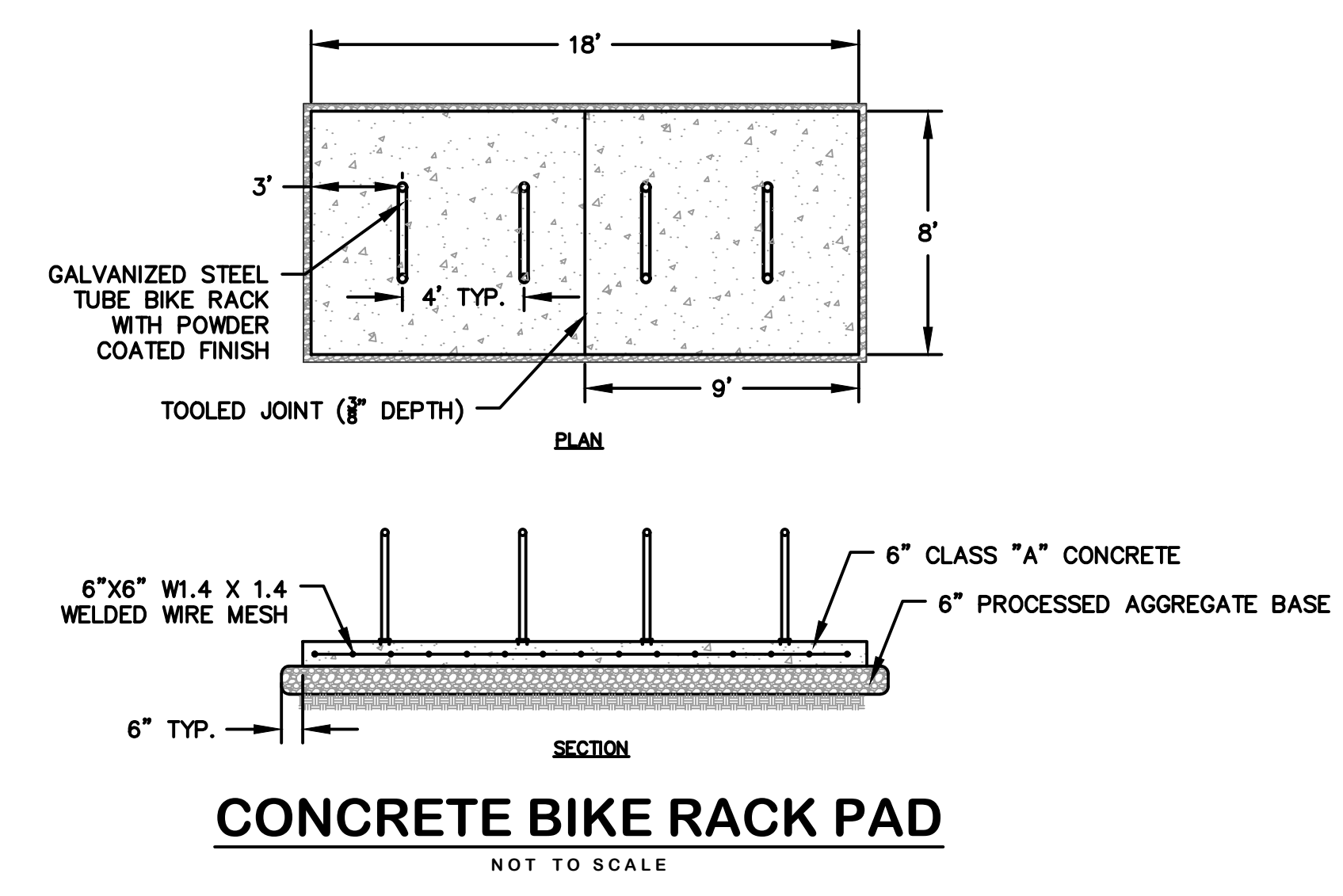
SHEET NUMBER
9 of 16



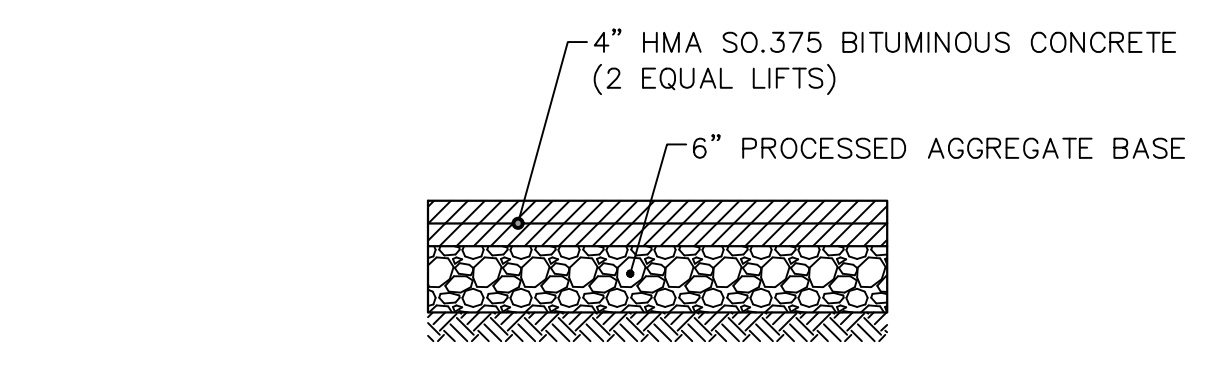
EXTRUDED CONCRETE CURB
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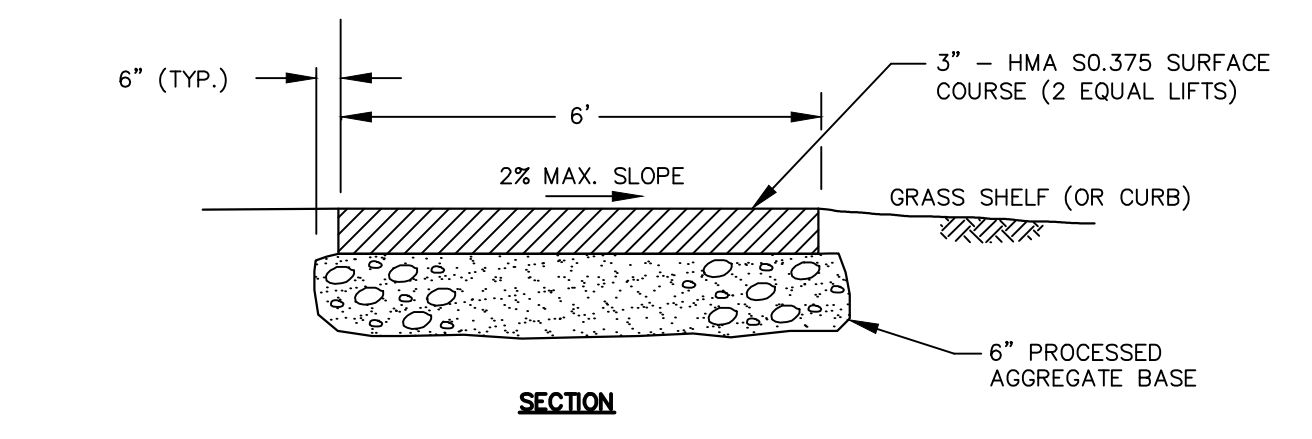
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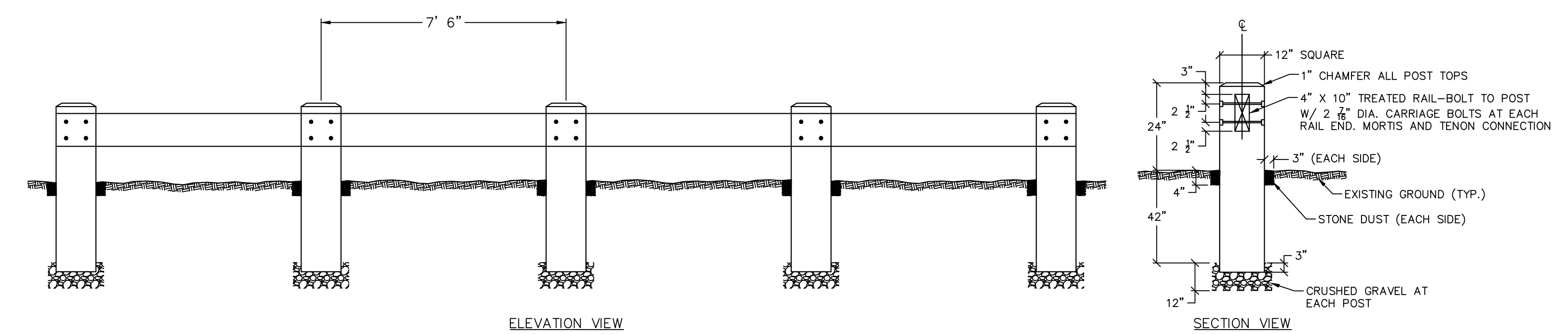
CONCRETE BIKE RACK PAD
NOT TO SCALE



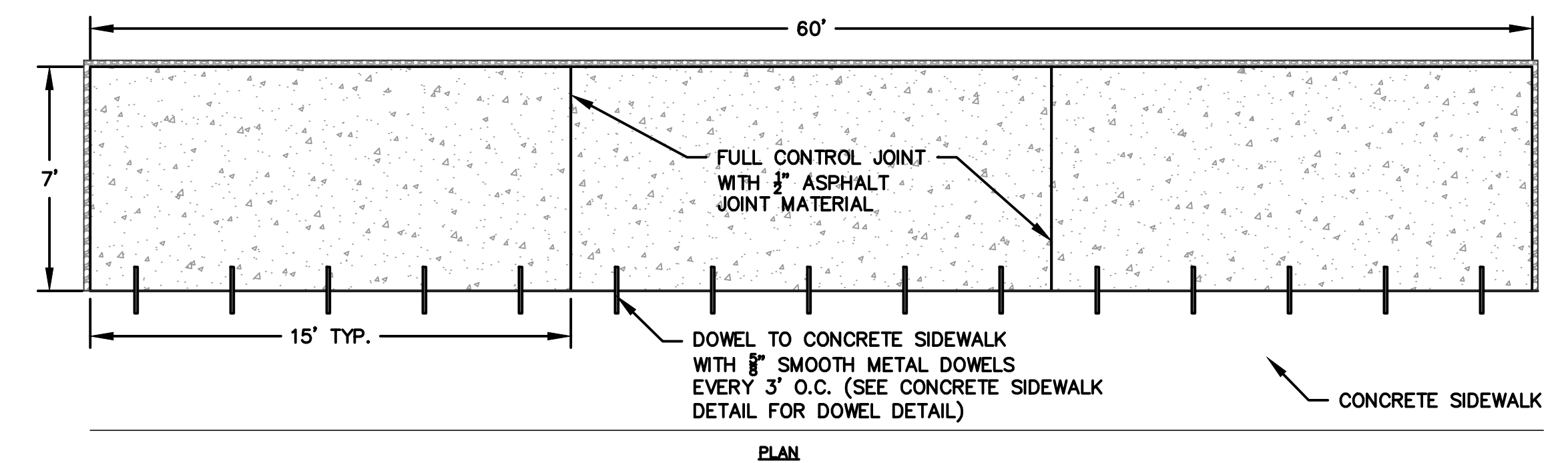
BITUMINOUS CONCRETE DRIVEWAY
NOT TO SCALE



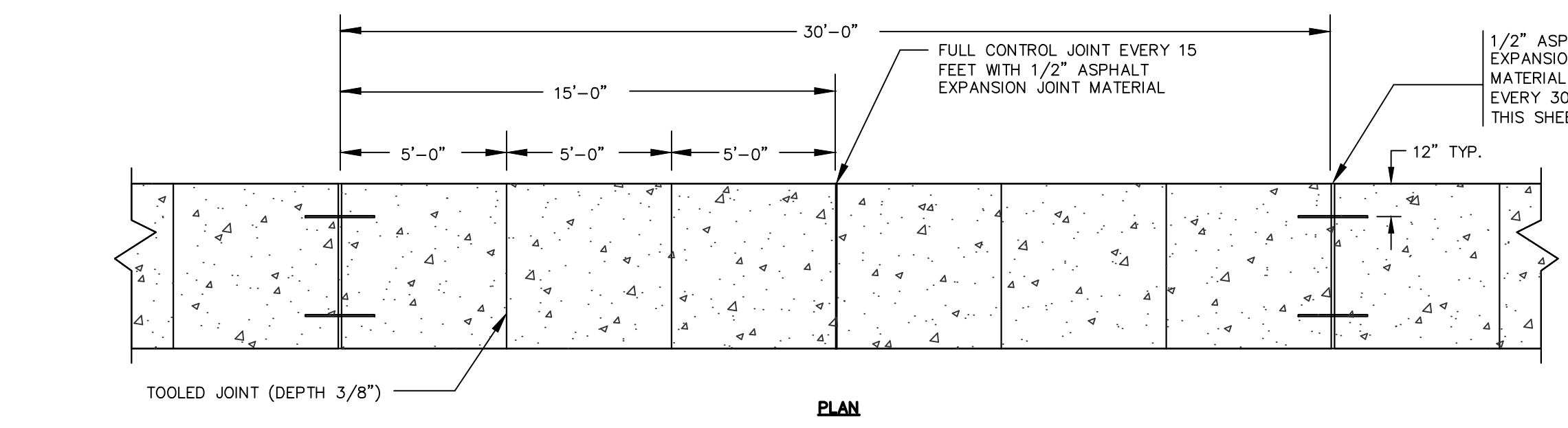
BITUMINOUS CONCRETE SIDEWALK
NOT TO SCALE



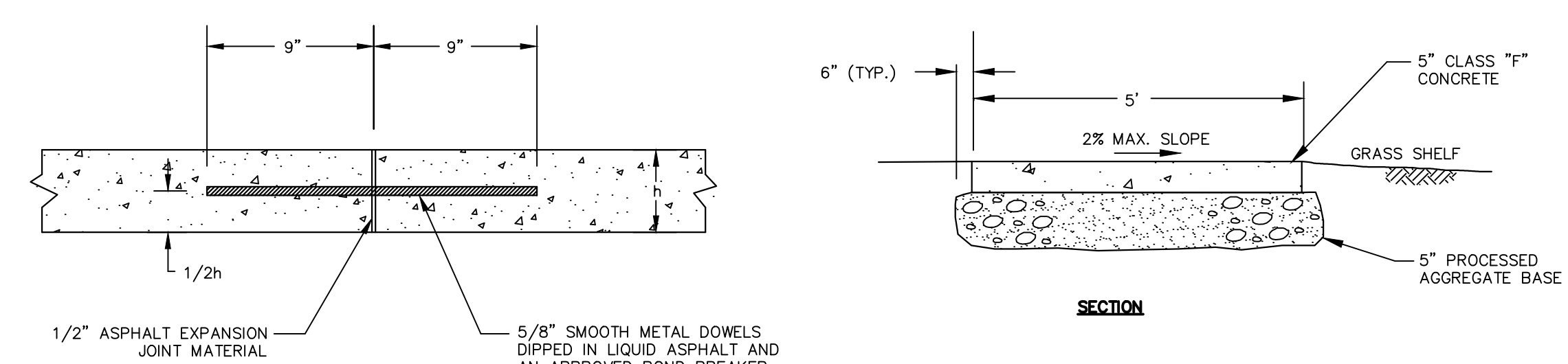
TIMBER RAIL
NOT TO SCALE



CONCRETE BLEACHER PAD
NOT TO SCALE



5\"/>



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TOWN OF MANCHESTER
PUBLIC WORKS DEPARTMENT
ENGINEERING DIVISION
494 MAIN STREET - P.O. BOX 191
MANCHESTER, CT 06045-0191

LEGEND

—	WETLAND BOUNDARY	○	LIGHT POLE
—	RETAINING WALL	⊗	CONIFEROUS TREE
—	STONE WALL	⊗	DECIDUOUS TREE
—	STOCKADE FENCE	⊗	SANITARY MANHOLE
—	WIRE FENCE	⊗	DRAINAGE MANHOLE
—	CHAIN LINK FENCE	⊗	CATCH BASIN
—	RAILROAD TRACKS	⊗	CULVERT END
—	SILT FENCE	⊗	HYDRANT
—	CONCRETE MONUMENT	⊗	CURB STOP
—	GRANITE MONUMENT	⊗	WATER VALVE
—	IRON PIPE	⊗	BUTTERFLY VALVE
—	IRON ROD	⊗	BLOW OFF
—	DRILL HOLE	⊗	BLIND
—	UTILITY POLE	⊗	DOUBLE POST SIGN
—	UTILITY POLE WITH LIGHT	⊗	MAIL BOX
—	ELECTRIC BOX	⊗	ROLLBAR
—	WETLAND FLAG	⊗	CONTROL CABINET
—		⊗	GAS GATE
—		⊗	TELEPHONE BOX
—		⊗	CATV TUBE

PROJECT NUMBER
2021078

FILENAME
2021078-PLAN-PZC.DWG

NO.	DATE	FILE
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DRAWN BY:
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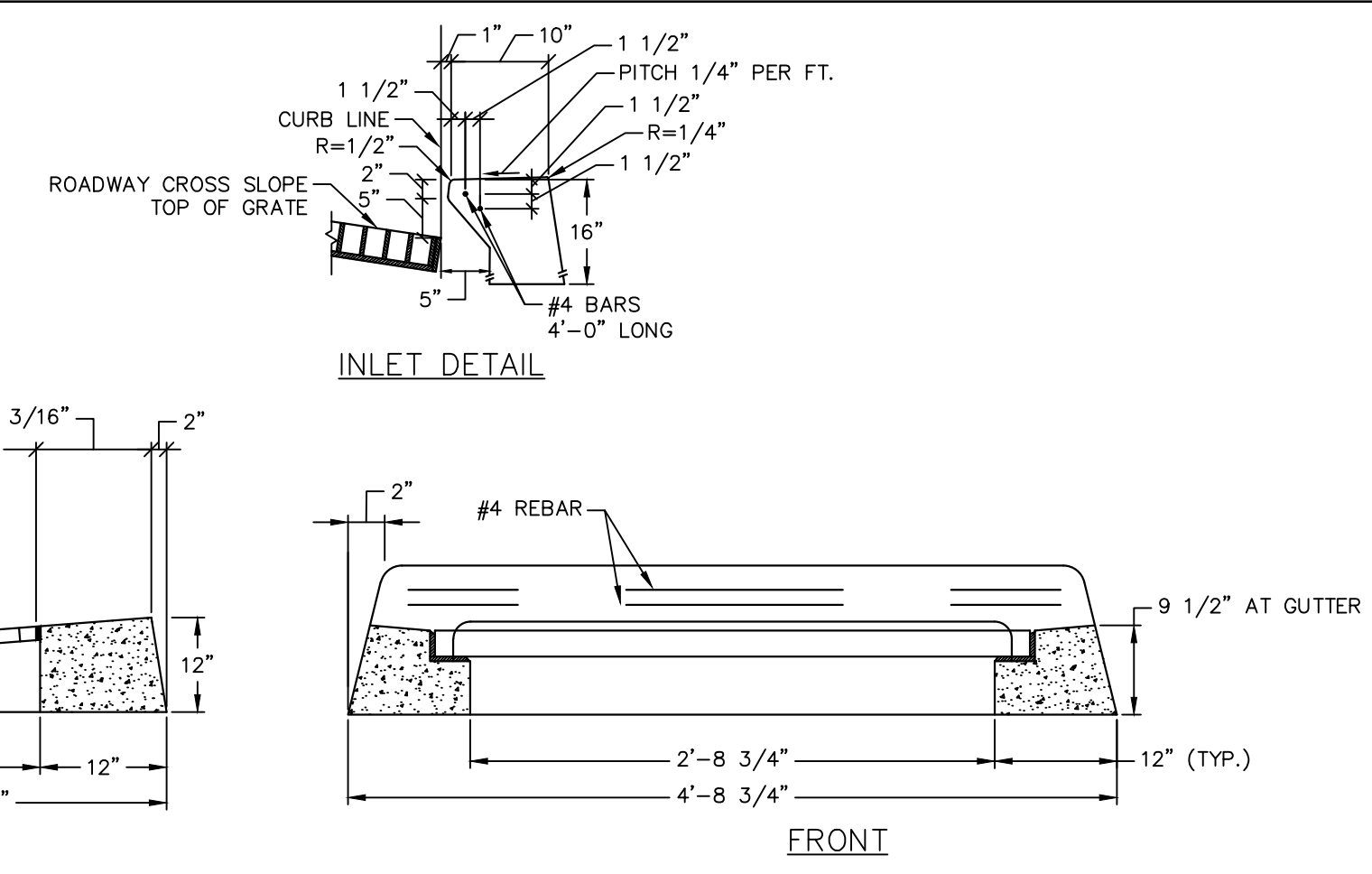
DATUM
HORIZONTAL: NAD83 VERTICAL: NAVD88

PROJECT LOCATION
**CHARTER OAK STREET
MANCHESTER, CT**

PROJECT TITLE
**PARK IMPROVEMENTS
CHARTER OAK WEST**

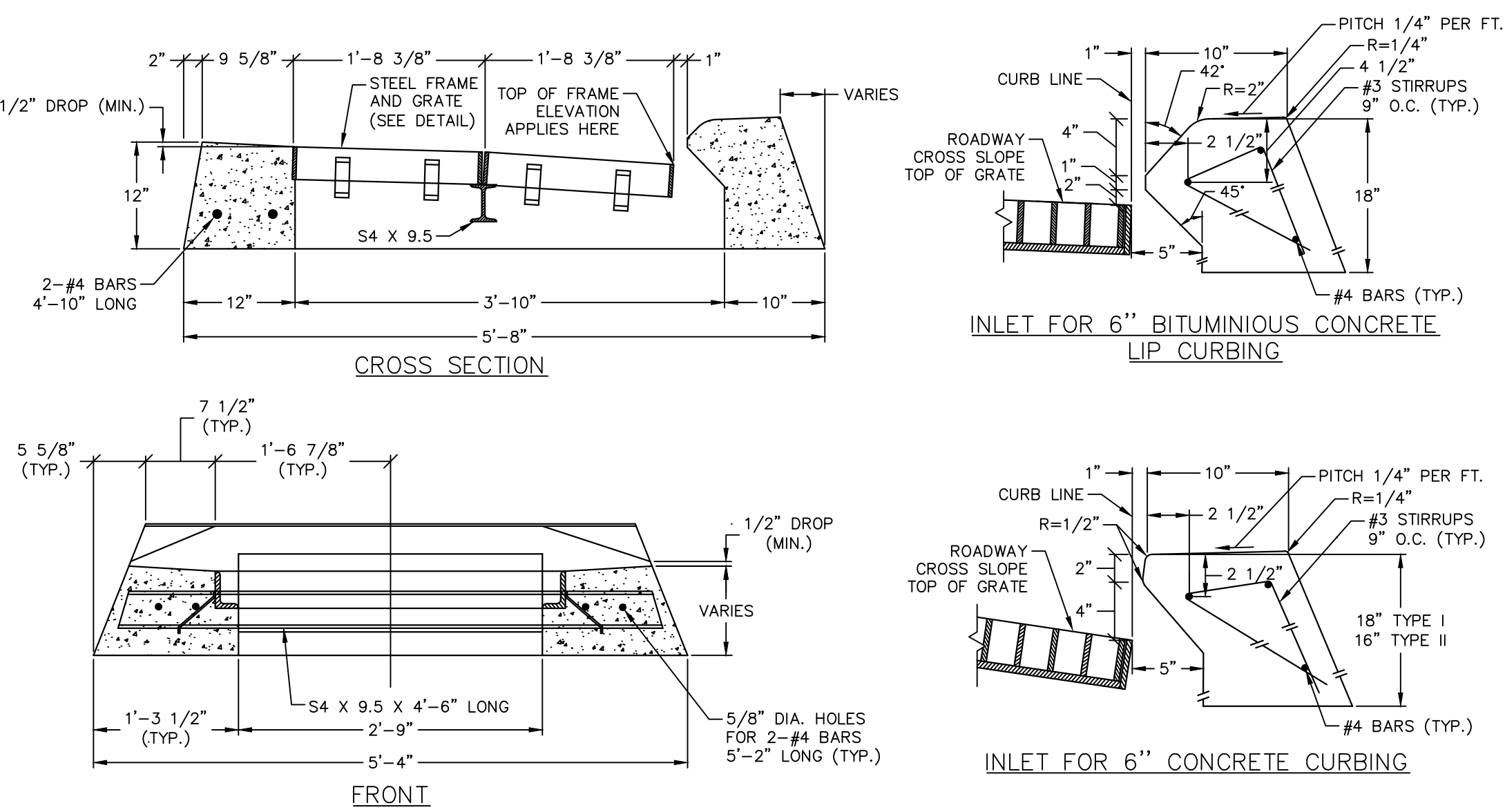
SHEET TITLE
DETAILS

SHEET NUMBER
10 of 16



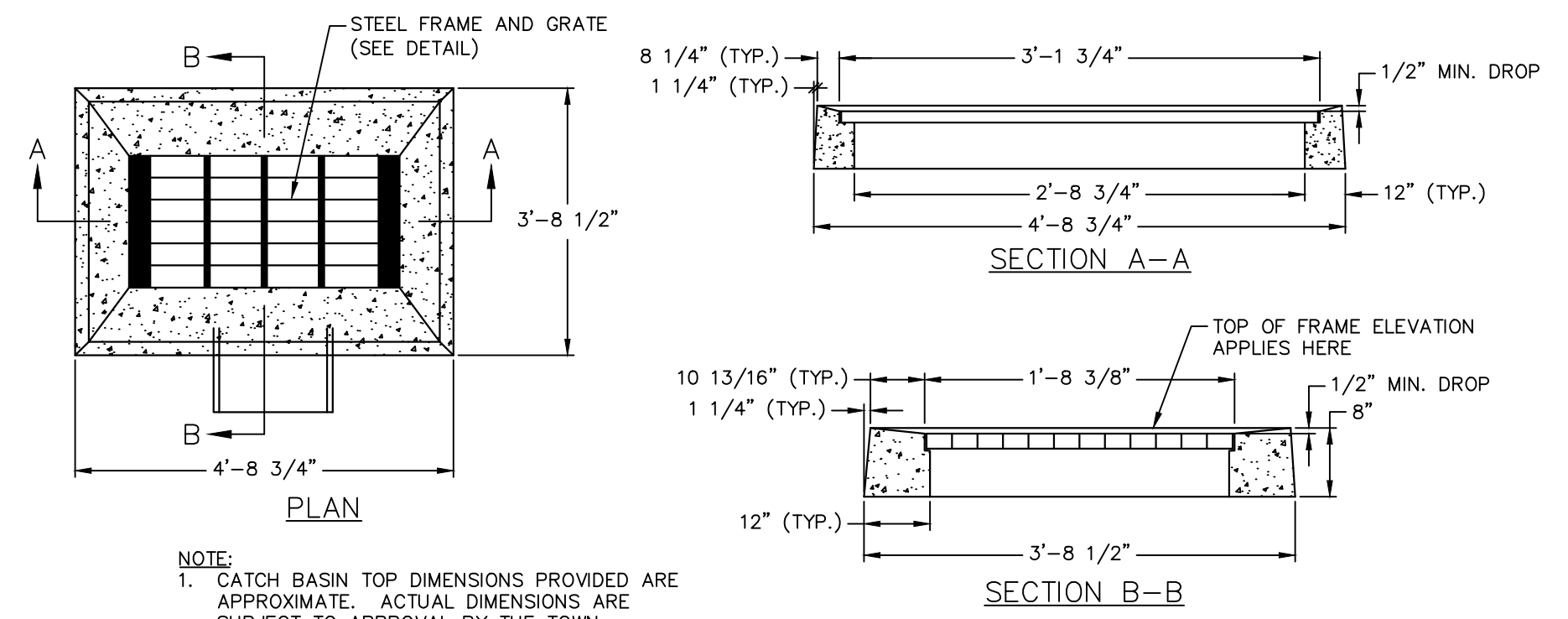
NOTES:
1. CONCRETE TOP SHALL BE CAST TO MATCH ADJACENT CONCRETE CURB.
2. CATCH BASIN TOP DIMENSIONS PROVIDED ARE APPROXIMATE. ACTUAL DIMENSIONS ARE SUBJECT TO APPROVAL BY THE TOWN.

**TYPE "C" CATCH BASIN TOP
(FOR CONCRETE CURB)**
NOT TO SCALE



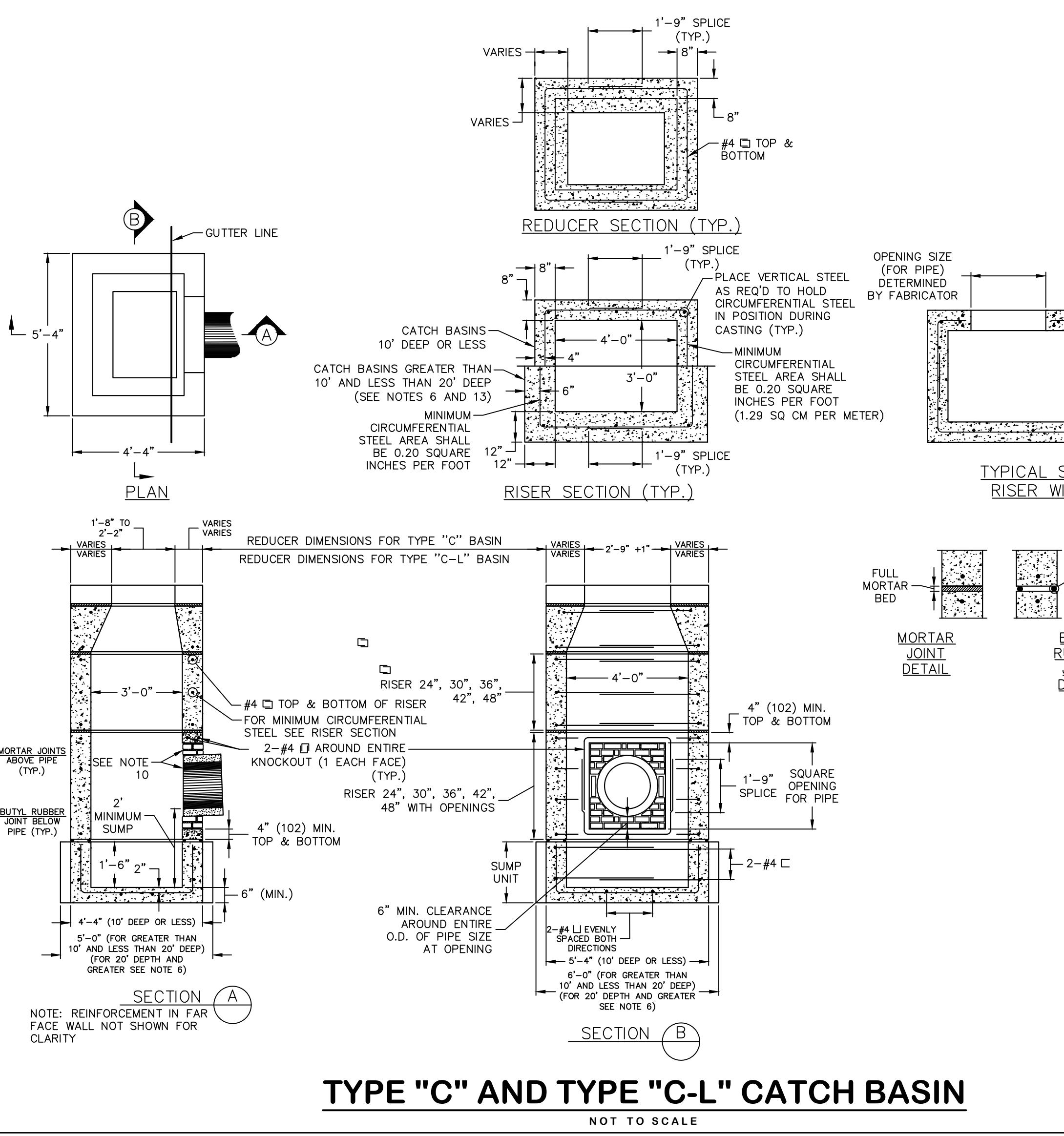
NOTE:
1. CATCH BASIN TOP DIMENSIONS PROVIDED ARE APPROXIMATE. ACTUAL DIMENSIONS ARE SUBJECT TO APPROVAL BY THE TOWN.

**TYPE "C" DOUBLE GRATE TYPE I CATCH BASIN TOP
(FOR CONCRETE CURB AND BITUMINOUS CONCRETE LIP CURB)**
NOT TO SCALE

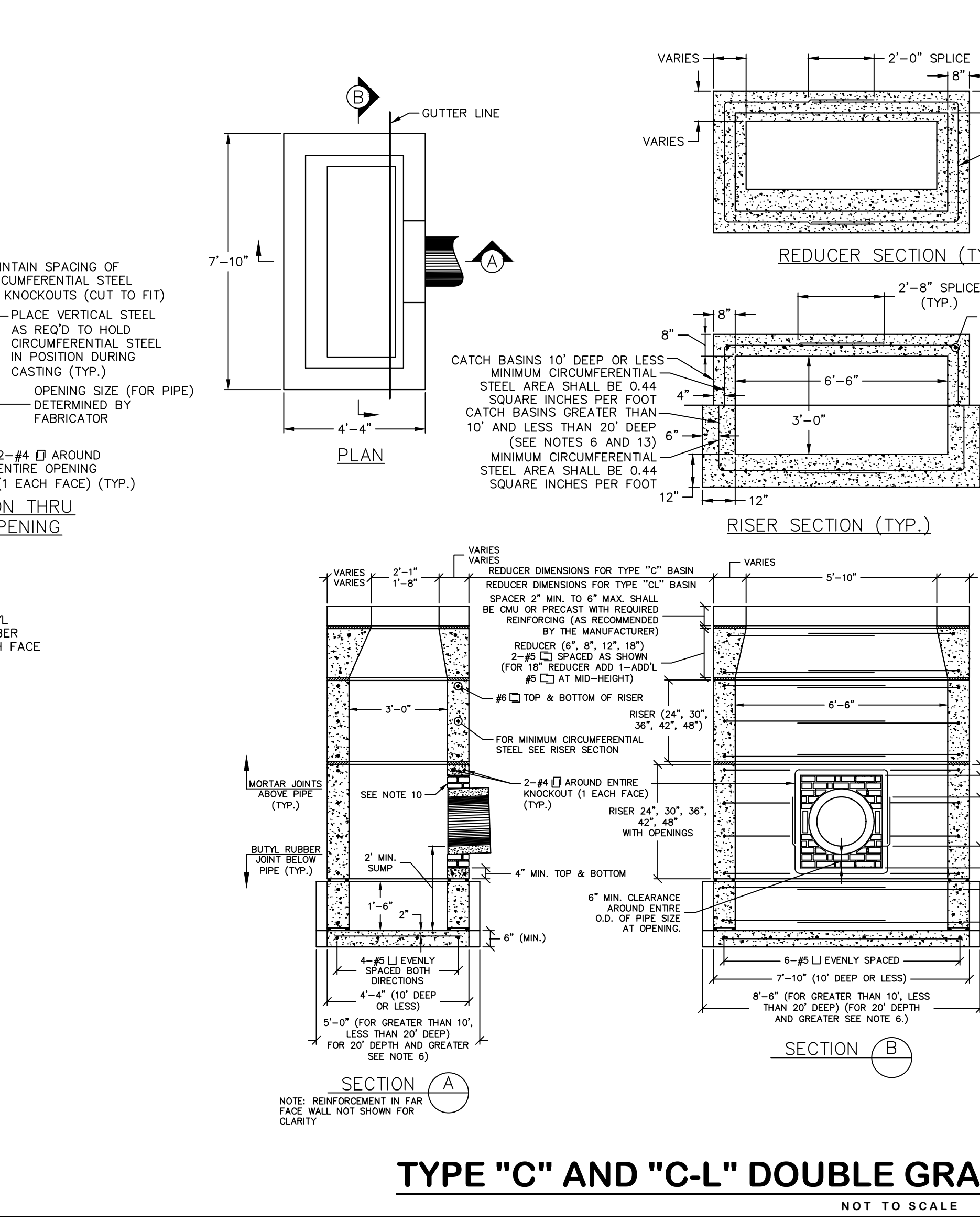


NOTE:
1. CATCH BASIN TOP DIMENSIONS PROVIDED ARE APPROXIMATE. ACTUAL DIMENSIONS ARE SUBJECT TO APPROVAL BY THE TOWN.

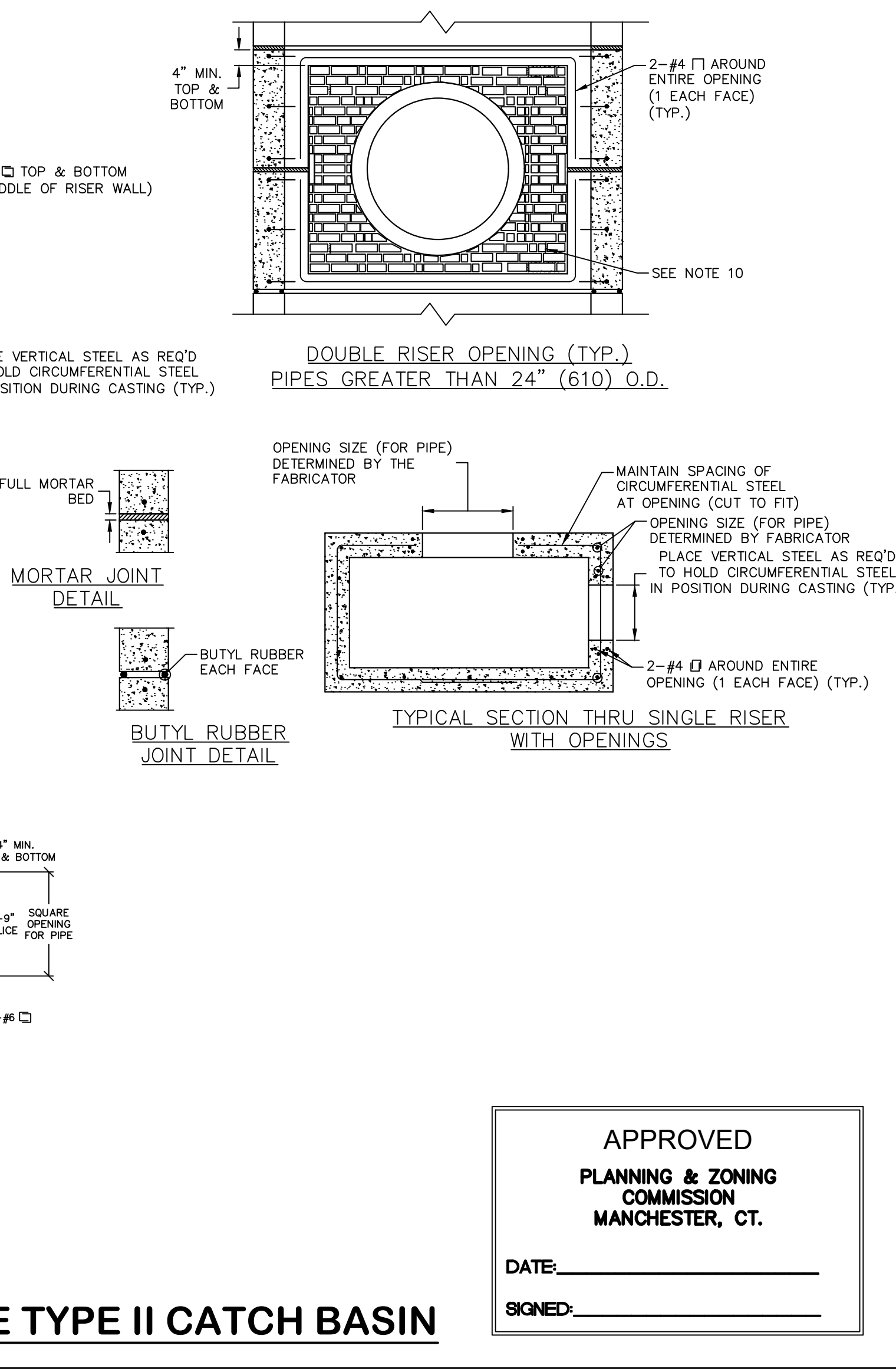
TYPE "C-L" CATCH BASIN TOP
NOT TO SCALE



TYPE "C" AND TYPE "C-L" CATCH BASIN
NOT TO SCALE

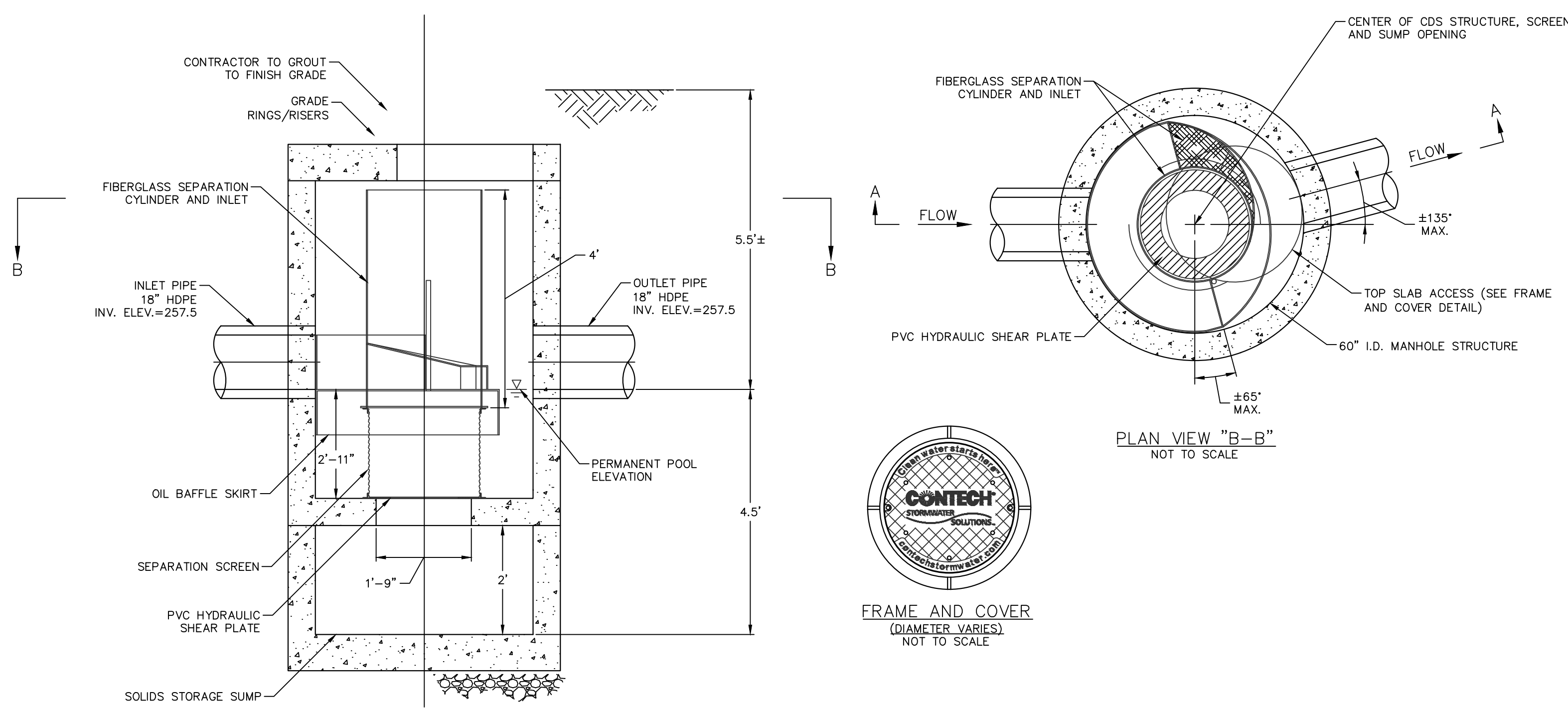


TYPE "C" AND "C-L" DOUBLE GRATE TYPE II CATCH BASIN
NOT TO SCALE

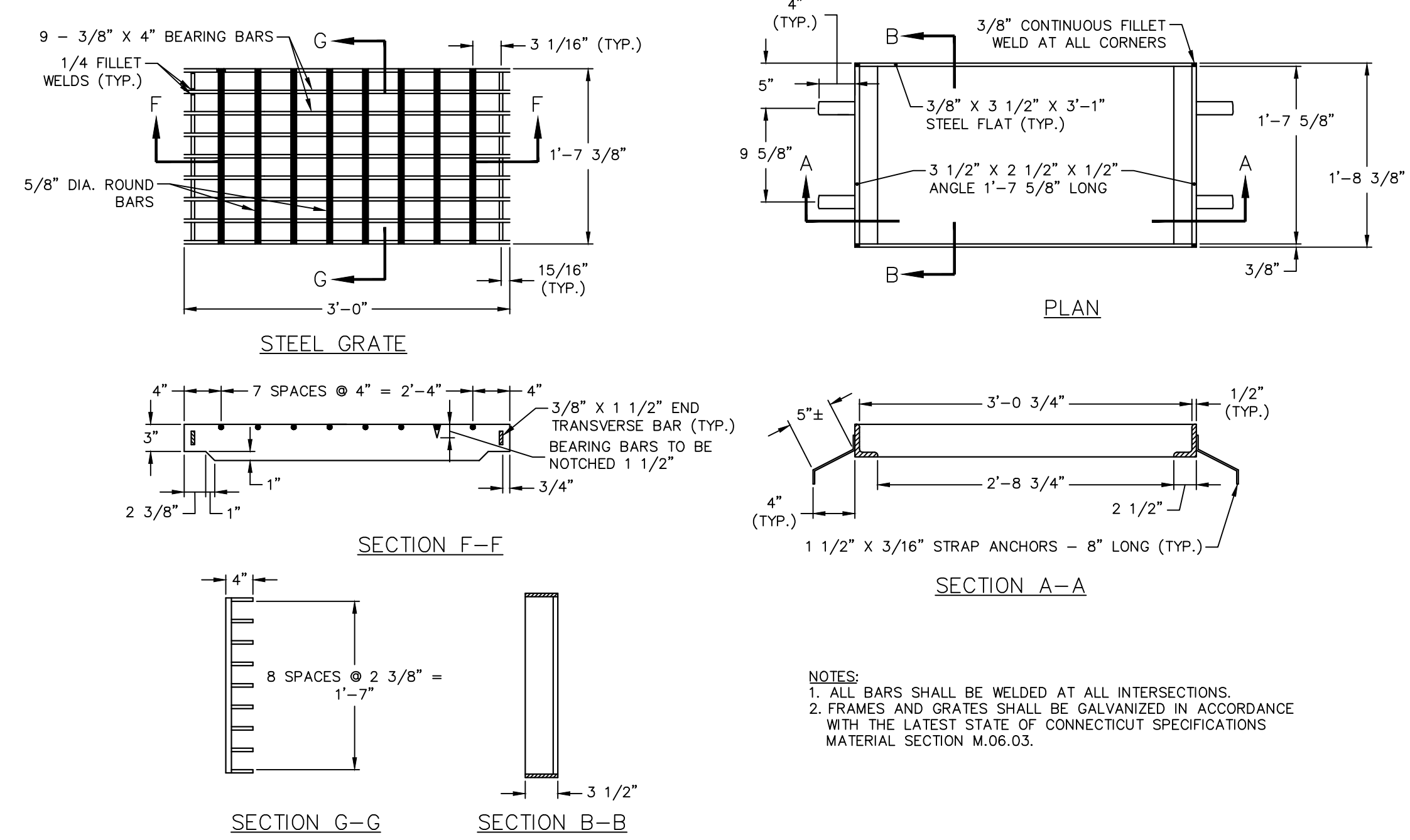


**DOUBLE RISER OPENING (TYP.)
PIPES GREATER THAN 24" (610) O.D.**

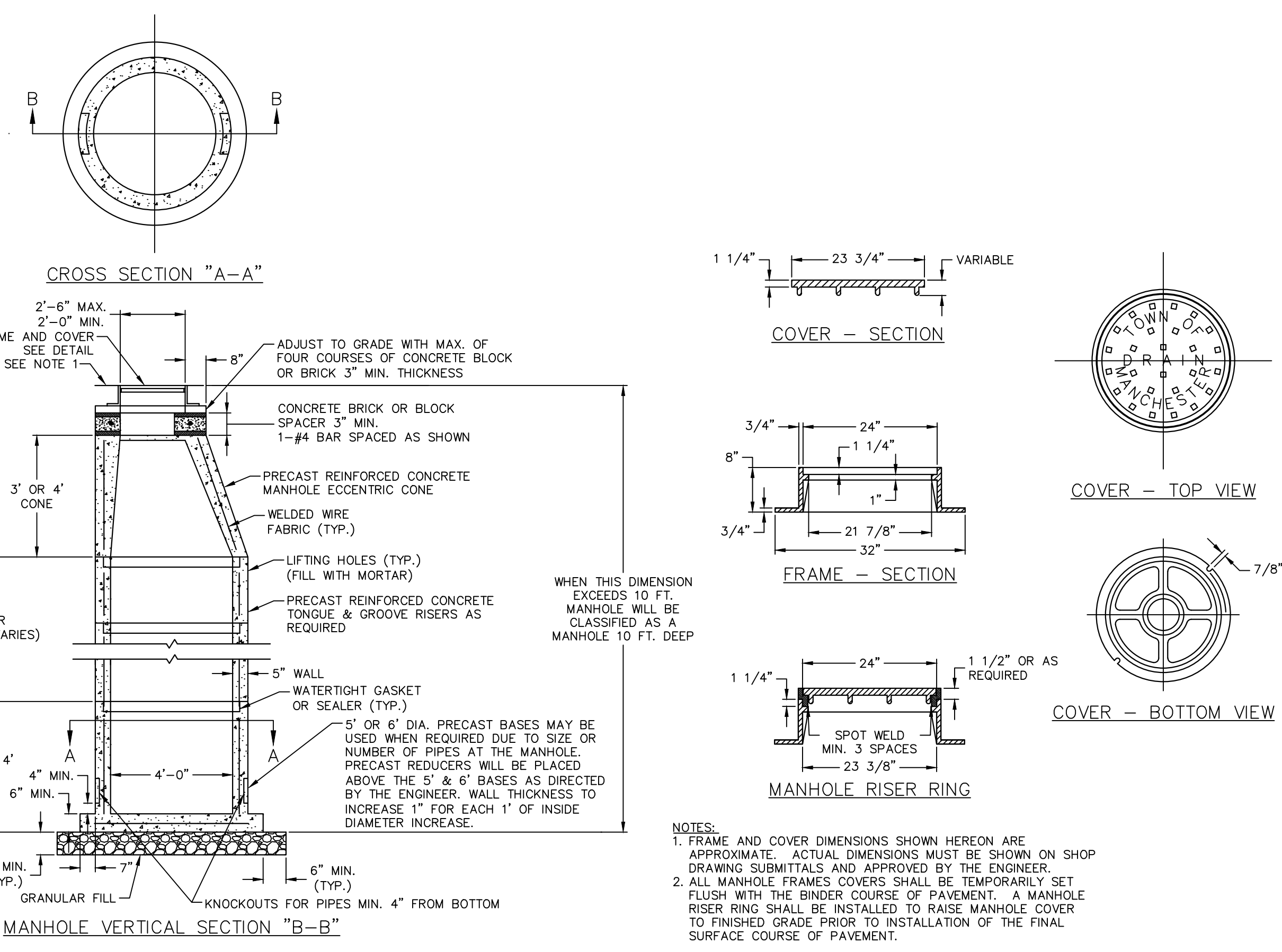
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PLANNING & ZONING
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MANCHESTER, CT.
DATE: _____
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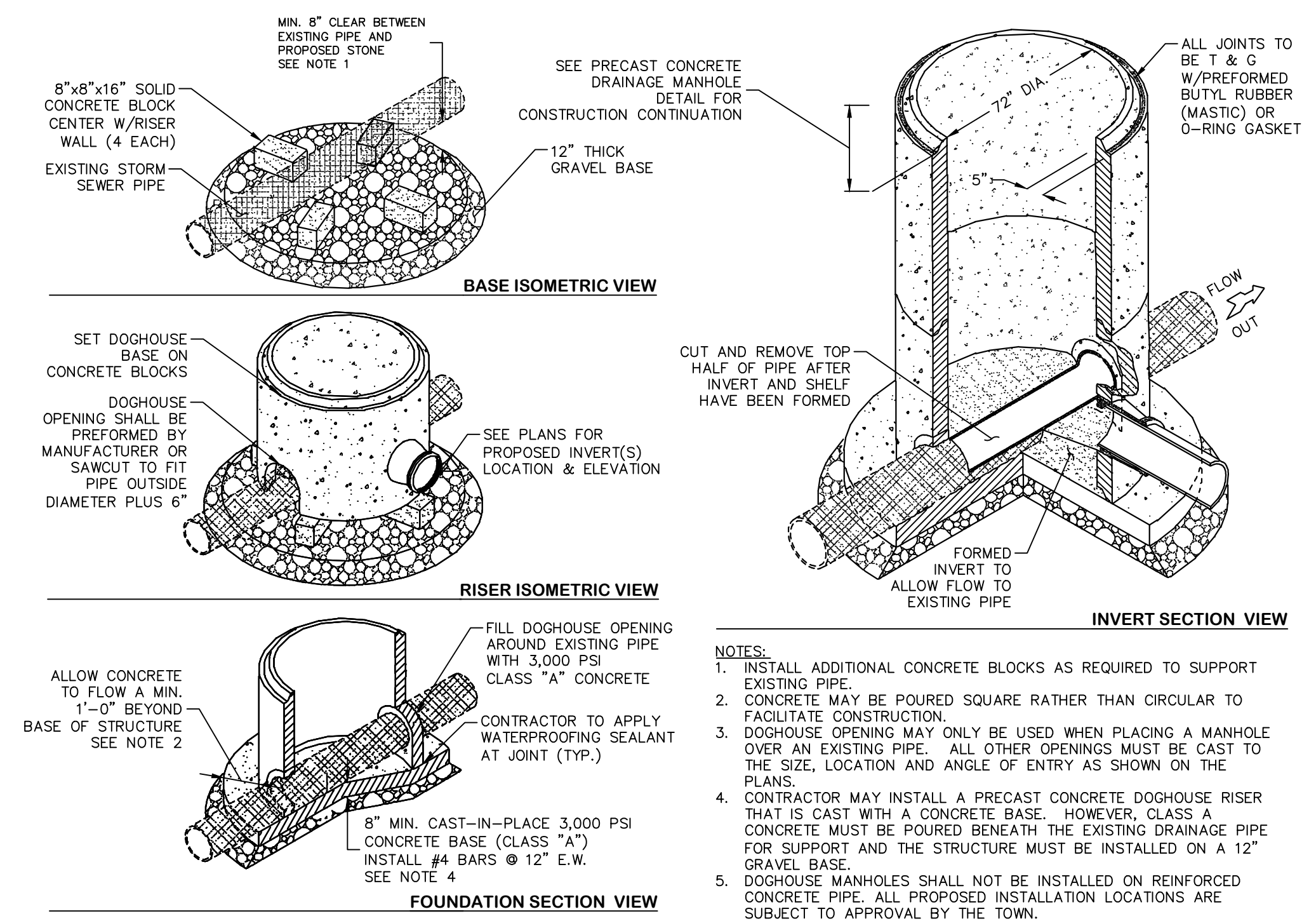
**CDS STORMWATER TREATMENT SYSTEM
MODEL CDS 2025-5-C
(PAY ITEM: "HYDRODYNAMIC SEPARATOR")**
NOT TO SCALE



STEEL FRAME AND GRATE
NOT TO SCALE



STORM MANHOLE
NOT TO SCALE



DRAINAGE (STORM) DOGHOUSE MANHOLE
NOT TO SCALE

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TOWN OF MANCHESTER
PUBLIC WORKS DEPARTMENT
ENGINEERING DIVISION
494 MAIN STREET - P.O. BOX 191
MANCHESTER, CT 06045-0191

LEGEND

—	WETLAND BOUNDARY	☆	LIGHT POLE
—	RETAINING WALL	⊗	CONIFEROUS TREE
—	STONE WALL	⊗	DECIDUOUS TREE
—	STOCKADE FENCE	⊗	SANITARY MANHOLE
—	WIRE FENCE	⊗	DRAINAGE MANHOLE
—	CHAIN LINK FENCE	⊗	CATCH BASIN
—	PROPERTY LINE	⊗	CULVERT END
—	RAILROAD TRACKS	⊗	HYDRANT
—	SILT FENCE	⊗	CURB STOP
—	CONCRETE MONUMENT	⊗	WATER VALVE
—	GRANITE MONUMENT	⊗	BUTTERFLY VALVE
—	IRON PIPE	⊗	BLOW OFF
—	IRON ROD	⊗	SIEN
—	CONTROL POINT	⊗	DOUBLE POST SIGN
—	DRILL HOLE	⊗	MAIL BOX
—	UTILITY POLE	⊗	ROLLARD
—	UTILITY POLE WITH LIGHT	⊗	CONTROLLER CABINET
—	TRAFFIC SPAN POLE	⊗	GAS GATE
—	ELECTRIC BOX	⊗	TELEPHONE BOX
—	WETLAND FLAG	⊗	CATV TUBE

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PROJECT LOCATION
**CHARTER OAK STREET
MANCHESTER, CT**

PROJECT TITLE
**PARK IMPROVEMENTS
CHARTER OAK WEST**

SHEET TITLE
DETAILS

SHEET NUMBER
11 of 16



TOWN OF MANCHESTER
PUBLIC WORKS DEPARTMENT
ENGINEERING DIVISION
494 MAIN STREET - P.O. BOX 191
MANCHESTER, CT 06045-0191

LEGEND

— WETLAND BOUNDARY	☆ LIGHT POLE
— RETAINING WALL	⊗ CONIFEROUS TREE
— STONE WALL	⊗ DEODOROUS TREE
— STOCKADE FENCE	⊗ SANITARY MANHOLE
— WIRE FENCE	⊗ DRAINAGE MANHOLE
— CHAIN LINK FENCE	⊗ CATCH BASIN
— PROPERTY LINE	⊗ CULVERT END
— RAILROAD TRACKS	⊗ HYDRANT
— SILT FENCE	⊗ CURB STOP
□ CONCRETE MONUMENT	⊗ WATER VALVE
■ GRANITE MONUMENT	⊗ BUTTERFLY VALVE
○ IRON PIPE	⊗ BLOW OFF
● IRON ROD	⊗ SIGN
△ CONTROL POINT	⊗ DOUBLE POST SIGN
● DRILL HOLE	⊗ MAIL BOX
⊗ UTILITY POLE	⊗ BOLLARD
⊗ UTILITY POLE WITH LIGHT	⊗ CONTROLLER CABINET
⊗ TRAFFIC SPAN POLE	⊗ GAS GATE
⊗ ELECTRIC BOX	⊗ TELEPHONE BOX
⊗ WETLAND FLAG	⊗ CATV TUBE

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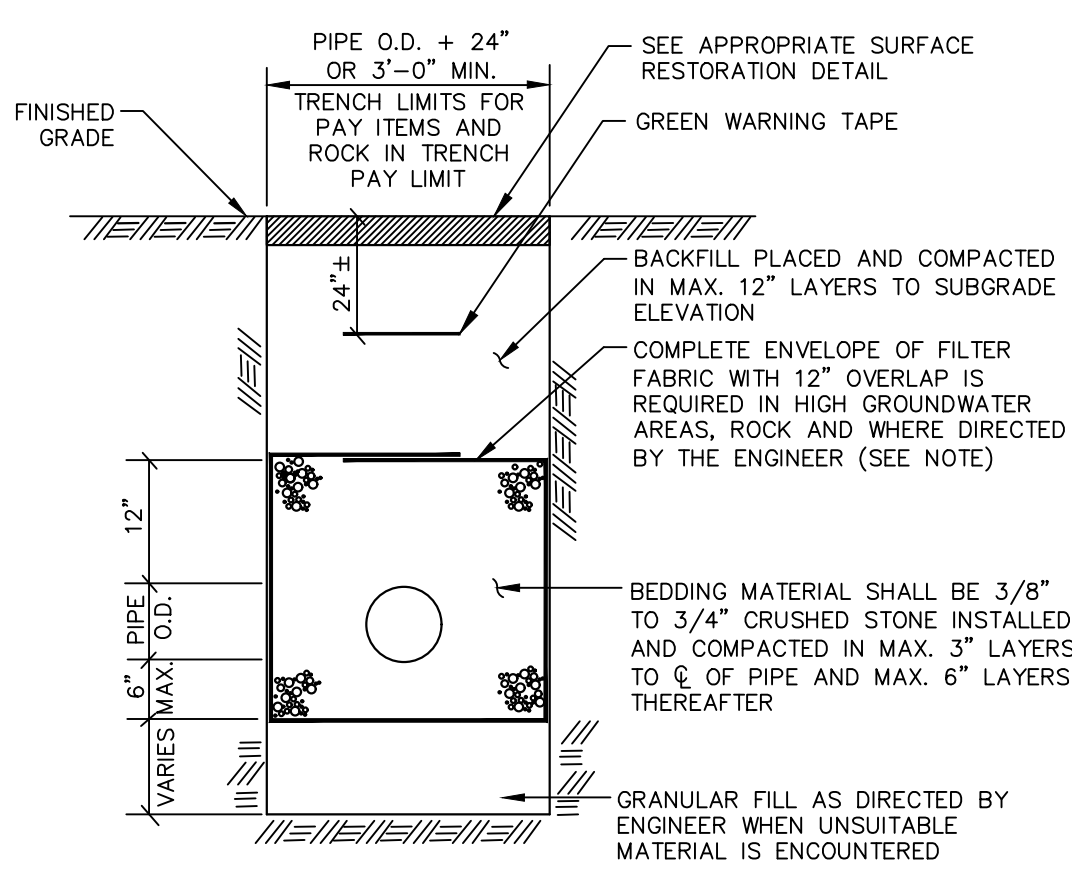
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PROJECT LOCATION
**CHARTER OAK STREET
MANCHESTER, CT**

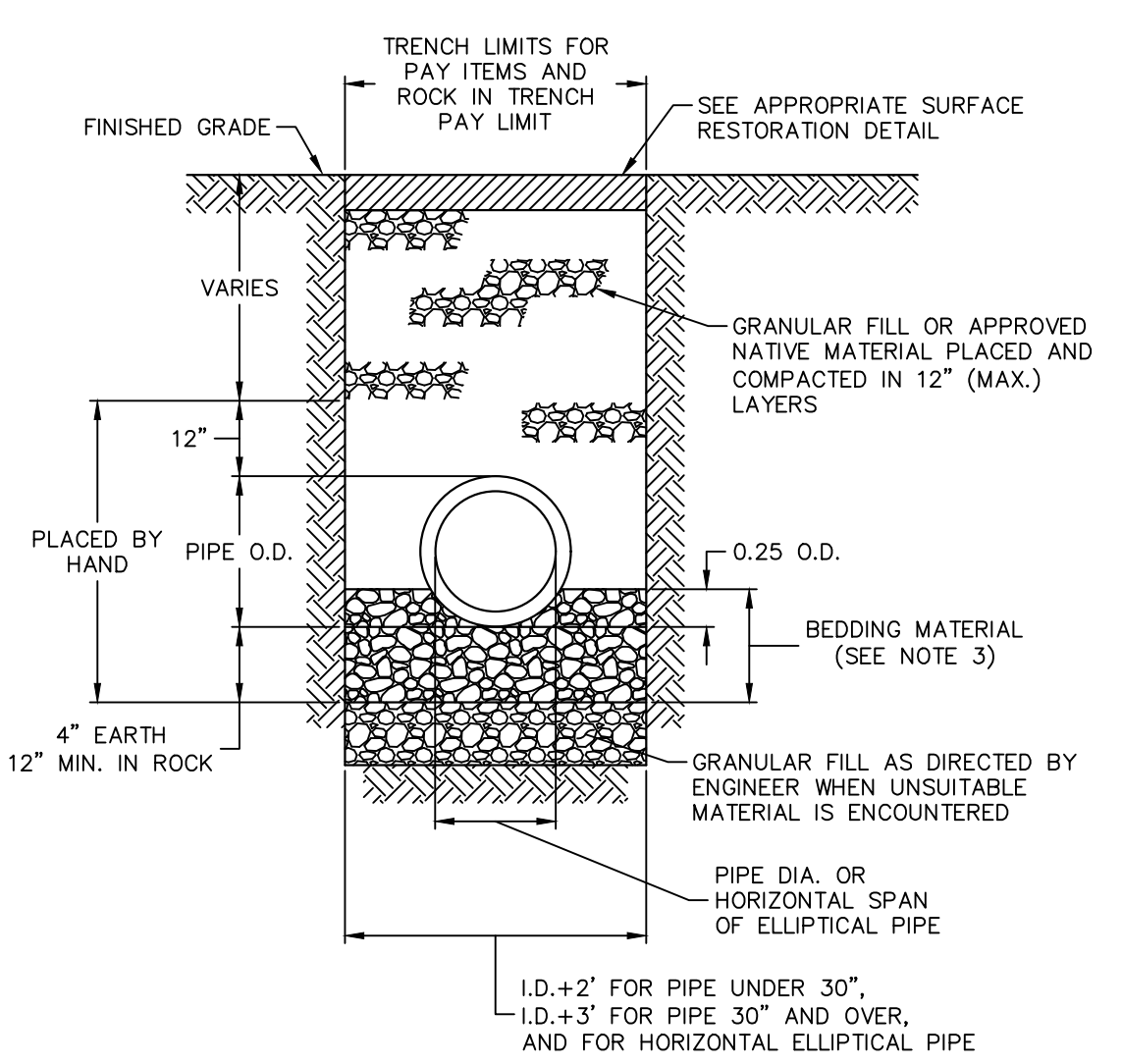
PROJECT TITLE
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SHEET TITLE
DETAILS

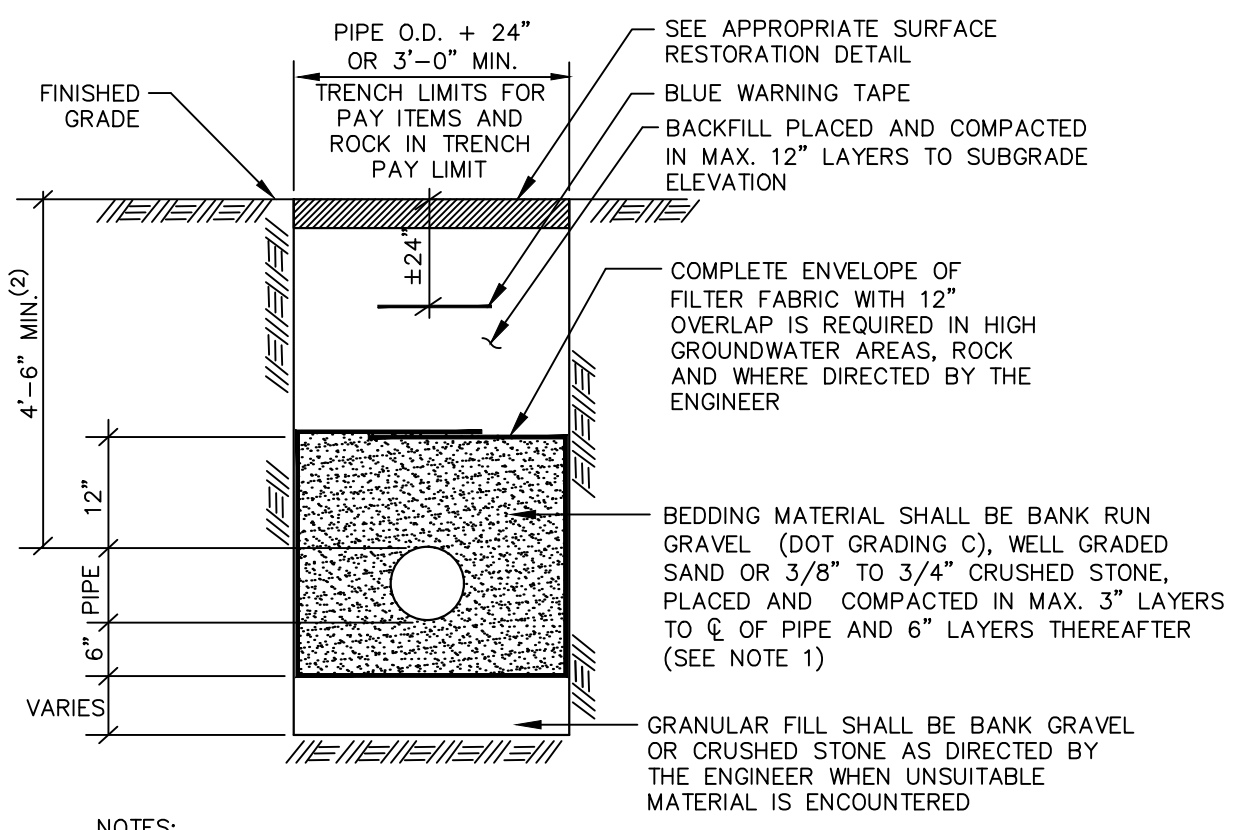
SHEET NUMBER
12 of 16



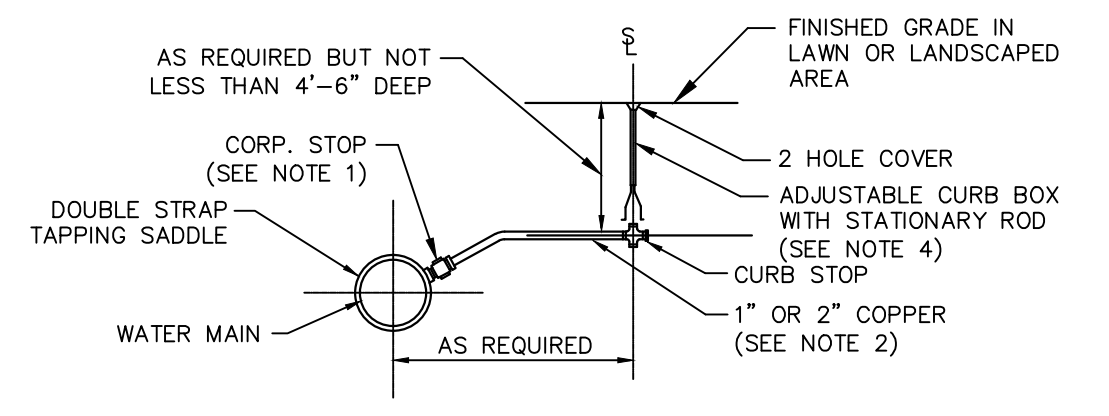
TYPICAL TRENCH DETAIL (SANITARY SEWER)
NOT TO SCALE



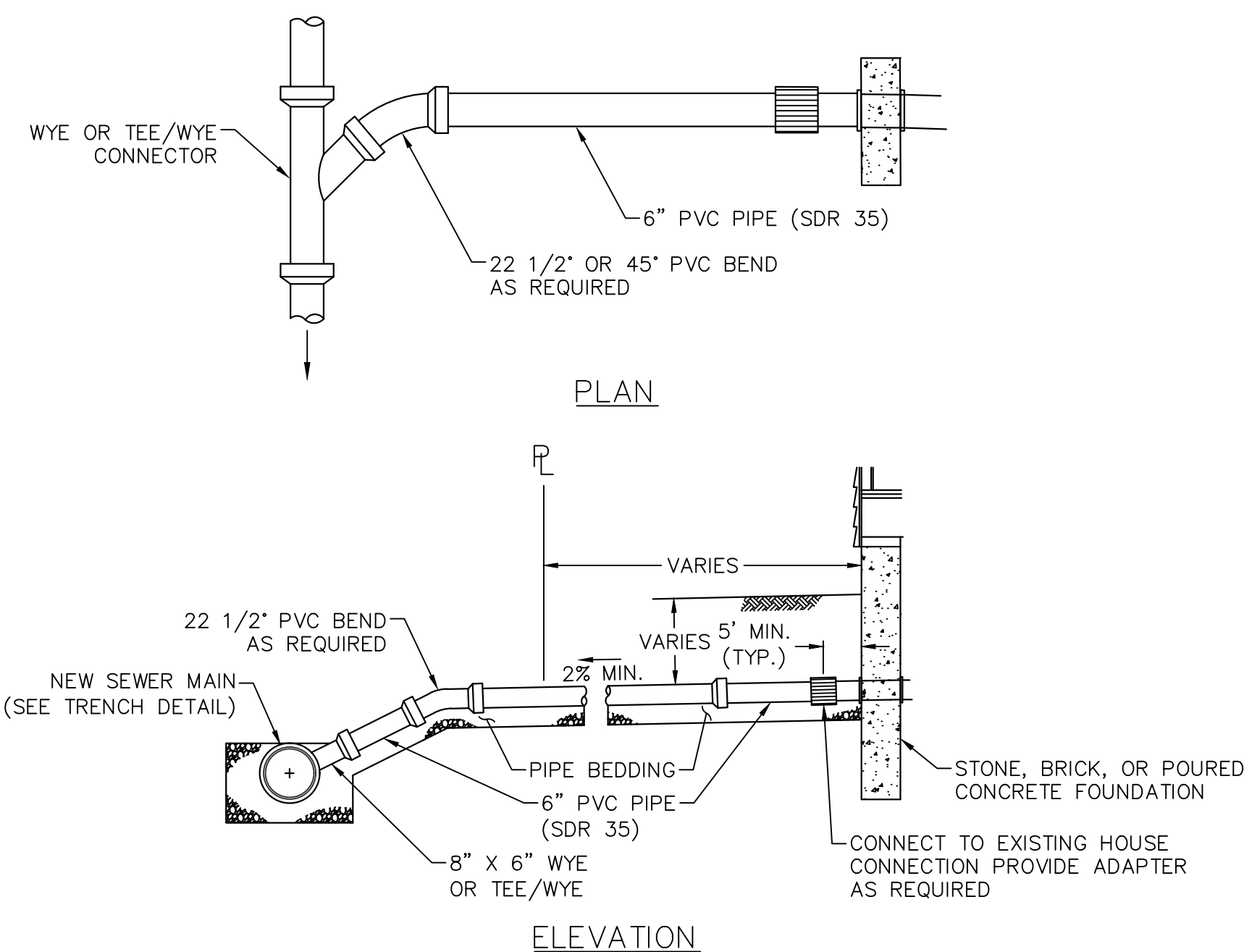
TYPICAL TRENCH DETAIL (STORM SEWER)
NOT TO SCALE



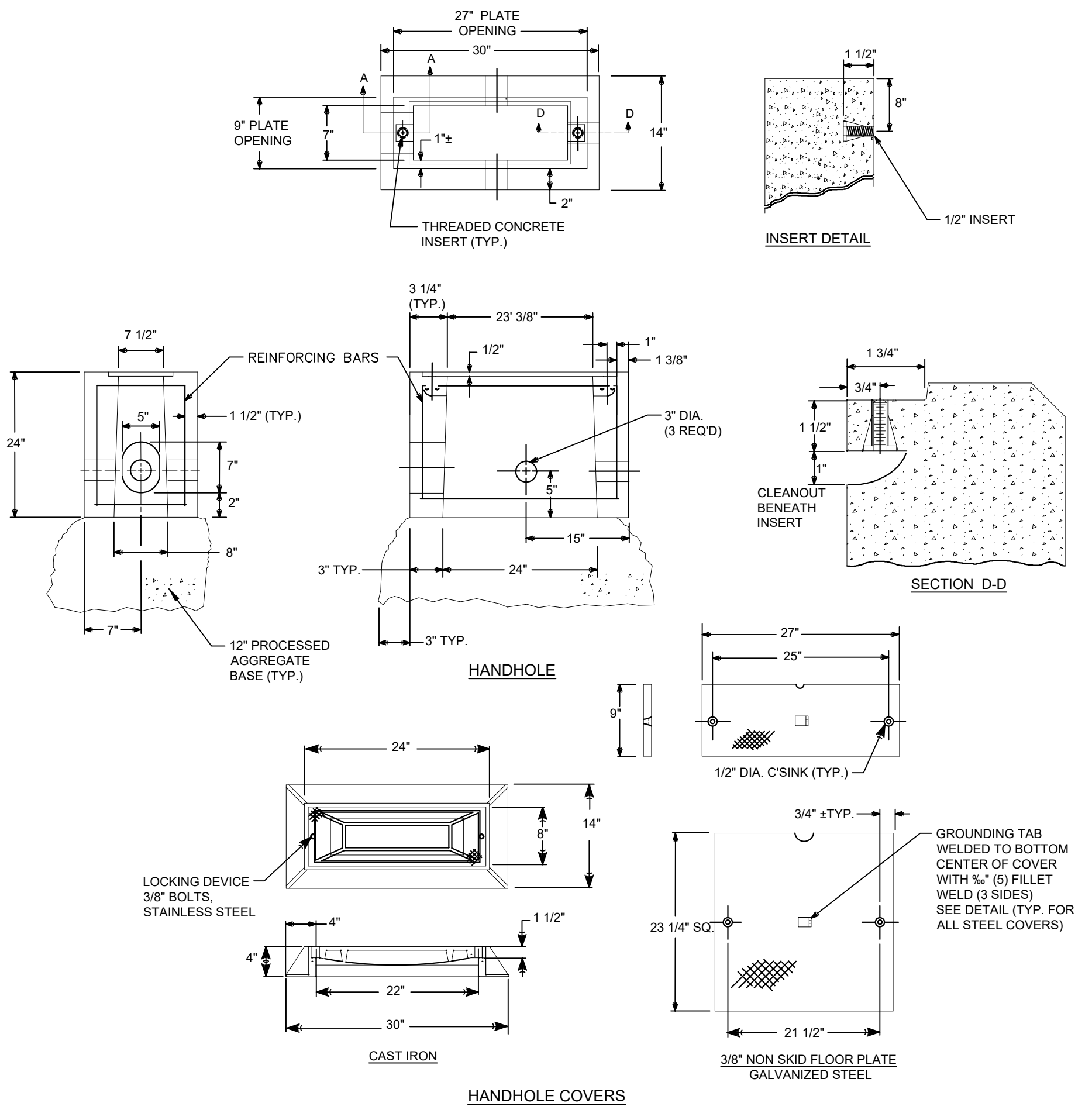
TYPICAL TRENCH DETAIL (WATER)
NOT TO SCALE



TYPICAL WATER SERVICE CONNECTION
NOT TO SCALE



6" SANITARY LATERAL CONNECTION (PAY ITEM: "6" PVC SANITARY SEWER LATERAL)
NOT TO SCALE

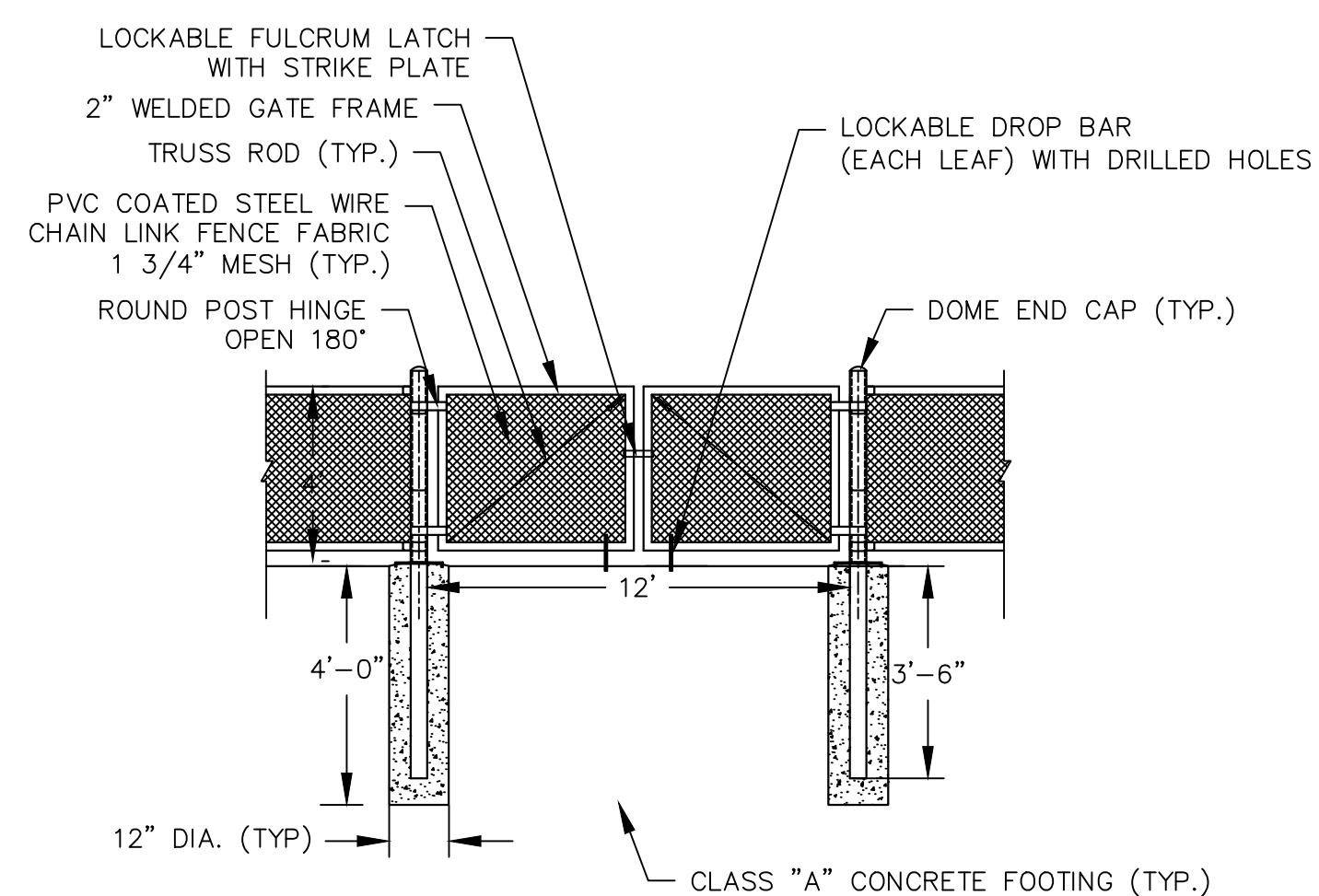


CONCRETE HANDHOLE
NOT TO SCALE

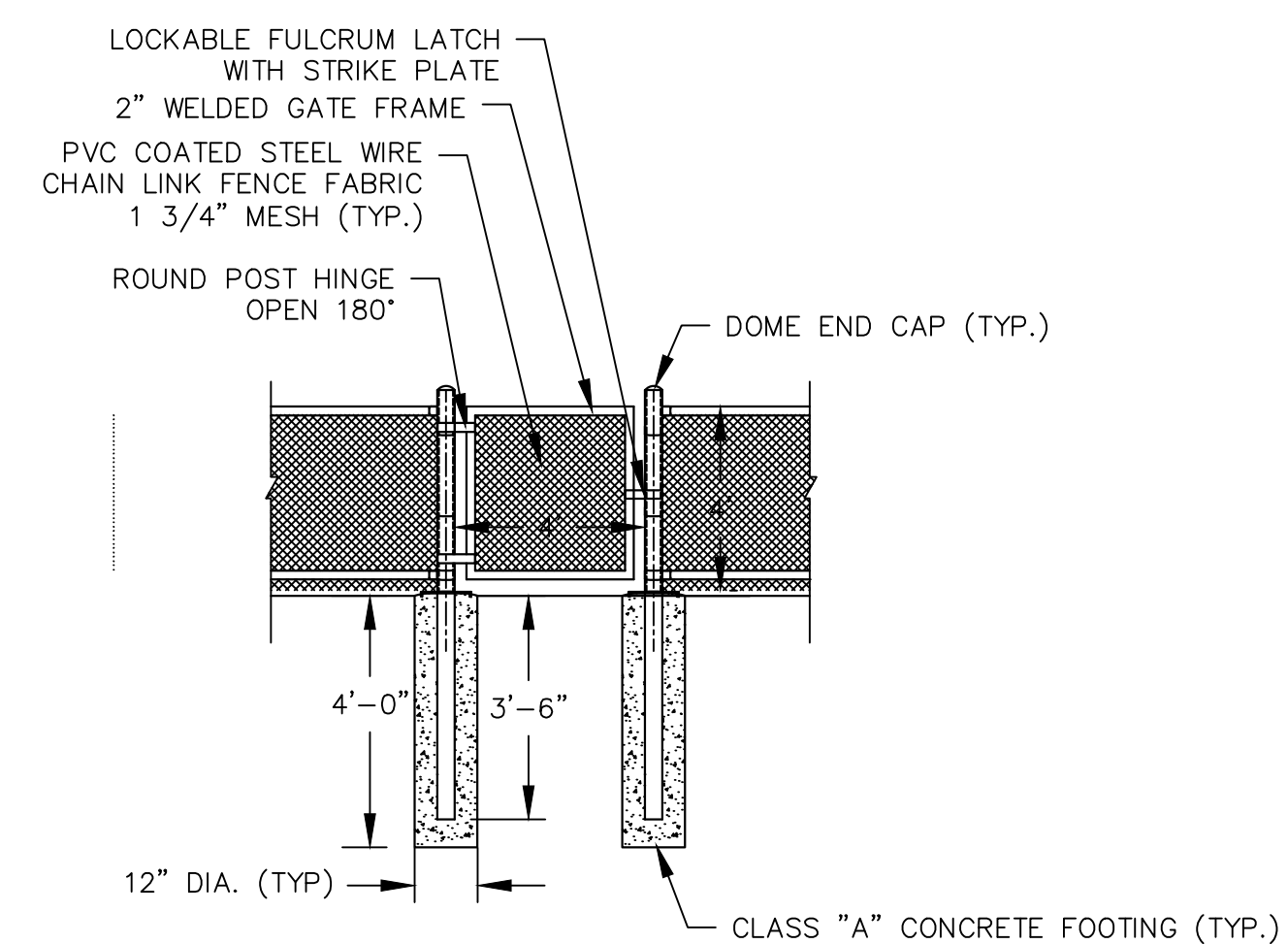
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TOWN OF MANCHESTER
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494 MAIN STREET - P.O. BOX 191
MANCHESTER, CT 06045-0191



4" HIGH CHAIN LINK DOUBLE GATE
NOT TO SCALE



4" HIGH CHAIN LINK SINGLE GATE
NOT TO SCALE

LEGEND

—	WETLANDS BOUNDARY	☆	LIGHT POLE
—	RETAINING WALL	⊗	CONIFEROUS TREE
—	CURB WALL	⊗	DECIDUOUS TREE
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—	STOCKADE FENCE	⊗	DRAINAGE MANHOLE
—	WIRE FENCE	⊗	CATCH BASIN
—	CHAIN LINK FENCE	⊗	CULVERT END
—	PROPERTY LINE	⊗	HYDRANT
—	RAILROAD TRACKS	⊗	CURB STOP
—	SILT FENCE	⊗	WATER VALVE
□	CONCRETE MONUMENT	⊗	BUTTERFLY VALVE
■	GRANITE MONUMENT	⊗	BLOW OFF
○	IRON PIPE	⊗	SIGN
●	IRON ROD	⊗	DOUBLE POST SIGN
△	CONTROL POINT	⊗	MAIL BOX
○	DRILL HOLE	⊗	ROLLARD
○	UTILITY POLE	⊗	CONTROLLER CABINET
○	UTILITY POLE WITH LIGHT	⊗	GAS GATE
⊗	TRAFFIC SPAN POLE	⊗	TELEPHONE BOX
⊗	ELECTRIC BOX	⊗	WETLAND FLAG
⊗	WETLAND FLAG	⊗	CATV TUBE

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PROJECT LOCATION
**CHARTER OAK STREET
MANCHESTER, CT**

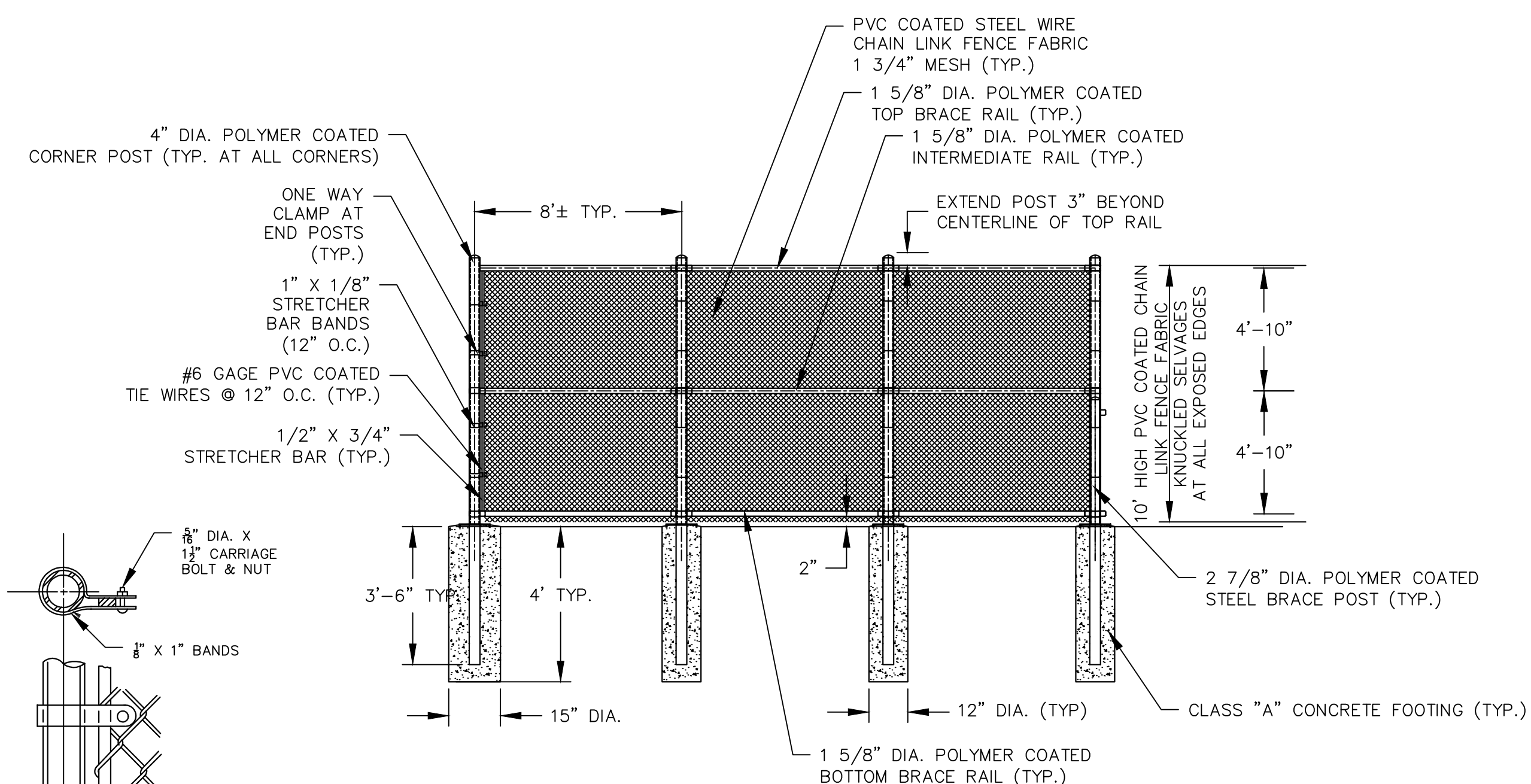
PROJECT TITLE
**PARK IMPROVEMENTS
CHARTER OAK WEST**

SHEET TITLE

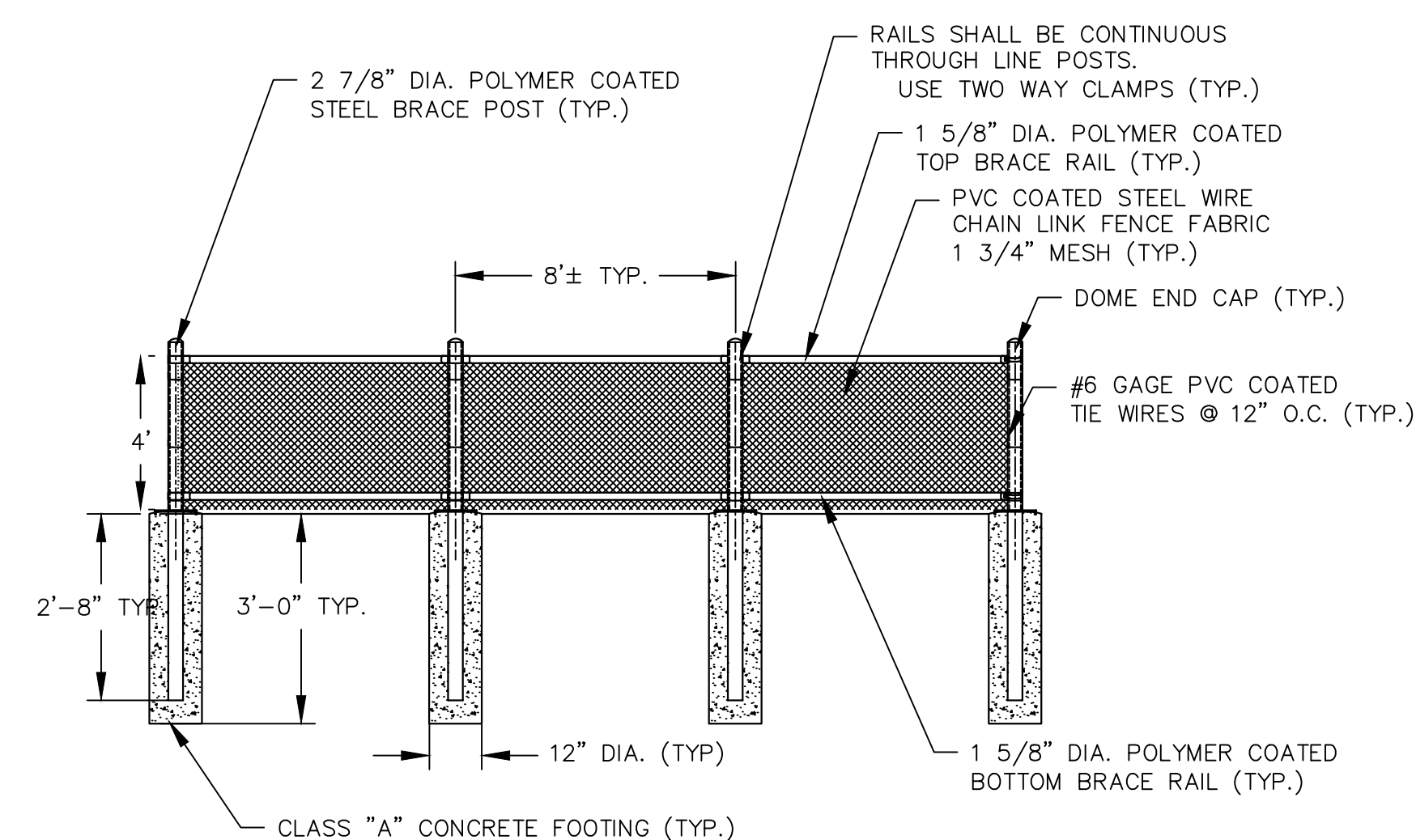
DETAILS

SHEET NUMBER

13 of 16



10' HIGH CHAIN LINK FENCE
NOT TO SCALE



4" HIGH CHAIN LINK FENCE
NOT TO SCALE

APPROVED
PLANNING & ZONING
COMMISSION
MANCHESTER, CT.
DATE: _____
SIGNED: _____

STRETCHER BAR ATTACHMENTS



TOWN OF MANCHESTER
PUBLIC WORKS DEPARTMENT
ENGINEERING DIVISION
494 MAIN STREET - P.O. BOX 191
MANCHESTER, CT 06045-0191

LEGEND

—	WETLANDS BOUNDARY	☆	LIGHT POLE
—	RETAINING WALL	⊗	CONIFEROUS TREE
—	STONE WALL	⊗	DECIDUOUS TREE
—	STOCKADE FENCE	⊗	SANITARY MANHOLE
—	WIRE FENCE	⊗	DRAINAGE MANHOLE
—	CHAIN LINK FENCE	⊗	CATCH BASIN
—	PROPERTY LINE	⊗	CULVERT END
—	RAILROAD TRACKS	⊗	HYDRANT
—	SILT FENCE	⊗	CURB STOP
—	CONCRETE MONUMENT	⊗	WATER VALVE
—	GRANITE MONUMENT	⊗	BUTTERFLY VALVE
—	IRON PIPE	⊗	BLOW OFF
—	IRON ROD	⊗	SIEN
—	CONTROL POINT	⊗	DOUBLE POST SIGN
—	DRILL HOLE	⊗	MAIL BOX
—	UTILITY POLE	⊗	BOLLARD
—	UTILITY POLE WITH LIGHT	⊗	CONTROLLER CABINET
—	TRAFFIC SPAN POLE	⊗	GAS GATE
—	ELECTRIC BOX	⊗	TELEPHONE BOX
—	WETLAND FLAG	⊗	CATV TUBE

PROJECT NUMBER
2021078

FILENAME
2021078-PLAN-PZC.DWG

NO.	DATE	FILE
—	03/15/24	PZC PERMIT SUBMISSION

DRAWN BY:
CHECKED BY: JL
RELEASED BY: TB

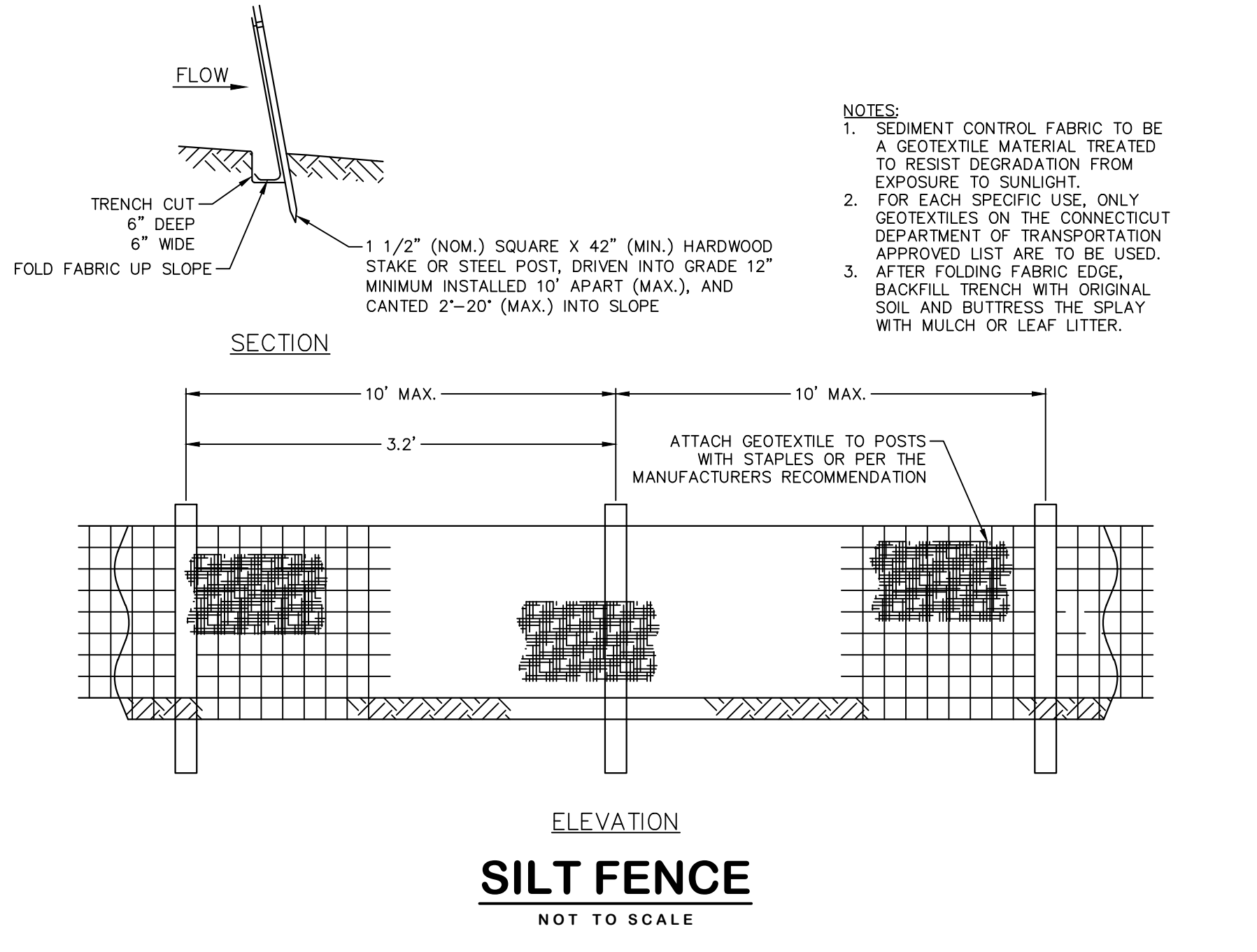
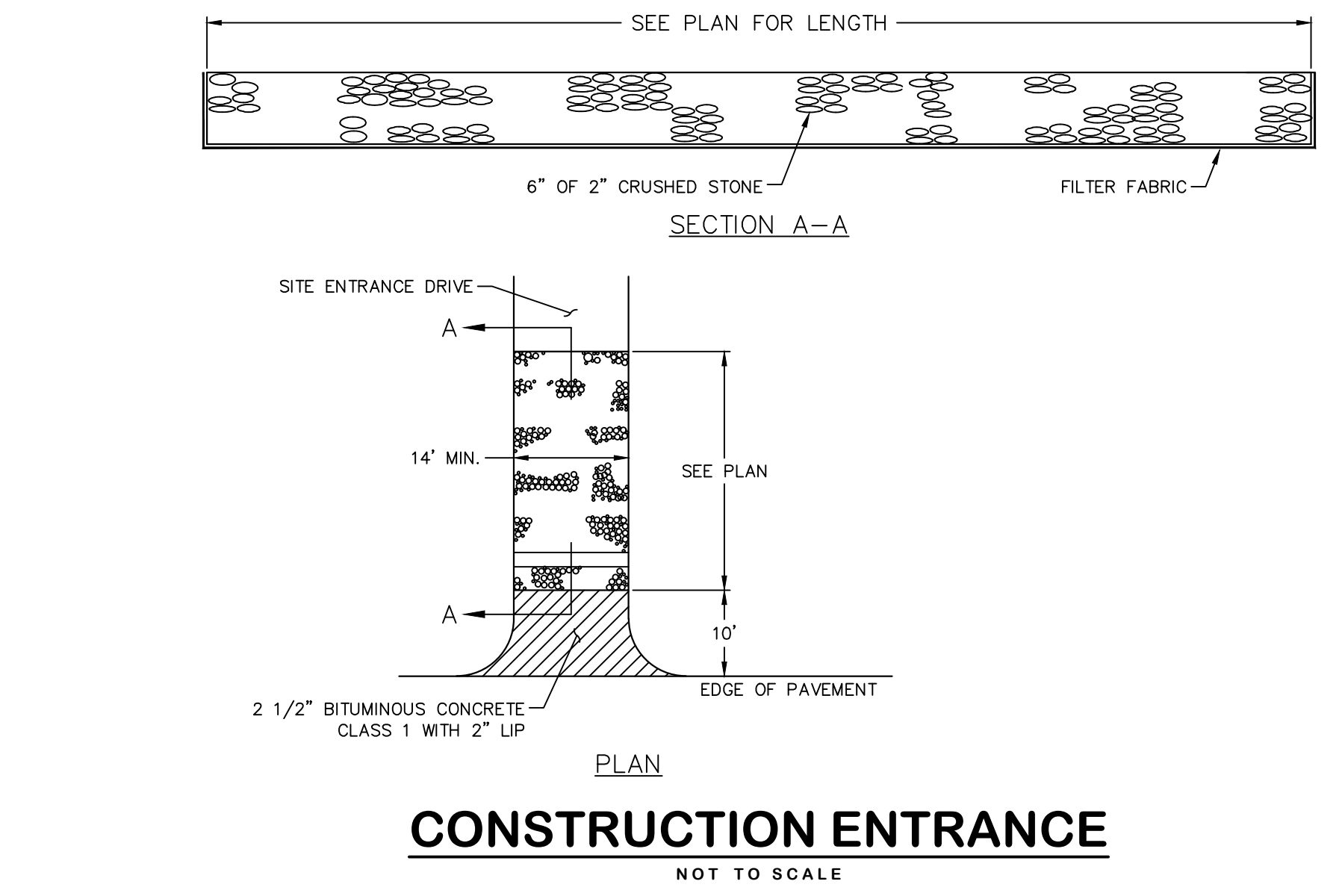
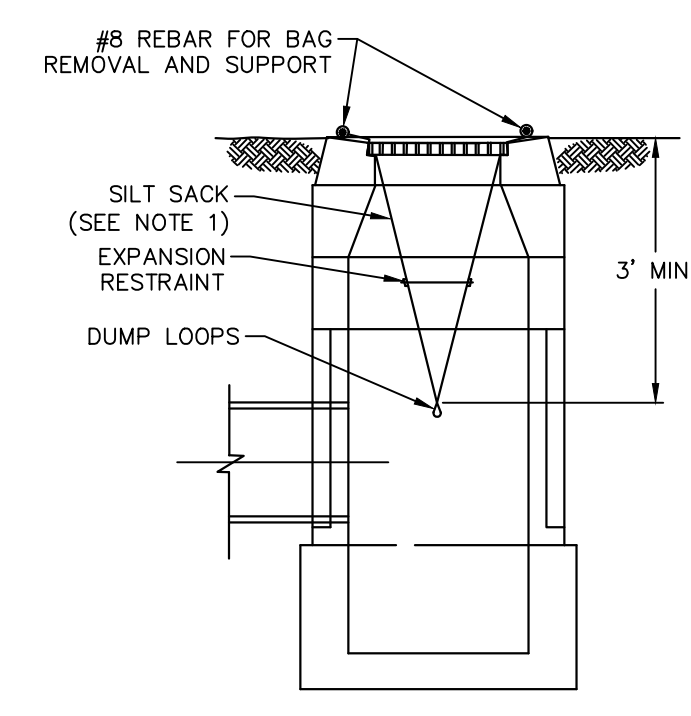
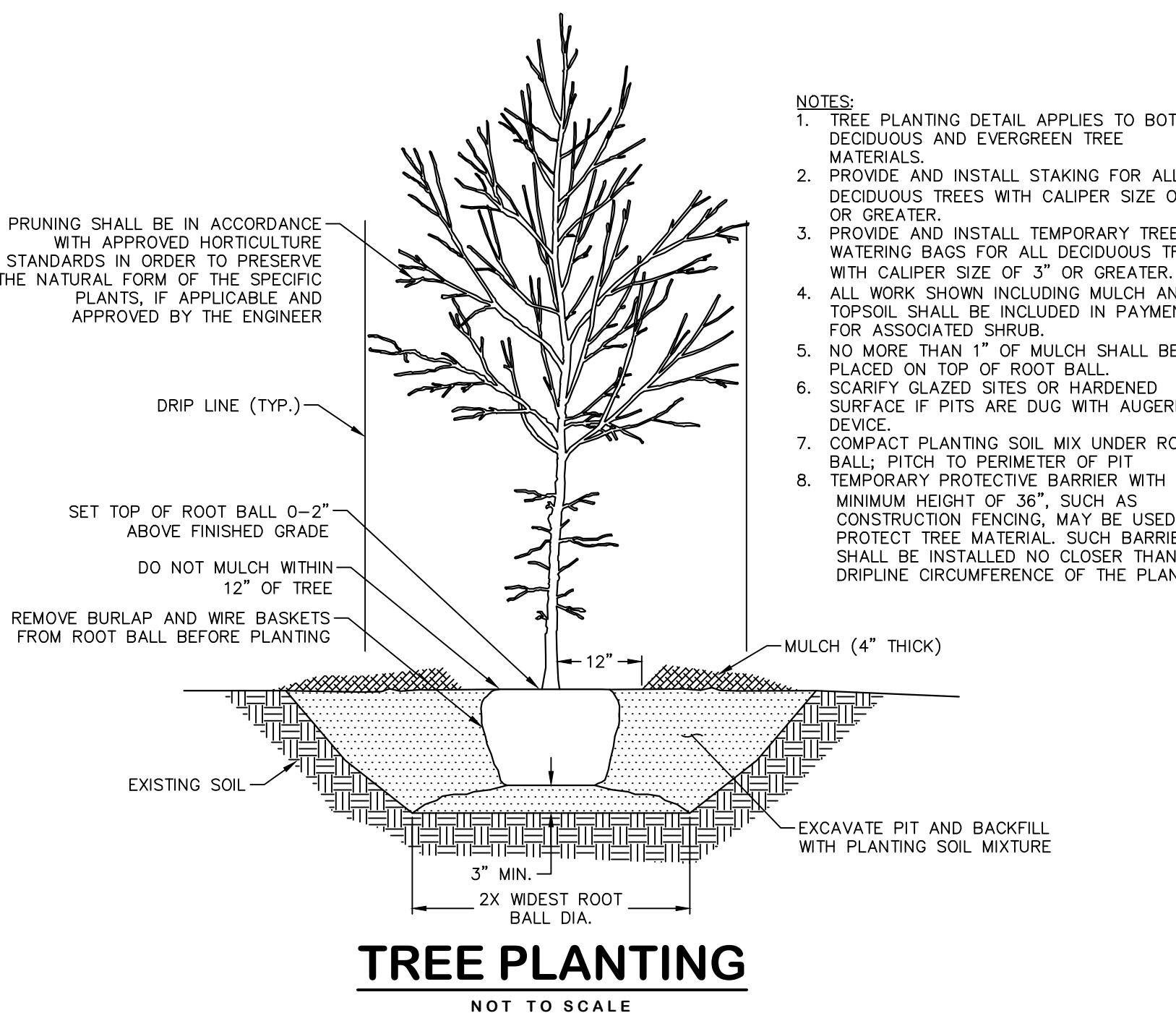
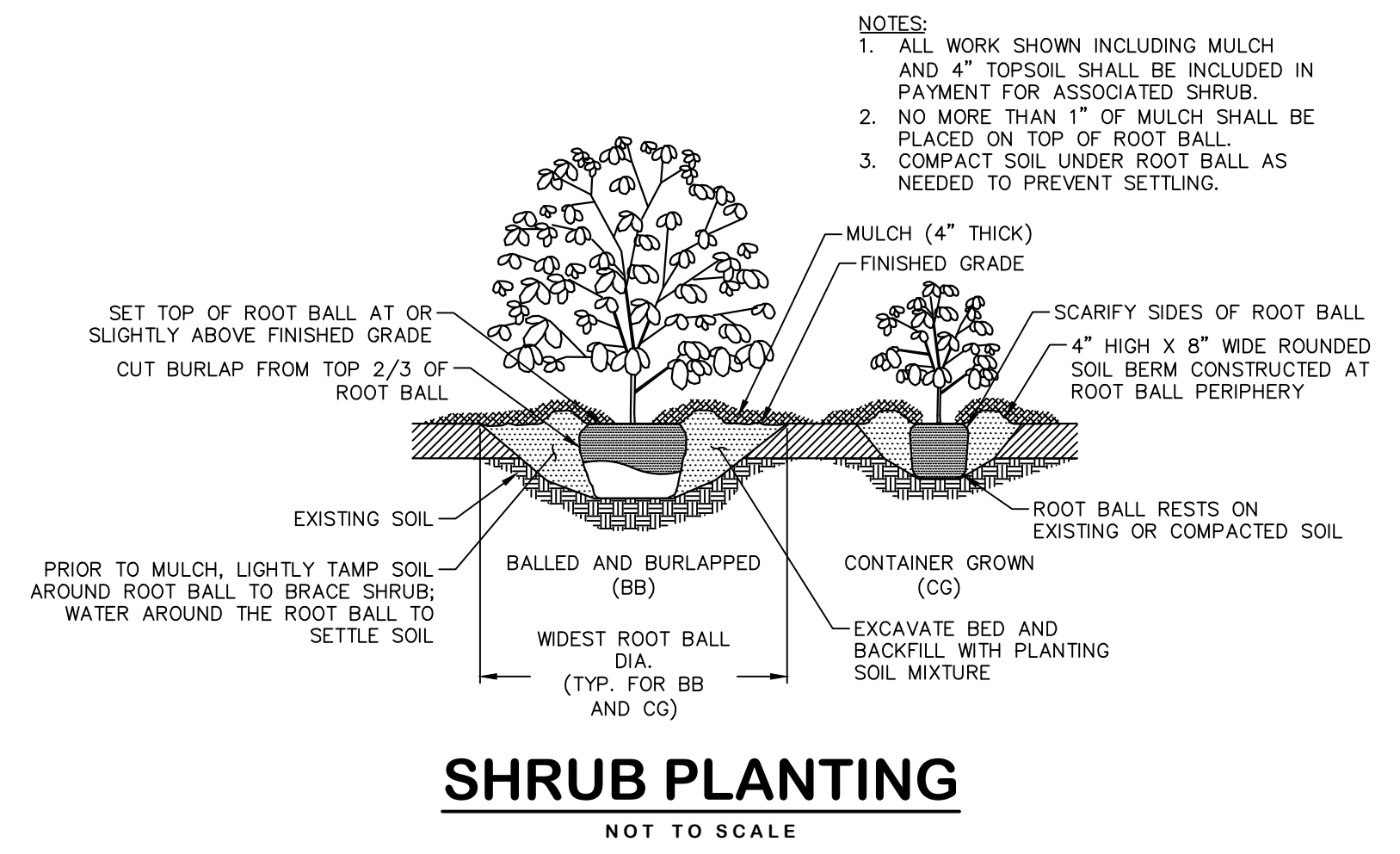
DATUM
HORIZONTAL: NAD83 VERTICAL: NAVD88

PROJECT LOCATION
**CHARTER OAK STREET
MANCHESTER, CT**

PROJECT TITLE
**PARK IMPROVEMENTS
CHARTER OAK WEST**

SHEET TITLE
DETAILS

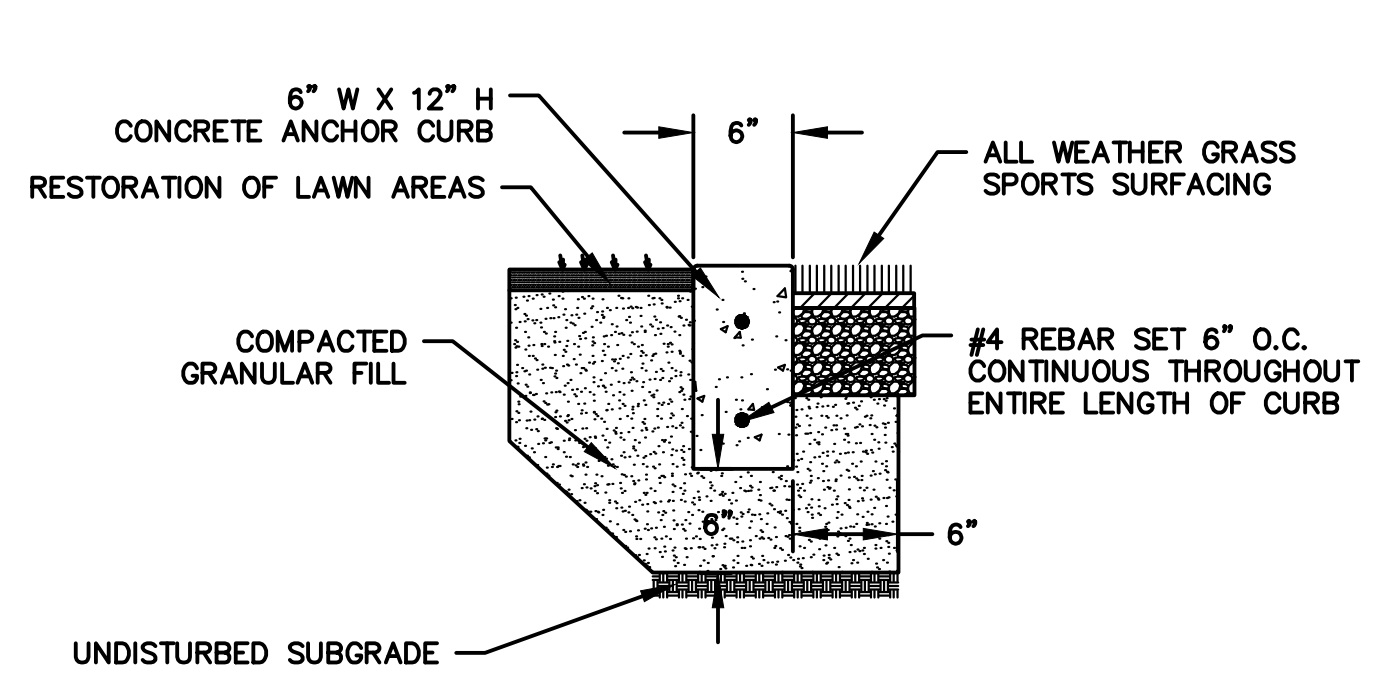
SHEET NUMBER
14 of 16



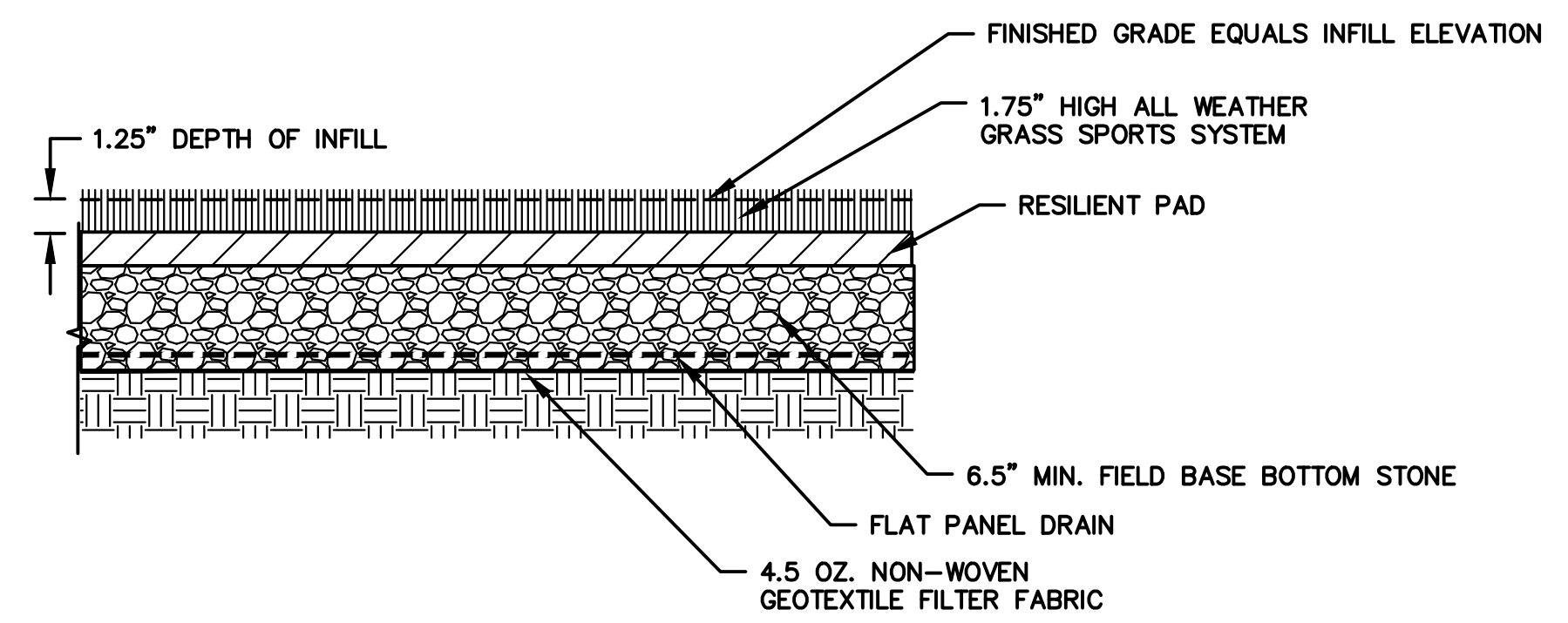
PROJECT NOTES

- ALL MATERIALS AND METHODS OF CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE "MANCHESTER PUBLIC IMPROVEMENT STANDARDS", EFFECTIVE OCTOBER 31, 2020, AS AMENDED AND THE STATE OF CONN. DEPT. OF TRANSPORTATION STANDARD SPECIFICATIONS FOR ROADS, BRIDGES, FACILITIES AND INCIDENTAL CONSTRUCTION, FORM 818, DATED 2020, INCLUDING ANY SUPPLEMENTS.
- ALL ELEVATIONS ARE BASED ON THE TOWN OF MANCHESTER CONTROL NETWORK.
- IMPLEMENTING WORKER SAFETY AND HEALTH PROTOCOLS THAT ADDRESS COMPLIANCE WITH ALL RULES, LAWS AND REGULATIONS REGARDING SAFETY AND RISK OF EXPOSURE TO PHYSICAL AND CHEMICAL HAZARDS IS THE SOLE RESPONSIBILITY OF THE CONTRACTOR. ALL EMPLOYEES OF THE CONTRACTOR AND SUBCONTRACTORS ARE TO WEAR REFLECTIVE VESTS AND HARD HATS AT ALL TIMES WHEN ON THE PROJECT SITE.
- A PRECONSTRUCTION MEETING WITH TOWN STAFF IS REQUIRED PRIOR TO THE START OF ANY CONSTRUCTION ACTIVITY.
- IT IS ANTICIPATED THAT IMPROVEMENTS IDENTIFIED ON THE PLANS AS BEING COMPLETED "BY OTHERS" WILL BE DONE PRIOR TO OR AT THE BEGINNING STAGES OF CONSTRUCTION.
- THE CONTRACTOR SHALL TAKE CARE NOT TO DISTURB EXISTING MONUMENTATION THAT MAY BE PRESENT NEAR THE PROJECT AREA.
- THE CONTRACTOR IS RESPONSIBLE TO OBTAIN ALL REQUIRED PERMITS AND PAY ASSOCIATED FEES PRIOR TO ANY CONSTRUCTION ACTIVITY. THE CONTRACTOR MUST OBTAIN AN ENCROACHMENT PERMIT FROM THE STATE OF CONNECTICUT, DEPARTMENT OF TRANSPORTATION, PRIOR TO BEGINNING WORK WITHIN THE CHARTER OAK STREET RIGHT-OF-WAY AND SHALL BE RESPONSIBLE FOR ALL ASSOCIATED FEES AND REQUIREMENTS.
- THE CONTRACTOR SHALL COMMIT SUFFICIENT RESOURCES TO THE PROJECT TO ENSURE THE PROJECT IS COMPLETED WITHIN THE ALLOTTED CONTRACT TIME. ONCE MOBILIZED, THE CONTRACTOR SHALL WORK CONTINUOUSLY ON THE PROJECT UNTIL COMPLETION. ANY UNAUTHORIZED VACATING OF THE JOBSITE IS SUBJECT TO PENALTIES DESCRIBED UNDER THE "LIQUIDATED DAMAGES" SECTION OF THE CONTRACT SPECIFICATIONS.
- CONSTRUCTION ENTRANCES SHALL BE INSTALLED WHERE SHOWN ON THE PLANS OR WHERE DIRECTED BY THE ENGINEER DURING CONSTRUCTION. ALL CONSTRUCTION TRAFFIC SHALL ACCESS AND EXIT THE SITES OVER CONSTRUCTION ENTRANCES. CONTRACTOR SHALL PROVIDE TEMPORARY BARRIERS OR CONSTRUCTION FENCE ACROSS OTHER ACCESS LOCATIONS TO THE SITE AT NO ADDITIONAL COST.
- NO WORK SHALL COMMENCE UNTIL ALL CONSTRUCTION AREA SIGNS ARE IN PLACE.
- ALL GRASSED AREAS DISTURBED BY THE CONTRACTOR SHALL BE REPLACED WITH TOPSOIL, FERTILIZED AND SEEDDED AS PER THE SPECIFICATIONS. CONTRACTOR SHALL MAKE ALL EFFORTS TO MINIMIZE THE LIMITS OF DISTURBANCE AND ASSOCIATED RESTORATION THAT IS REQUIRED.
- ALL SEDIMENT CONTROL SYSTEMS SHALL MEET THE "GUIDELINES FOR SOIL EROSION AND SEDIMENT CONTROL" AS PREPARED BY THE CONNECTICUT COUNCIL ON SOIL AND WATER CONSERVATION, LATEST REVISION. THE CONTRACTOR IS RESPONSIBLE FOR THE INSTALLATION, MAINTENANCE AND REPAIR OF EROSION CONTROLS REQUIRED FOR THE PROJECT. ADDITIONAL EROSION CONTROLS SHALL BE INSTALLED BY THE CONTRACTOR FOR TEMPORARY STOCKPILING OF EXCAVATED MATERIAL AND WHERE DEEMED NECESSARY BY THE ENGINEER. EROSION CONTROLS SHALL REMAIN IN PLACE AND BE MAINTAINED BY THE CONTRACTOR UNTIL THE SITE IS STABILIZED AND THE ENGINEER APPROVES THEIR REMOVAL.
- SILT SACKS SHALL BE INSTALLED IN ALL EXISTING AND PROPOSED CATCH BASINS WITHIN THE PROJECT AREA AND WHERE DIRECTED BY THE ENGINEER. SILT SACKS SHALL BE THE APPROPRIATE TYPE FOR CATCH BASINS WITH AND WITHOUT CURB INLETS.
- HORIZONTAL AND VERTICAL LOCATIONS OF PROPOSED WORK MAY BE ADJUSTED TO FIT EXISTING FIELD CONDITIONS WITH THE APPROVAL OF THE ENGINEER.
- THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE ENGINEER IF CONDITIONS ENCOUNTERED IN THE FIELD ARE DIFFERENT THAN INFORMATION SHOWN ON THE PLANS.
- THE EXISTENCE OF UTILITIES AND APPURTENANCES AS SHOWN ON THESE DRAWINGS ARE FOR REFERENCE ONLY. THE EXACT SIZE, LOCATION, TYPE, AND ELEVATION OF ALL UTILITIES WITHIN ALL WORK AREAS SHALL BE THOROUGHLY INVESTIGATED BY THE CONTRACTOR PRIOR TO THE START OF CONSTRUCTION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR NOTIFYING "CALL-BEFORE-YOU-DIG" AT 1-800-922-4455 AND MUST HAVE ALL UTILITIES MARKED ON THE GROUND PRIOR TO THE START OF CONSTRUCTION.
- THE QUANTITIES AS INDICATED IN THE CONTRACT DOCUMENTS ARE APPROXIMATE AND MAY NOT INDICATE THE ACTUAL QUANTITIES OF WORK REQUIRED. THE CONTRACTOR MUST VERIFY ALL QUANTITIES.
- SURPLUS EXCAVATED MATERIAL SHALL BECOME THE PROPERTY OF THE CONTRACTOR. THE CONTRACTOR SHALL DISPOSE OF SURPLUS EXCAVATED MATERIAL IN ACCORDANCE WITH STATE AND FEDERAL REGULATIONS.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR TEMPORARY HANDLING OF ALL STORMWATER RUNOFF DURING CONSTRUCTION. METHODS OF HANDLING RUNOFF SHALL BE APPROVED BY THE ENGINEER.
- AT THE END OF EACH WORKING DAY, THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONNECTING NEW DRAINAGE SYSTEMS TO EXISTING. ALL DRAINAGE SYSTEMS WITHIN THE CONSTRUCTION LIMITS SHALL BE MAINTAINED BY THE CONTRACTOR.
- FOR CONNECTIONS TO EXISTING SANITARY SEWER STRUCTURES AND PIPING, THE CONTRACTOR SHALL VERIFY EXISTING INVERT ELEVATIONS, NOTIFY THE ENGINEER IF A DISCREPANCY EXISTS, AND ADJUST THE PIPE SLOPES AS DIRECTED.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR TEMPORARY HANDLING OF ALL SEWAGE FLOWS DURING CONSTRUCTION. METHODS OF HANDLING SEWAGE FLOWS SHALL BE APPROVED BY THE ENGINEER.
- AT THE END OF EACH WORKING DAY, THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONNECTING EXISTING SANITARY SEWER LATERALS TO NEW SANITARY FACILITIES. THE CONTRACTOR IS RESPONSIBLE FOR PROPER DISPOSAL OF ALL MATERIALS.
- ALL SANITARY SEWER MANHOLES SHALL HAVE A 48" INTERNAL DIAMETER UNLESS SPECIFIED OTHERWISE ON THE PLANS.
- ALL NEW WATER MAINS AND SERVICES SHALL HAVE 4.5 FEET OF COVER UNLESS OTHERWISE INDICATED ON THE PLANS. COVER LESS THAN OR IN EXCESS OF 4.5 FEET SHALL BE ALLOWED ONLY WHERE INDICATED ON THE PLANS OR APPROVED BY THE ENGINEER. WATER MAINS AND SERVICES WITH COVER LESS THAN 4.5 FEET SHALL BE INSULATED UNLESS APPROVED OTHERWISE BY THE ENGINEER.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING ANY TEMPORARY THRUST RESTRAINT THAT IS REQUIRED.

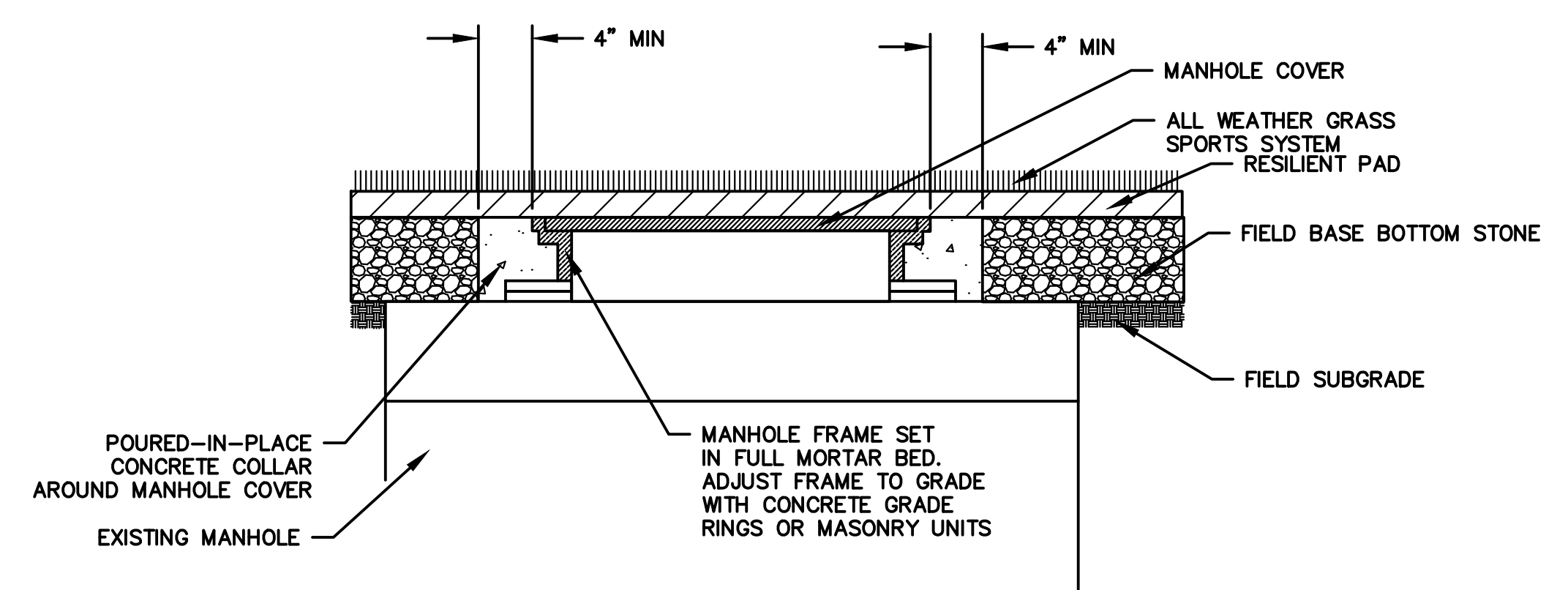
APPROVED
PLANNING & ZONING
COMMISSION
MANCHESTER, CT.
DATE: _____
SIGNED: _____



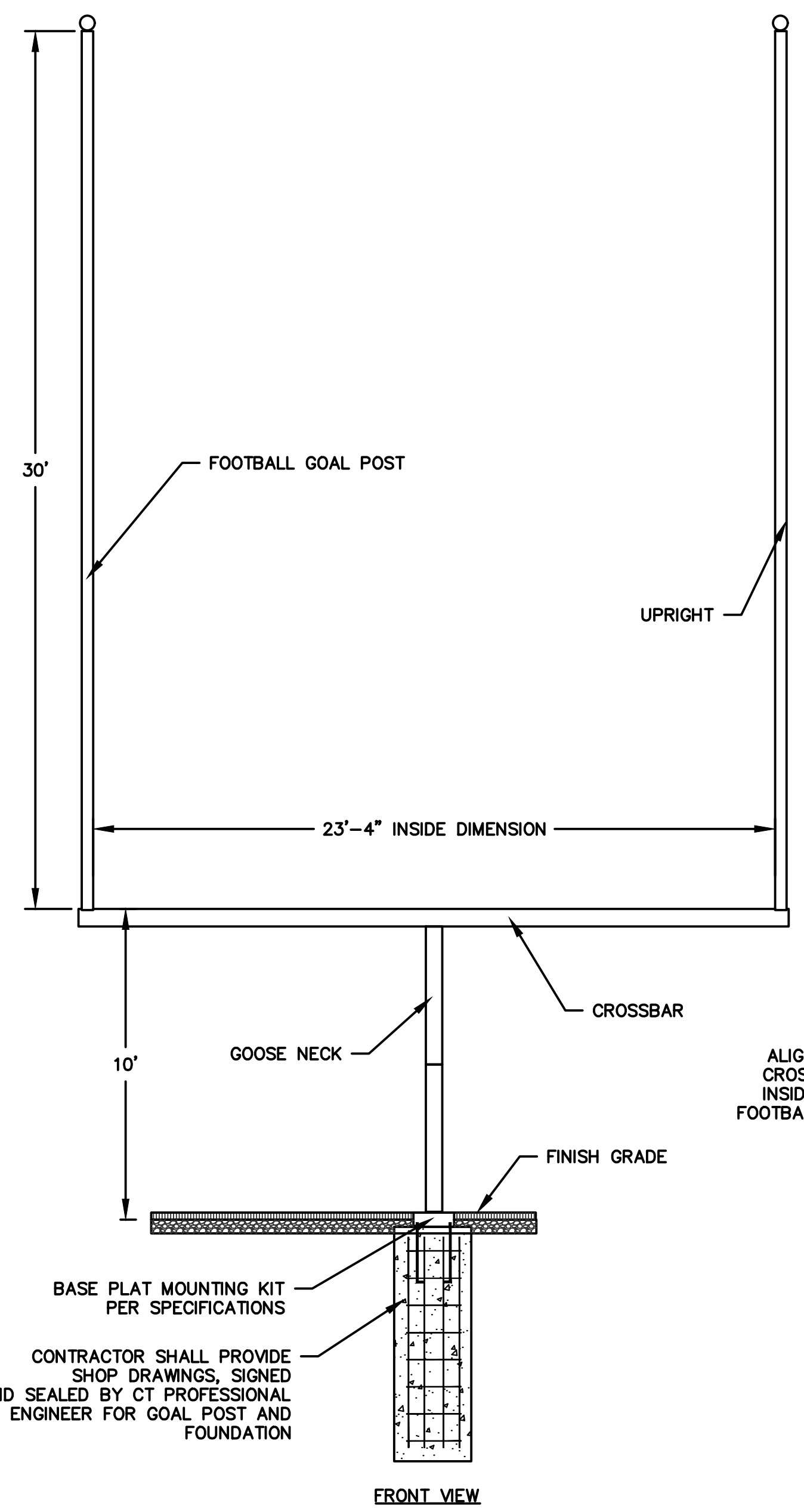
CONCRETE ANCHOR CURB
NOT TO SCALE



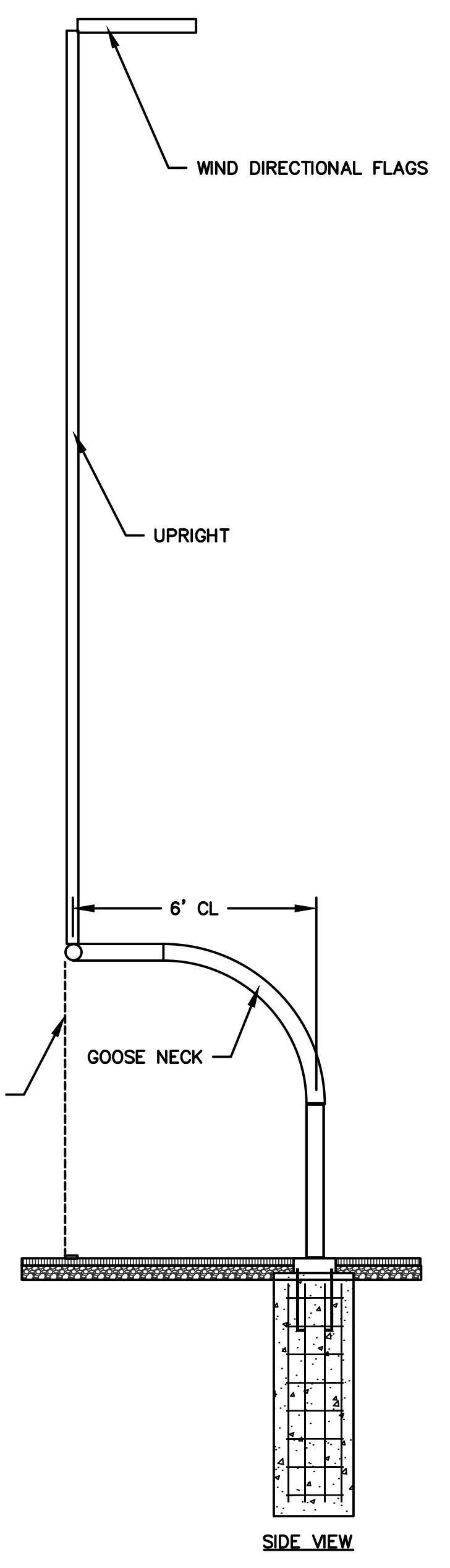
ALL WEATHER GRASS SPORTS SURFACE
NOT TO SCALE



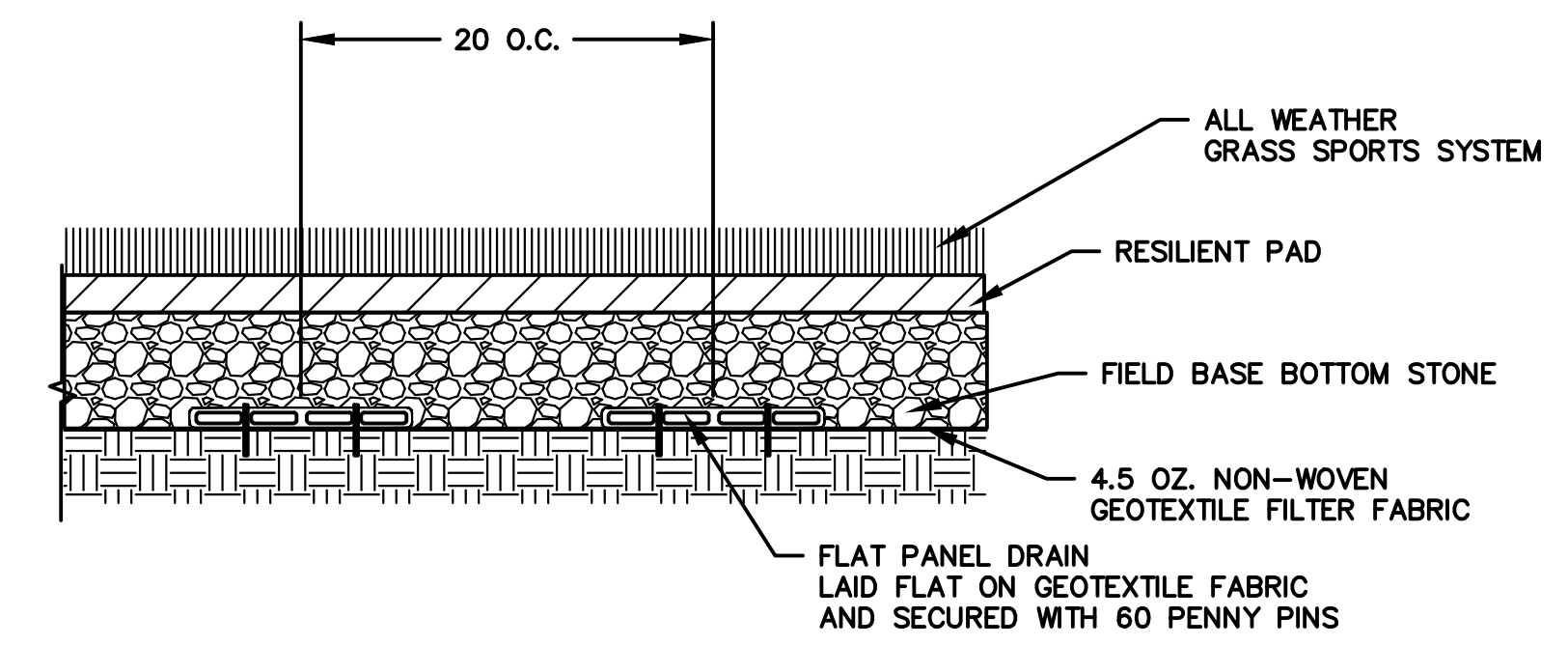
MANHOLE IN ALL WEATHER GRASS
NOT TO SCALE



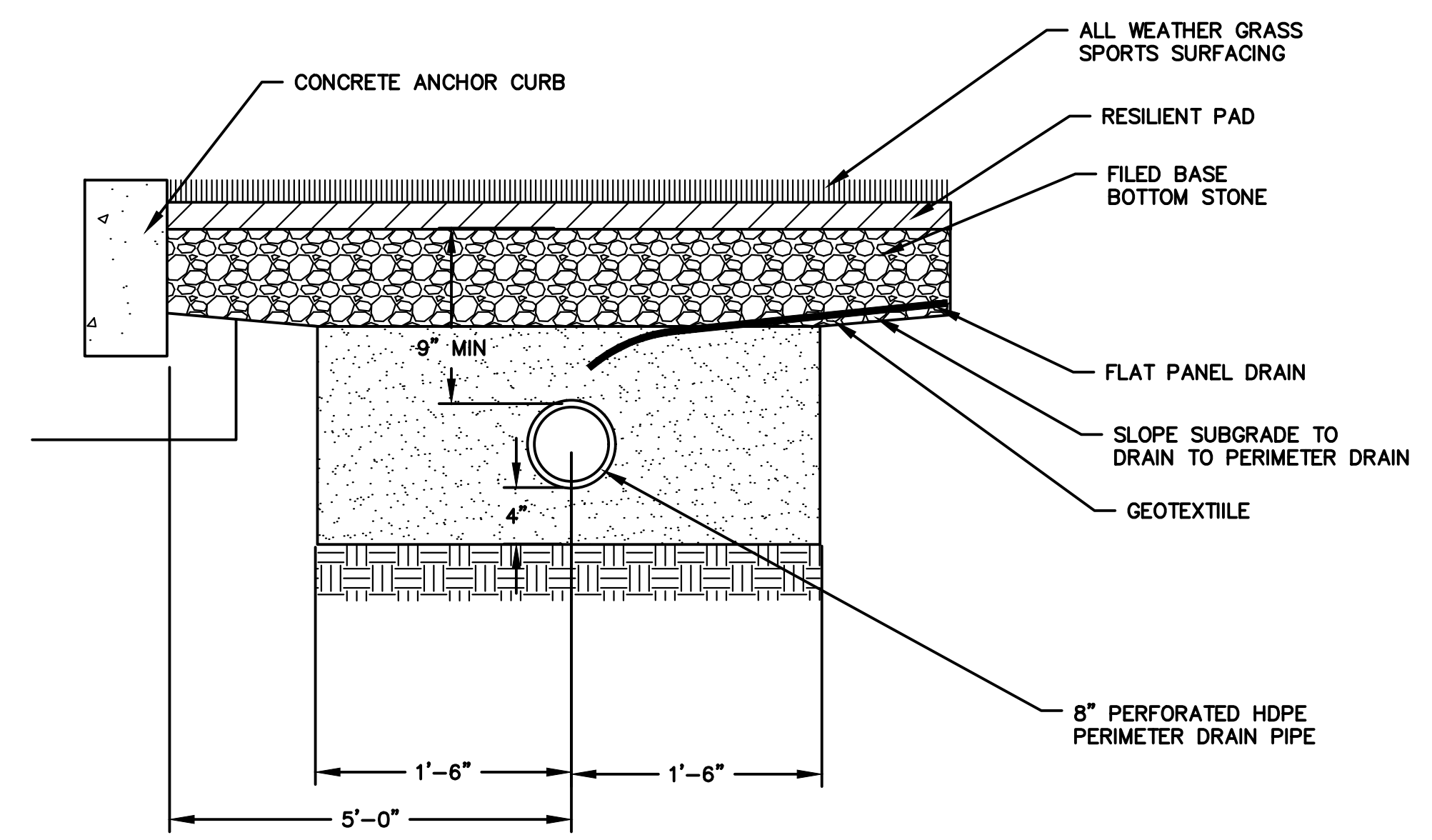
FOOTBALL GOAL POST
NOT TO SCALE



SIDE VIEW



FLAT PANEL DRAIN
NOT TO SCALE



FIELD PERIMETER COLLECTION DRAIN
NOT TO SCALE

APPROVED
PLANNING & ZONING
COMMISSION
MANCHESTER, CT.
DATE: _____
SIGNED: _____



TOWN OF MANCHESTER
PUBLIC WORKS DEPARTMENT
ENGINEERING DIVISION
494 MAIN STREET - P.O. BOX 191
MANCHESTER, CT 06045-0191

LEGEND

—	WETLAND BOUNDARY	☆	LIGHT POLE
—	RETAINING WALL	⊗	CONIFEROUS TREE
—	QUAKE WALL	⊙	DECIDUOUS TREE
—	STONE WALL	⊕	SANITARY MANHOLE
—	STOCKADE FENCE	⊖	DRAINAGE MANHOLE
—	WIRE FENCE	⊗	CATCH BASIN
—	CHAIN LINK FENCE	⊕	CULVERT END
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—	SILT FENCE	⊕	WATER VALVE
□	CONCRETE MONUMENT	⊖	BUTTERFLY VALVE
■	GRANITE MONUMENT	⊕	BLOW OFF
○	IRON PIPE	⊗	SIGN
●	IRON ROD	⊕	DOUBLE POST SIGN
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⊕	TRAFFIC SPAN POLE	⊗	TELEPHONE BOX
⊖	ELECTRIC BOX	⊕	CATV TUBE
⊗	WETLAND FLAG		

PROJECT NUMBER
2021078

FILENAME
2021078-PLAN-PZC.DWG

NO.	DATE	FILE
—	03/15/24	PZC PERMIT SUBMISSION

DRAWN BY:
CHECKED BY: JL
RELEASED BY: TB

DATUM
HORIZONTAL: NAD83 VERTICAL: NAVD88

PROJECT LOCATION
**CHARTER OAK STREET
MANCHESTER, CT**

PROJECT TITLE
**PARK IMPROVEMENTS
CHARTER OAK WEST**

SHEET TITLE
DETAILS

SHEET NUMBER
15 of 16

DRAFT

**TOWN OF MANCHESTER
MINUTES OF PUBLIC HEARING
HELD BY THE PLANNING AND ZONING COMMISSION/
INLAND WETLANDS AND WATERCOURSES AGENCY
APRIL 1, 2024**

MEMBERS PRESENT:

In Person: Eric Prause, Chairman
Patrick Kennedy, Vice Chairman
Michael Stebe, Secretary
Daniela Luna
Michael Farina
Electronically: Teresa Ike

ALTERNATE MEMBERS SITTING:

In Person: Bonnie Potocki

ABSENT:

Chris Schoeneberger
Maliha Ahsan
Zachary Schurin

ALSO PRESENT:

In Person: Megan Pilla, Principal Development Planner
Gary Anderson, Director of Planning & Economic
Development
Electronically: David Laiuppa, Environmental Planner/Wetlands
Agent
Nancy Martel, Recording Secretary

The Chairman opened the Public Hearing at 7:00 P.M. The Secretary read the legal notice when the call was made.

PROPOSED SIDEWALK AND CURB PLAN 2024-2029 – Proposed amendments to the Town Sidewalk and Curb Plan

Ms. Pilla presented the proposed Sidewalk and Curb Plan, referring to the ordinance that created it. The ordinance dictates the procedure for determining whether a developer needs to install sidewalks when building a new building on an undeveloped lot. That comes into play if such installation is in conformance with the sidewalk and curb plan as approved by the Town Planning and Zoning Commission.

Per the ordinance, the plan sets forth:

- The Commission is responsible for preparing and adopting the plan and updating it every five years.

- The plan shall set forth the policies for determining where sidewalks and curbs will be installed.
- Enumerates examples of what constitutes good cause for allowing or requiring payment in lieu of sidewalk installation.
- Before adoption, the plan shall be submitted to the Board of Directors for review and comment, which has been done.

Proposed Changes:

- Adding a “Purpose” section to clearly reference the ordinance that creates the sidewalk plan.
- There are several locations with assorted language revisions.
- Adding a section “Maps” to identify the two maps with the plan. The written documents and the plan go together.
 - o The Sidewalk Location Map is the map of existing and anticipated sidewalks along public streets.
 - o The Roadway Classification Map is a classification of all public streets by type, to be used in accordance with the policies outlined below for locating new sidewalks. This map is not proposed to change but is included in the plan because it is a reference for some of the criteria, including:
 - Table One – Minimum Standards for Sidewalks are based on the roadway classification (arterial street, collector street, local street, rural/scenic road).

Priority Streets

The list of priority streets for sidewalk installations is updated, which Ms. Pilla detailed. Several on the list have been partially completed. Ms. Pilla reported that, although these are the priority streets identified for sidewalks and extensions, the timing of projects is dependent on the availability of funds and other circumstances.

Removal of Sidewalks

Some of the language has been removed. Staff has agreed that, at this point, the Town doesn’t recommend sidewalk removals on the prior scale. On a case-by-case basis, it may be prudent to consider removals.

The new language was explained by Ms. Pilla.

Plan of Conservation and Development

There is an update to the refence to the Plan of Conservation and Development, as that has been updated.

Good Cause for Payment in Lieu of Installation

The language in the plan has been reworded and clarified.

Sidewalk Location Map

Proposed Changes:

- Updating completed projects.
- Removing the category “Neighborhood Identified for Possible Sidewalk Removal Projects” from the legend.
- Strike the term “bike lane” in the legend, as they are “multi-use trails.”
- Remove any trails that are not adjacent to public streets.
- Remove connection between Redwood Road and Briarwood Drive, which is infeasible.
- Hartford Road is currently called out as bituminous trail, which will be changed to concrete sidewalk.
- Completed sidewalks need to be reflected as such.

Roadway Classification Map

This map is not proposed to be changed.

After a question from Ms. Potocki, Mr. Jeff LaMalva, Town Engineer, stated that, to his knowledge, there are not any projects that would have any drainage implications for the MS-4 permit, though he will look into it. He added that there are no plans to rip up sidewalks to be in accordance with the MS-4 permit.

Ms. Pilla clarified that “disconnect impervious surfaces” is disconnecting from the Town storm water system and sidewalks generally do not contribute to the Town storm water system.

Ms. Luna asked about the priority street selection process, particularly related to Bush Hill Road. Mr. LaMalva explained that it is a rural scenic path. He reported that the priority list is in no particular order and is based on discussions with the Board of Education and e-mails the Town Engineering Department receives.

After a suggestion from Mr. Anderson, Mr. LaMalva detailed the types of sidewalk classifications. The classifications in the rural areas are to preserve the rural characteristics of the neighborhood.

Mr. Prause assumed that Tolland Turnpike is still on the list because there is still some work to be done, which Mr. LaMalva confirmed. One revision shown includes an off-road shared use path between Buckland Street and Chaplin Road.

After noting the number of projects that have been completed, Mr. Prause speculated that the list is too aggressive for a five-year plan. Mr. LaMalva stated that, in the past 8-10 years with the Complete Streets policy, most of the sidewalk gap filling done had been associated with that. They are not standalone sidewalk installation projects. New ARPA funding has been put toward sidewalk extensions.

Mr. Farina asked about the Redwood to Briarwood section and whether there is an easement, which was unknown. Noting Mr. LaMalva's comment on state funding for Tolland Turnpike, since it is in the future, he asked whether it should be taken off the list. Mr. LaMalva explained that it is not being removed, only relocated. Noting an area that had not been on the priority list, Mr. Farina asked how it was completed. Mr. LaMalva stated that it was on the map, just not identified as a priority in the plan, and gave a detailed explanation of the process.

Mr. Stebe proposed several language edits, which were discussed with Mr. LaMalva. He commented that a "crosswalk" is needed between the road rebuilding, the sidewalk and the Complete Streets to make it a complete loop. It needs to be stated that the sidewalks are part of a Complete Streets plan and should be and will be included in the rebuilding, repair and maintenance of the Town roads.

Mr. Prause asked whether this should be revisited in a couple of weeks, adding language and tweaking recommendations about taking away some of the sidewalk removal icons. Ms. Pilla agreed, though it could also be a modification to an approval.

Mr. Anderson observed that the conversation is not complete at this point, and as there are additional items, it may need to be brought back.

Ms. Potocki asked what dictates the location of the sidewalk, which Mr. LaMalva explained, noting that the classification of the road determines whether both sides of a street should have sidewalks. Mr. Anderson asked whether the Commission would like to add to the plan, which was agreed upon.

After an inquiry from Ms. Ike, Ms. Pilla explained the change from PWD to DPW.

Mr. Farina gave an overview of his policy document, which was prompted because of children who are forced to walk in the street to get to school. He reported:

School Walk Safety Policy – Prioritize arterial and collector roads within 1 and 1-1/2 mile from schools.

Last Mile Transportation Policy (in the POCD) – How bus stops fit into transit-oriented development in the Complete Streets policy.

Parks & Rec – Some children can walk to recreational facilities while others cannot, creating inequity.

Roadwork Construction – Taking DPW roadwork plans and creating the sidewalk plan accordingly.

Length of Time on Priority List – If something has been on the priority list for a long period of time, perhaps it should be elevated.

Street Pedestrian Lighting Policy

Implementation Policy – In the POCD, everything cannot be completed immediately, but the Commission should be making somewhat realistic recommendations.

Ms. Potocki stated that she is a proponent of linking the plan to Connecticut State Routes to School.

Mr. Kennedy speculated that the items in the document are likely considered by the Board of Directors and the DPW. He commented that a second document does not interact well with the actual plan. Fiscal decisions are the BOD's responsibility. The stated purpose of the Commission's document is in the capacity of approving subdivisions and whether to require sidewalks. If any edits are to be made, it should be within the context of the Commission's plan.

Mr. Farina noted that his goal is not to have two separate documents. He would like to see at least some of the policies he put together becoming a part of the Commission's sidewalk plan. It is his opinion that the Commission does not have the policies within the plan to the extent they could for prioritization. Mr. Farina added that he wrote the Street Pedestrian Lighting policy because it is on Page 69 of the POCD.

Ms. Luna agreed with Mr. Kennedy that the budget and what is allowed falls to the Board of Directors. She asked why it has taken so long to address items on the sidewalk plan and whether the Commission could add a timeline to the plan.

Ms. Pilla clarified that it is not the case that none of the priority extensions have been completed. There is one crossed off, a few that are not crossed off because there are gaps remaining, and at least one that has not been completed within the five-year period, but funding has been secured.

Mr. Stebe offered that there are many valid points, noting that the Commission is tasked with driving the planning and development within the Town of Manchester. A large part of the verbiage in the sidewalk plan has not been touched in three or four cycles of this revision. Striking the removal of sidewalks is the largest revision since he has been on the Commission. Other parts of the Town must look at the sidewalk plan for various uses. The Commission should include verbiage to link the sidewalk plan with other plans in existence.

Mr. Prause thanked Mr. Farina for drafting policies and felt the School Walk Safety, Last Mile Transportation and Parks & Rec policies could be directed options. It was his opinion that the Length of Time policy and the Implementation policy are hard for the Commission to dictate.

Ms. Pilla stated that staff did consider the POCD recommendation on pedestrian lighting and agreed with the intent of the POCD recommendation to include it in the sidewalk plan. However, the sidewalk plan may not be the appropriate place for it. Staff agrees with the draft policies but the reasons she did not recommend including them in the proposal are:

1. They do potentially encroach upon the decision-making processes of Public Works.
2. Although the ordinance states, "The plan shall set forth the policies for determining where sidewalks and curbs would be installed," it must be taken in the context of the

ordinance. The ordinance is for sidewalk and curb installation on existing streets simultaneous with building construction and is not directing the Commission to adopt policies outside of that realm. The Town Attorney's office strongly recommended against adopting any other policies outside the framework of the ordinance. They suggested that, if the Commission is inclined to consider adding the policies, the public hearing should be kept open so Attorney Tim O'Neil can join in the next meeting.

Ms. Pilla explained that they can include statements within the plan that refer to them, but to call it a policy is outside the framework of the ordinance that establishes this plan.

Mr. Prause sought clarification about sidewalk and curb installation on existing streets simultaneous with construction. It appears the policies referred to have to do with when a street is being constructed.

Ms. Pilla clarified that, when a new building or subdivision is being constructed, the plan must be referred to in order to determine whether the developer is responsible for installing sidewalks along the development.

Mr. Anderson added that there is a way to incorporate these ideas in the document, stating that these are the ways the streets on the plan will be prioritized. It could be stated that concepts and ideas come from the POCD, make a lot of sense, and are a way to modernize the way we think about sidewalks. There is not a lot missing from the plan at this time and the Commission should take a step back. Over the last 20 years, the Town (PZC, BOD and staff) has done an amazing job in implementing this sidewalk plan and connecting a lot of the gaps in the sidewalk network in town. It should be noted that a lot of progress has been made over the last couple of decades.

Mr. LaMalva stated that, after hearing a lot of these policies, it reminded him that, in 1990, the Public Works Department presented to the BOD on something for the sidewalk replacement program. There were 10 criteria and they physically assigned each segment a rating. It seems to him that it could be done for the sidewalk extensions using these criteria to develop the priorities.

Mr. Farina remarked that there is also state statute that clearly gives the Commission the authority to produce any map on sidewalks that they like with the priorities they want. He reiterated his belief that it is a good idea to have policies which answer many questions and read from his implementation policy. He pointed out that, on his road, the sidewalks were replaced, and currently they are being torn up.

Ms. Luna and Mr. LaMalva had a brief discussion about a particular sidewalk. Ms. Luna asked, if the policies can't be included in the sidewalk plan, whether they could be a recommendation to the BOD.

Mr. Anderson explained that there is a way to include the proposal for prioritization of sidewalks. The ideas in here are important, but some of the details are not within the PZC's purview and he suggested the members leave those to the DPW.

Ms. Potocki felt that this ordinance is antiquated. She felt the title should add redevelopment. The Commission could produce a list of evaluation criteria to be considered by DPW.

Mr. Kennedy observed that this is a specific application that arises under the specific ordinance and only the Board of Directors can pass a new ordinance. The Commission must act on what is before them.

Ms. Pilla noted that the current plan is valid through this month. If there is a delay, a gap of a couple of weeks is acceptable.

There were no members of the public to speak.

Mr. Prause noted that this will most likely be continued and produced a list of items to be addressed in the meantime, after which a discussion was undertaken. Mr. Anderson recommended mentioning Mr. Farina's policies in the beginning of the document or where sidewalk gap closures and expansions are discussed. Mr. Prause referred to comments regarding prioritization.

Mr. Kennedy commented that, if the Commission must get into prioritization, which he is dubious about given their jurisdiction, perhaps the plan should state "consider" regarding priorities. Anything more than that gets beyond the Commission's jurisdiction.

Mr. LaMalva stated that, based on the funding they have been receiving over the last 15-20 years, they are not selecting sidewalk projects. 90% of the sidewalk extension projects done are in conjunction with a road project. For others, they are chasing grants. Most of the funding opportunities are very specific. They are not missing out on grants because of lack of information in the plan or map.

Mr. Anderson remarked that staff needs more detail about how to incorporate the items Mr. Farina has mentioned. There should be some criteria in the document stating how decisions will be made. He asked whether the Commission wants staff to put something together. He asked if there is any input from the full Commission about how they envision it.

Mr. Prause would like to see the first three policies addressed. He was unsure about having rationale for all the different streets.

Ms. Pilla sought clarification because, if the Commission wants to call them policies, Attorney O'Neil would like to be here to discuss it.

Mr. Anderson suggested staff draft something that can be reviewed by the Town Attorney's office.

Mr. Prause suggested that the undercurrent of the discussion is that, when looking at the plan every five years, it is very similar to the prior lists.

MOTION: Mr. Stebe moved to continue the public hearing to April 15, 2024. Ms. Luna seconded the motion and all members voted in favor.

The public hearing was closed at 8:50 P.M.

I certify these minutes were adopted on the following date:

Date

Eric Prause, Chairman

NOTICE: A DIGITAL RECORDING OF THIS PUBLIC HEARING CAN BE HEARD IN THE PLANNING DEPARTMENT.

DRAFT

**TOWN OF MANCHESTER
MINUTES OF BUSINESS MEETING
HELD BY THE PLANNING AND ZONING COMMISSION/
INLAND WETLANDS AND WATERCOURSES AGENCY
APRIL 1, 2024**

MEMBERS PRESENT:

In Person: Eric Prause, Chairman
Patrick Kennedy, Vice Chairman
Michael Stebe, Secretary
Daniela Luna
Michael Farina
Electronically: Teresa Ike

ALTERNATE MEMBERS SITTING:

In Person: Bonnie Potocki

ABSENT:

Chris Schoeneberger
Maliha Ahsan
Zachary Schurin

ALSO PRESENT:

In Person: Megan Pilla, Principal Development Planner
Gary Anderson, Director of Planning & Economic
Development
Electronically: David Laiuppa, Environmental Planner/Wetlands
Agent
Nancy Martel, Recording Secretary

The Chairman opened the Business Meeting at 8:50 P.M.

PROPOSED SIDEWALK AND CURB PLAN 2024-2029 – Proposed amendments to the Town Sidewalk and Curb Plan.

The public hearing was continued to April 15, 2024.

TOWN OF MANCHESTER PUBLIC WORKS DEPT. – Expansion of Charter Oak Park facilities including a new skate park/pump track, synthetic turf playing field, and parking lot at 30 Charter Oak Street. – Inland Wetlands Permit – Determination of Significance (IWP-0003-2024)

Mr. Jeff LaMalva, Town Engineer, representing the Public Works Department, introduced himself. Mr. LaMalva gave a brief history of the project. He detailed the existing conditions of the parcel, which is currently a grass field used for recreational purposes. It is zoned Residence AA.

The proposed project is to install a 360 ft. x 240 ft. synthetic turf soccer field along the east side of the parcel for adult and youth soccer. There will be vinyl fencing surrounding the field, bleachers and sidewalk along the west, and a 73-stall parking lot to accommodate both the turf field and the skate park utilizing the current curb cut. A 42 ft. x 16 ft. utility building is proposed for storage, restrooms, and a small staff office.

Mr. LaMalva reported that the project includes LED lighting for both the parking lot and the soccer field. Other amenities include benches, bike paths, landscaping, and sidewalk connections. There is no design yet for the skate park and pump track. The parking lot and turf field will go out to bid soon and will be constructed this year into next year. The skate park will be constructed in 2025.

The total site area is 7.4 acres with a total disturbance of 4.2 acres. There are no direct wetland impacts. There is a 0.77-acre impact within the 100 ft. regulated wetlands buffer.

Storm Water Management - The turf field is designed with a stone infiltration system, which was detailed. The parking lot drains to the southwest with proposed catch basins and storm drains directed to a hydrodynamic separator. It will be connected into an existing 48" storm pipe.

Erosion and Sedimentation Control – The total disturbed area is 4.2 acres. Silt fence is proposed downstream of work areas and silt sacks. A construction entrance will be located along the existing curb cut. A topsoil stockpile area will be at the future skate park location.

The project will be inspected daily by Town staff, and they will coordinate with Mr. Laiuppa for his inspection.

Mr. LaMalva reported that the total projected cost is \$5 million. Phase I (turf field and parking lot) will begin this summer and be completed by next May. Phase II will commence immediately following Phase I.

Mr. Stebe sought confirmation that everything being done on this site is taking the grass field and will be considered an impervious cover on the entire project. He asked why there will be a turf field instead of natural turf. The parking lot and skate park will be impervious, aside from a dirt pack for the bumps.

Mr. LaMalva explained that synthetic turf does infiltrate into the ground similar to a grass field. The reason behind the synthetic turf is that the grass fields get tremendous use and more time slots can be programmed on synthetic turf. The reason why there is no detention area shown is that the Public Improvement Standards state that, when along the watercourse, a detention area is not recommended.

After a question from Ms. Luna, Mr. LaMalva reported that most people park in the church parking lot. The proposed parking lot should accommodate the proposed activity contained within the parcel.

To answer a question from Ms. Potocki, Mr. LaMalva stated that there has not been a problem with flooding.

Mr. LaMalva explained that the storm water system being installed will receive the water from the field and the parking lot. Ms. Potocki asked whether permeable pavement was considered. For the parking lot, Mr. LaMalva stated that was not considered. There is a flat panel drain surrounded by stone, which leads to an 8” plastic pipe for overflow leading out into the parking lot system.

A discussion was held between Mr. LaMalva and Mr. Farina regarding parking, as well as permeable pavement.

Mr. Prause expressed concern over the effect of artificial turf on a wetland. Mr. LaMalva reported that he contacted the vendor, who gave information. They are basing their spec off the Manchester High School field. The vendor has done testing and is certifying that their particular turf is PFAS-free.

Mr. Laiuppa stated that his assumption is that the artificial turf is made so little pieces do not break off or break down. He acknowledged that he does not have any expertise or knowledge in the area.

Mr. Prause speculated that not having more information on artificial turf may warrant a public hearing. Ms. Potocki concurred with Mr. Prause’s opinion.

Mr. Kennedy observed that this only goes a small amount into the upland review area. In his opinion, it does not warrant a public hearing.

After a question from Mr. Prause, Mr. LaMalva stated that there is an issue currently that the natural turf is not holding up and they are not able to program as much as they would like to.

Inland Wetland Permit – Determination of Significance (IWP-0003-2024)

MOTION: Ms. Potocki moved to find the proposed activity at the above referenced location as shown on the inland wetlands permit application IWP-0003-2024 may have a significant impact on the wetlands and is in the public interest, and therefore will require a public hearing. Mr. Farina seconded the motion.

Mr. Laiuppa observed that some of the discussion points were about the flood plain. As was mentioned, this is outside of the flood plain. There is no activity directly within the wetland or watercourse, but it is within the upland review area. The two items he felt were relevant were:

1. Any activity involving deposition or removal of material which will or may have a substantial effect on the wetland or watercourse, or wetlands or watercourses outside the area in which the activity is proposed.
2. Any activity which is likely to cause or has the potential to cause substantial turbidity, siltation or sedimentation to a wetland or watercourse.

A public hearing can be called for different reasons, one of which is significant impact. Another would be public interest.

Mr. Kennedy stated that whether it is done on the basis of significant impact or just the public interest has implications for what must be done for the public hearing. That is the reason to pick one or the other.

Mr. Anderson felt the higher standard would be “may” for the public hearing.

Mr. Laiuppa interjected that, if it is decided that there is not a significant impact, it could be stated “there is no significant impact, but in the public interest.”

Mr. Stebe asked whether there is anything else on the application that would trigger a public hearing.

Mr. Anderson clarified that the public hearing would be about the wetlands.

MOTION: Mr. Farina moved to amend the motion to remove “and is in the public interest.” Ms. Ike seconded the motion. Mr. Prause, Mr. Stebe, Ms. Luna, Mr. Farina, Ms. Ike, and Ms. Potocki voted in favor of the motion. Mr. Kennedy abstained. The motion to amend the motion passed six to zero.

Mr. Prause, Mr. Stebe, Ms. Luna, Mr. Farina, Ms. Ike, and Ms. Potocki voted in favor of the amended motion. Mr. Kennedy voted against the amended motion. The amended motion passed six to one.

DISCUSSION: OPEN SPACE ACQUISITION PROCESSES

Ms. Pilla recapped that there was a proposed draft resolution from Mr. Farina pertaining to land acquisition processes. She had some hesitancy about the PZC moving forward and referred it to the Town Attorney for feedback. The Town Attorney advises against it because the resolution as drafted would essentially be this Commission attempting to take on a responsibility that the Board of Directors has already determined belongs to another committee. The Town Attorney’s recommendation was, if this Commission truly feels there is an issue with the current land acquisition process, the Commission should request that the Board of Directors re-evaluate that process and decide if any changes need to be made to the process. The Town Attorney’s office requested that, if the Commission is inclined to pursue it, that they table the discussion so that Attorney O’Neil can join the next meeting.

Mr. Farina summarized his proposal. He stated that the Planning & Zoning Commission does have the authority to make recommendations for the implementation of the Plan of Conservation and Development. They may also make plans for open space acquisition and greenways protection and development. As the resolution is written, there is no current plan for open space acquisition or greenway protection.

Ms. Potocki noted that the Conservation Commission used to have an open space plan. She was unsure when the Land Acquisition and Historic Property Investment Committee was formed, but she recalled that there was an open space plan that would be presented to the PZC and other commissions; that is no longer the case. It may be a good initiative to develop an open space greenways protection plan and perhaps present that to the Land Acquisition and Historic Property Investment Committee.

Mr. Prause, noting that he is on the Committee, described how the Land Acquisition and Historic Property Investment Committee has been functioning with different interests to try to make decisions as required. The initial policies dated back to 2005. The most recent referendum was passed in 2016 for open space acquisition and historical preservation and has been recharged in 2023. He reported that, in the last three to four years, they have made a priority list with approximately 30 target parcels, but people are not interested in selling for various reasons. The practicality of making those acquisitions is tough. He was concerned about whether this Commission would be redundant in that task. If a priority list is published, perhaps that would compromise the ability to negotiate.

Mr. Kennedy commented that, regarding the Commission's jurisdiction, the Town Attorney is correct. This is usurpation of the Board of Directors' function. With land acquisition, the PZC cannot spend a dime. He noted that the largest problem since he has been on the Committee was recommending buying the MDC land and, months later, finding out that the Board of Directors hadn't bought it, though they eventually did. In his opinion, the Land Acquisition and Historic Property Investment Committee has done a good job of filtering out where money should be spent.

Mr. Farina noted that this is a discussion item and he can decide if he would like to submit it as an action item to a future meeting.

Mr. Prause stated that, if there is a will to move forward, they would put it on the agenda for the next meeting with Attorney O'Neil.

Ms. Potocki sought information on what is open space and what is not. Ms. Pilla responded that open space is undeveloped land.

DISCUSSION: MUNICIPAL FINES FOR INLAND WETLANDS VIOLATIONS

Ms. Pilla reported that the draft ordinance in the packet has been refined by the Inland Wetlands Agent in consultation with the Town Attorney. If the Commission is satisfied with it, they can refer this draft to the Board of Directors.

Mr. Laiuppa stated that some of the revisions were a simplification of some of the language, which he detailed. Rather than reiterating a definition already in existence, this would be a supplement to the existing wetland regulations. A question the Town Attorney is looking into is for Articles VI and VII. There is a reference to a state statute, and it may not be the correct statute.

Ms. Pilla added that the Commission could move forward, if so inclined, with referring this to the BOD. Those references to the statute can be clarified during that process.

Ms. Potocki asked whether this would be helpful as a deterrent for uncooperative parties. Mr. Laiuppa reminded Ms. Potocki of the process and that hopefully this can be avoided. He is hopeful that it will rarely, if ever, be used.

Mr. Prause reminded the members about the concern that this could be seen as something implemented with bias. Mr. Laiuppa commented that the wetlands regulations are due for an update. If this is to be adopted, there should be language in it that makes it clear that this would be in an extreme case.

Mr. Prause remarked that Art. II can be removed for definitions. He added that the Agency cannot cite the Town because the money would just go to the Collector of Revenue. Mr. Laiuppa stated that one item to explore would be having the fines earmarked to go somewhere other than the General Fund, such as the Wetland Mitigation Bank.

Mr. Kennedy stated that fining the Town is not a real action. Hopefully, issues with the Town can be dealt with informally. He warned Mr. Laiuppa to be cautious about what he writes into regulations or guidelines. Additionally, Mr. Kennedy noted that the Commission is not acting on an application, though it is on the agenda so it can be acted upon. He added that he is ambivalent, as he does not see a lot of reason for it. There have not been many situations that have shown a need for it.

Mr. Prause stated that a recommendation could be made.

Mr. Stebe remarked that there have been several items before the Agency in the last year and a half where Mr. Laiuppa has had difficulty with a violator. Essentially, the violators know that he went as far as he could go. The only further step is to convince a Town Attorney to bring a case in court, which will cost the Town many times the amount of money to be collected. Having this in the toolkit would be a valuable item for Mr. Laiuppa. He felt the Agency is at the point where it can go to the Board of Directors for review and action.

MOTION: Mr. Farina moved to make a favorable recommendation to the Board of Directors to consider adoption of an ordinance regarding municipal fines for inland wetlands violations. Ms. Potocki seconded the motion. Mr. Prause, Mr. Stebe, Ms. Luna, Mr. Farina, Ms. Ike, and Ms. Potocki voted in favor of the motion. Mr. Kennedy abstained. The motion passed six to zero.

ADMINISTRATIVE REPORTS

Ms. Pilla gave a brief update on the Siting Council petition for 250 Carter Street. A revised schedule has been posted with the public hearing set for May 2nd. At their upcoming meeting, the Board of Directors will discuss pursuing Intervener Party status.

Emma Petersen, Principal Comprehensive Planner, will be attending an upcoming meeting for a workshop with the members on the POCD implementation strategy.

Mr. Prause remarked that he spoke with Mr. Laiuppa and the Town Attorney about the Inland Wetlands Agency's place in 250 Carter Street. Looking at the docket, any wetlands information is based off the Army Corps of Engineers criteria and not the State criteria. In his opinion, it is not in compliance with Connecticut state wetlands requirements. The statute states that the Agency still retains its rights as an Inland Wetlands Commission and questioned what that means, aside from enforcing the Connecticut Wetlands. He speculated whether the Town Attorney would like the Commission to make an interrogative or through the petitioner make an interrogative.

Ms. Potocki commented that there are other local inland wetland agencies that have provided comments on petitions' declaratory rulings to the Siting Council. It wouldn't be unusual, but how much weight it has is unknown.

APPROVAL OF MINUTES

March 18, 2024 – Public Hearing/Business Meeting

MOTION: Mr. Kennedy moved to approve the minutes as written. Mr. Stebe seconded the motion and all members voted in favor.

RECEIPT OF NEW APPLICATIONS

1. **TOWN OF MANCHESTER PUBLIC WORKS DEPT.** – Inland Wetland Permit (IWP-0003-2024); Erosion & Sedimentation Control Plan (ESC-0003-2024) – Expansion of Charter Oak Park facilities including a new skate park/pump track, synthetic turf playing field, and parking lot at 30 Charter Oak Street.
2. **TRIVIK BUILDERS, LLC** – PRD Detailed Site Plan Modification (PRD-0001-2024) – Modification to site plans at 27 Lillian Drive.
3. **HARTFORD HEALTHCARE CORPORATION** – Zoning Regulation Amendment (REG-0002-2024) – To add a subsection to Article IV, Section 13 to permit a limited exception to the maximum signage limitation for medical services.

The Business Meeting was closed at 10:15 P.M.

I certify these minutes were adopted on the following date:

Date

Eric Prause, Chairman

NOTICE: A DIGITAL RECORDING OF THIS BUSINESS MEETING CAN BE HEARD IN THE PLANNING DEPARTMENT.